

AGENDA

- I. **CALL TO ORDER**
- II. **APPROVAL OF MINUTES FROM AUGUST 28, 2023**
- III. **NEW BUSINESS**
 - 1. *Case #23-20 - Swig Soda Shop Final Development Plan*
 - 2. *Case #23-19 - Swig - 5959 Barkley Final Plat*
 - 3. *Case #23-12 Morrison Ridge Final Plat*
 - 4. *Public Hearing: Case #23-10 - Comprehensive Plan*
- IV. **OLD BUSINESS**
- V. **PLANNING COMMISSION COMMENTS**
- VI. **STAFF UPDATES**

Questions concerning this meeting may be addressed to the staff contact,
Karie Kneller, City Planner, at (913) 676-8366 or kkneller@missionks.org



AT A GLANCE

Applicant:
Superstar Holding, LLC

Case Number:
#23-20

Location:
5959 Barkley Street

Project Name:
Swig Soda Shop Final Development Plan

Property ID:
KF251208-2050

Project Summary:
Proposed final development plan for a drive-through soda and cookie shop on the northeast corner of Martway Street and Barkley Street.

Current Zoning:
C-2A

Proposed Zoning:
N/A

Current Land Use:
Vacant

Staff Contact:
Karie Kneller, Planner

Proposed Land Use:
Drive-through Food Establishment

N/A Public Hearing Required

Legal Notice:
N/A



PROPERTY BACKGROUND AND INFORMATION

The applicant, Superstar Holdings, LLC, submitted an application for a preliminary development plan for property located on the northeast corner of Martway Street and Barkley Street, which was recommended for approval by the Planning Commission in June 2023 and approved by the City Council in July 2023. The subject property is approximately .71 acres that is currently a parking lot with a gazebo structure. The property is zoned C2-A “Pedestrian Oriented Business District.” The property lies within the Form Based Code overlay district.

PROJECT PROPOSAL

The applicant proposes a drive-through soda shop with vehicle parking, landscaping, internal pedestrian walkways, and outdoor seating. Vehicular circulation is confined to the existing northernmost entrance/exit. The southernmost curb-cut on Barkley will be eliminated. Cars enter the site and circulate clockwise through double-stacked lanes of the drive-through to a pick up window in the one-story 650 square foot building. The estimated daily vehicle count is approximately 347 cars, with a peak time during the 8:00AM and 5:00PM hours. There is also a pedestrian walk-up window for orders and pickup with a bike rack located nearby. Eight proposed parking spaces, including one ADA accommodation and an EV charging station, are located on the north side of the lot, generally in the rear. A dumpster enclosure is located within the green space, constructed of CMU blocks, which will be painted to match the primary structure. Greenspace will increase with the proposal from .22 acres (31%) to .37 acres (52%), including access for the community to a small, centrally located parklet with park benches and shade trees.

PLAN REVIEW AND ANALYSIS

Municipal Code

Consideration of final development plans is outlined in the Mission Municipal Code at §440.190. A final development plan which contains modifications from the approved preliminary development plan but is in substantial compliance with the preliminary development plan, may be approved by the Planning Commission without a public hearing if the landscaping and screening plan is adequate as determined by the Commission.

Analysis: Modifications are not significant and are in compliance with the municipal code. The applicant and design team considered Planning Commission, City Council, and public input in its final design.

Sustainability

The project team met with the Sustainability Commission on June 5, 2023. The team provided a completed scorecard with an 80/100 score at the gold level. The Scorecard Sub-Committee will be reviewing the criteria and will provide options for them to improve the score. Favorable comments from

the meeting included a decrease of impervious area and increased landscaping with drought-resistant plants. Nearby transit opportunities, bike racks, and extended sidewalk connections in place of existing curb cuts, along with “no idling” signs that will help improve the health and mobility options for visitors. An EV charging station provides alternative energy for vehicular transportation. Additionally, a shaded parklet central on the site will provide space for recreation and community gathering for pedestrians, bicyclists, and visitors traveling by vehicle. LED lighting with Dark Sky Association standards will follow sustainable lighting standards.

Analysis: Elements that promote environmental, social, and economic improvements on the site enhance the project and further Mission’s sustainability goals. While operations will primarily serve customers in vehicles, the enhanced pedestrian and bicycle amenities, along with an alternative fuel source on site and minimal parking, will encourage multi-modal visitors.

RECOMMENDATION

Staff recommends that the Planning Commission recommend approval of the Swig Soda Shop Final Development Plan, Case #23-20 with the following conditions in accordance with the approved preliminary development plan:

1. Landscaping shall be native and non-invasive species, following guidelines provided by the Kansas State Extension Office for Northeast Kansas.
2. All site improvements shall be maintained in perpetuity by the owner or owner’s agent of the property.
3. Lighting shall be indicated on the construction permitting documents as applicable to the standards of the International Dark Sky Association.
4. All signage shall be submitted as a package with a separate sign permit application prior to installation; all municipal code regulations shall be applicable.
5. The applicant shall submit all necessary construction permitting documents to the City for review and shall adhere to the approved final development plan and conditions.
6. The applicant shall obtain all approvals from the Consolidated Fire District No. 2 prior to building permit issuance.
7. The applicant shall obtain all approvals from Johnson County Wastewater and Johnson County Water District #1 prior to building permit issuance.
8. The applicant shall be responsible for all damage to existing City infrastructure, including roads, curbs, and sidewalks during construction. Repairs shall be of a quality like or better than existing conditions before final Certificate of Occupancy issuance.

9. The applicant shall provide a two (2) year warranty bond on all public infrastructure installed as part of this Preliminary Development Plan; bond(s) will be placed on file with the City of Mission Community Development Department.

10. The preliminary development plan approval shall lapse in five (5) years from the effective date of the ordinance (June 26, 2023) if construction on the project has not begun; provided, however, that the applicant may request a hearing before the City Council to request an extension of this time period for up to 12 months.

PLANNING COMMISSION ACTION

The Planning Commission will consider Case #23-20, Swig Soda Shop Final Development Plan, at its September 25, 2023 meeting.

CITY COUNCIL ACTION

None.

GENERAL PROJECT NOTES:

- The Contractor shall, at a minimum, have the following document(s) at the job site at all times:
Signed approved Plans,
Contract Documents and Project Specifications,
Standard Specifications (City of Mission and Kansas City Metro Chapter-APWA),
All required permits
- This Project shall be constructed in accordance with these Plans current City of Mission Specifications, and Project Specifications, in their absence Kansas City Metro Chapter of American Public Works Association (most current version).
- All work required to complete the project and that is not specifically itemized in the Contractor's proposal shall be considered subsidiary to other work itemized in the proposal.
- All materials and workmanship associated with this project shall be subject to inspection by the City of Mission and the Owner. The City and/or Owner reserves the right to accept or reject any such materials and workmanship that does not conform to the Standards and Technical Specifications.
- The Contractor shall notify the Engineer immediately of any discrepancies in the plans.
- By use of these Plans the Contractor agrees that he shall be solely responsible for the safety and protection of the construction workers and the public.
- Contractor is to obtain the necessary permits for all construction activities.
- Contractor shall be responsible for determining the exact locations of all underground utilities or appurtenances prior to commencing construction. Existing underground utilities shown on the drawings are for reference only, and their accuracy and completeness are not guaranteed. Contractor shall be responsible for repair or replacement of all underground utilities damaged during construction.
- It shall be the responsibility of the Contractor to control erosion and siltation during all phases of construction.
- Any streets, driveway, sidewalk, curb & gutter, or pavement disturbed, damaged or destroyed during construction shall be replaced by Contractor at no additional cost to Owner.
- Modified curb shall be used at all locations where pavement drains away from curb.
- Contours shown are to finished grade.
- Contractor shall be responsible for removal and proper disposal of all removed materials and debris.
- Refer to easement documents for limits of construction.

GENERAL UTILITY NOTES:

- All existing utilities indicated on the drawings are according to the best information available to the Engineer; however, all utilities actually existing may not be shown. Utilities damaged through the negligence of the Contractor to obtain the location of same shall be repaired or replaced by the Contractor at his expense.
- All backfill shall be compacted to 98% standard density.
- All materials and workmanship associated with this project shall be subject to inspection by the City of Mission. The City of Mission reserves the right to accept or reject any such materials and workmanship that does not conform to the City of Mission standards and technical specifications. The Contractor shall notify the City of Mission Public Works Department twenty-four (24) hours prior to the beginning of construction.
- Relocation of any utilities thereof required for the construction of this project shall be the responsibility of the Contractor at his expense.
- The Contractor shall install and properly maintain a mechanical plug at all connection points with existing lines until such time that the new line is tested and approved.

STORM NOTES:

- All HDPE pipe shall be Water-Tight
- All High Density Polyethylene (HDPE) pipe shall conform to AASHTO M294 Type S. Acceptable pipe must come from a Plastic Pipe Institute (PPI) certified manufacturer and have passed the PPI 3rd Party Certification testing. Each individual section of pipe shall be marked in accordance with AASHTO M294 and shall be affixed with the PPI Certification label. HDPE pipe shall be joined with water tight joints meeting the requirements of AASHTO M294 Paragraph 7.9.3.
- Pipe lengths are from center of structure to center of structure.
- Provide minimum 24" separation between sanitary and storm per APWA 5505.1C(5)C and minimum 18" separation between water and storm.





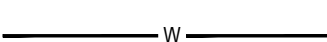




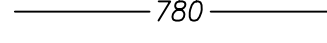

GRADING NOTES:

- Erosion protection shall be in place prior to any land disturbance.
- Contours shown are to finished grade.
- The construction area shall be cleared, grubbed, and stripped of topsoil and organic matter from all areas to be occupied by building and paving. Topsoil for replacement on slopes may be stockpiled on site. Excess topsoil may be wasted in fill slopes provided that no topsoil will be wasted within 20 feet of the edge of the building or parking area (including future building and future parking areas per the direction of the Engineer). Stripping existing topsoil and organic matter shall be to a minimum depth of six (6) inches.
- Areas to receive fill shall be scarified and the top eight (8) inch depth compacted to 95% standard proctor density. Any unsuitable areas shall be undercut and replaced with suitable material before any fill material can be applied.
- Earthwork under the building shall comply with the project building plans. Other fill material shall be made in lifts not to exceed nine (9) inches depth compacted to 95% standard proctor density. Fill material may include rock from on-site excavation if carefully placed so that large stones are well disturbed and voids are completely filled with smaller stones, earth, sand or gravel to furnish a solid embankment. No rock larger than three (3) inches in any dimension nor any shale shall be placed in the top 12 inches of embankment.
- Areas that are to be cut to subgrade levels shall be proof rolled with a loaded dump truck (per Owner requirements, typically a tandem axle with at least a 50,000 lb gross weight) or similar approved construction equipment to detect unsuitable soil conditions. All proof rolling and undercutting should be performed during a period of dry weather.
- In all areas of excavation, if unsuitable soil conditions are encountered, a qualified Geotechnical engineer shall recommend to the Owner on the methods of undercutting and replacement of property compacted, approved fill material.
- All slopes are to be 3:1 or flatter unless otherwise indicated.
- All slopes and areas disturbed by construction shall be graded smooth and four (4) inches of topsoil applied. If adequate topsoil is not available on-site, the Contractor shall provide topsoil, approved by the Owner, as needed. Any areas disturbed for any reason shall be corrected by the Contractor at no additional cost to the Owner prior to final acceptance of the project.
- All disturbed areas shall be seeded, fertilized and mulched or sodded in accordance with the standards and specifications adopted by the reviewing governing agency and good engineering practices.

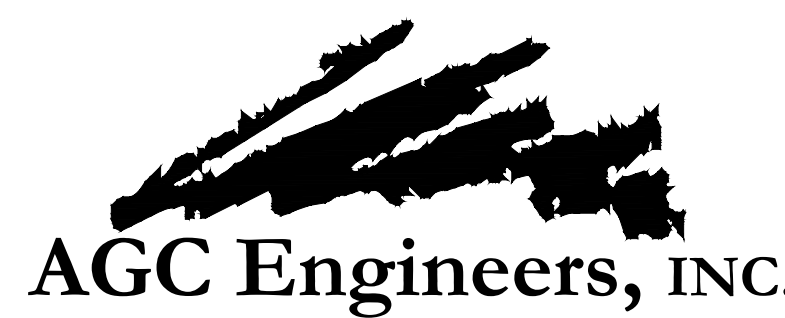
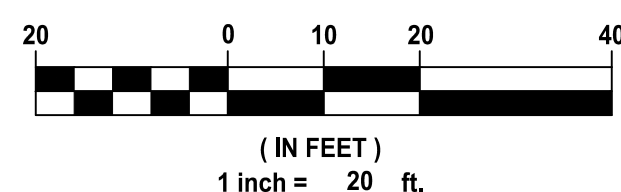
EROSION CONTROL NOTES:

- Control of sediment is a very dynamic (ever changing) process. These plans are provided as a basis of anticipated erosion control measures. The Contractor shall modified add or delete with the Owner's permission the erosion control measure shown to prevent the migration of sediment off of the Owner's property and/or into jurisdictional waters/waterways.
- RESERVED.
- Provide and maintain Vehicle Tracking Control Device at all construction entrances. Any sediment deposited on public streets shall be removed immediately by Contractor at his sole expense.
- Stockpile excavation materials away from existing channels and grade to drain to adequate erosion control measures.
- Remove silt build up in temporary sediment basins (if applicable), inlet protection devices and/or silt fence until site is completely stabilized. Verify grade prior to final seeding, lining or rip-rap installation.
- All disturbed areas shall be seeded, fertilized and mulched or sodded in accordance with the standards and specifications adopted by the reviewing governing agency and good engineering practices. Permanent fertilizing, seeding and mulch shall be in accordance with APWA 2401 SEEDING except as clarified below.
-Fertilizer shall be applied at the rate of 435 lbs per acre
-Seed mixture shall be Mix #1 at a rate of 600 lbs per acre
-Final acceptance per MoDOT Section 805.4; however, grass density shall be 95% (rather than 70%) over 100% of the disturbed area.
- Seeding/Sodding shall be completed within 14 days after completing the work, in any area. If this is outside of the recommended seeding period, erosion control measures or other similarly effective measure shall remain and be maintained by Contractor until such time that the areas can be seeded and a stand of grass established per MoDOT Section 805.4 and Missouri DNR standards.
- When sediment deposits reach approximately one-half the height of the BMP, the sediment shall be removed or a second BMP shall be installed. All costs associated with this work, including related incidents, shall be the Contractor's responsibility and shall be included in the bid for the proposed work.
- Contractor shall perform BMP inspection once a week and after each rainfall event, and provide Owner a copy of report within 48 hrs. Faulty or inadequate erosion control measures shall be remediated or modified the same day of inspection so as to minimize the risk of sediment discharge from the Owner's property or jurisdictional waters/waterways.
- Contractor shall protect and maintain erosion control measures until a complete stand of grass as defined by Missouri DNR standards and MoDOT Section 805.4 has been established.
- Concrete Washout Areas will be determined onsite by the Owner.
- The Contractor shall install Erosion Control Blanket (ECB) on all slopes with 3:1 slope or greater. ECB shall be Landlok CS2 or approved equal.
- Reference KC Metro Chapter of APWA Division III Standard Drawings - Erosion & Sediment Control for standard details.
<http://kcmetro.apwa.net/PageDetails/439>

LEGEND

EXISTING	PROPOSED		
CB	CATCH BASIN		SANITARY STRUCTURE
CMP	CORRUGATED METAL PIPE		SANITARY SEWER
CWS	CROSSWALK SIGNAL		STORM STRUCTURE
DC	DEPRESSED CURB		STORM SEWER
EM	ELECTRIC METER		WATERLINE
FH	FIRE HYDRANT		WATER METER
GP	GATE		WATER VALVE
HDPE	HIGH DENSITY POLYETHYLENE		ELECTRICAL LINE
IG	IRON GRATE		CLEANOUT
LP	LIGHT POLE		CONTOUR
LP	LIGHT POLE		LIGHT POLE (SITE PARKING)
MH	MANHOLE	D/E	DRAINAGE EASEMENT
PA	POLE ANCHOR	GM	GAS METER
PP	POLE POLE	WM	WATER METER
RCP	REINFORCED CONCRETE PIPE	E/E	ELECTRIC EASEMENT
ROH	ROOF OVERHANG	U/E	UTILITY EASEMENT
SSMH	SANITARY SEWER MANHOLE	B/L	BUILDING LINE SETBACK
SP	SIGN POST	MH	MANHOLE
SSMH	STORM WATER MANHOLE	R	RADIUS OR RAMP (as it relates to sidewalks)
TS	TRAFFIC SIGNAL	L	LANDING (as it relates to sidewalks)
UGM	UNDERGROUND UTILITY MARKER	S/W or SW	SIDEWALK
UV	UTILITY VAULT	AC	AIR CONDITIONER
VCP	VITRIFIED CLAY PIPE	MEP	MECHANICAL, ELECTRICAL & PLUMBING
WM	WATER METER	WSD	WATER SERVICES DEPARTMENT
YH	YARD HYDRANT	D.S.	DOWN SPOUT
X	FENCE	TC	TOP OF CURB
P	OVERHEAD POWER LINE	G	GROUND
T	OVERHEAD TELEPHONE LINE	P	PAVEMENT
G	GAS LINE	LP	LOW POINT
SS	SANITARY SEWER LINE	HP	HIGH POINT
SW	STORM WATER LINE		
UE	UNDERGROUND ELECTRIC LINE		
UFO	UNDERGROUND FIBER OPTIC LINE		
UT	UNDERGROUND TELEPHONE LINE		

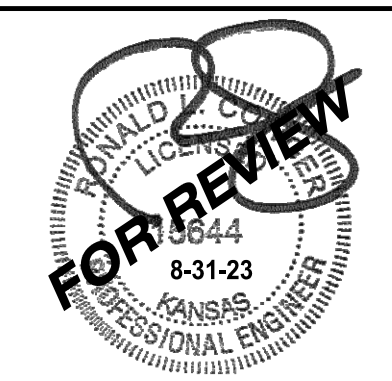
BY	REVISION	DATE
ACA	ADDRESS CITY COMMENTS DATED 8-23-23	8-31-23
ACA	SUBMITTED TO CITY FOR REVIEW	7-24-23



405 S. Leonard St., Suite D
Liberty, Missouri 64068

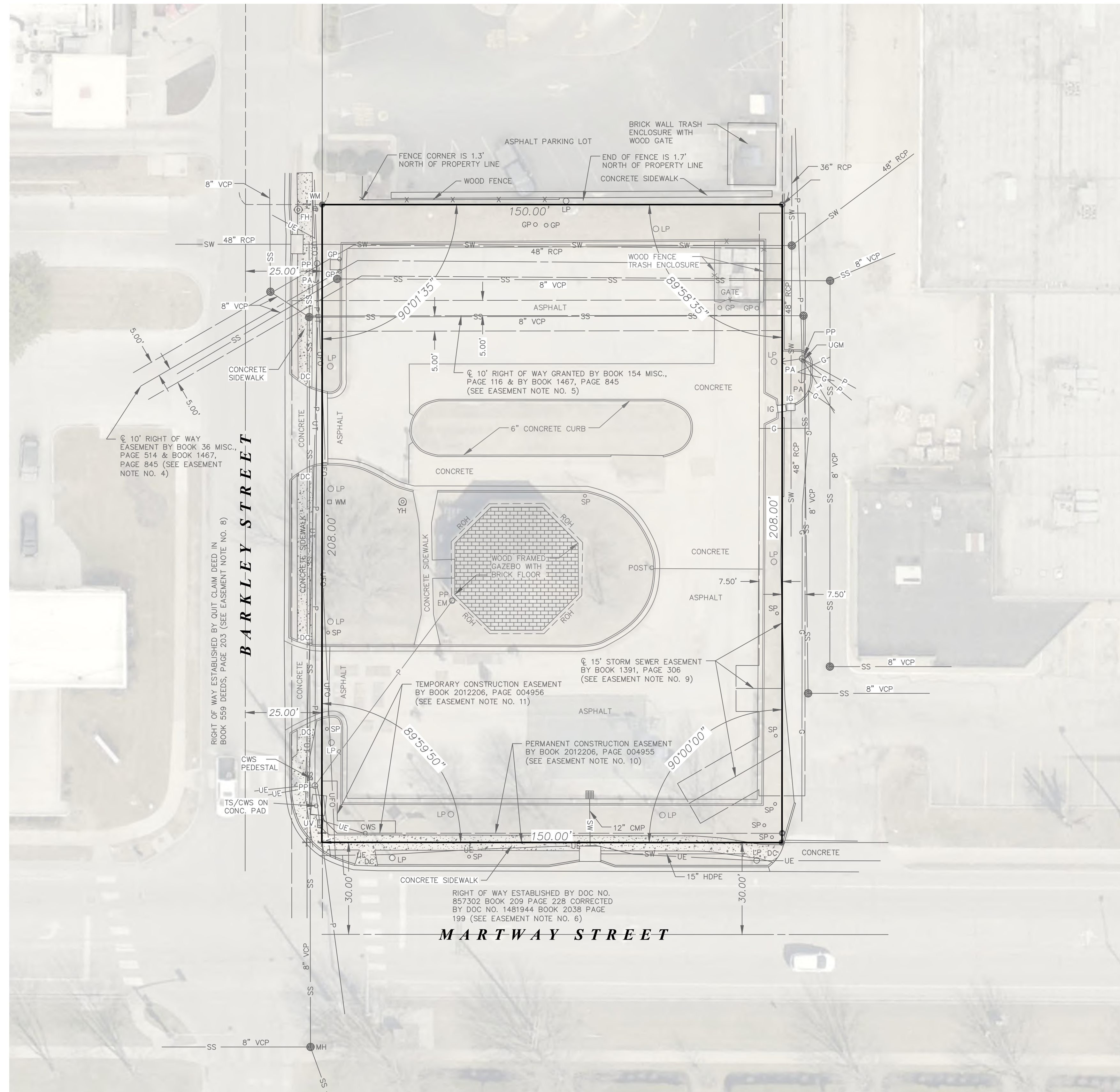
816.781.4200 ■
fax 792.3666

www.agcengineers.com



SWIG - 5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS

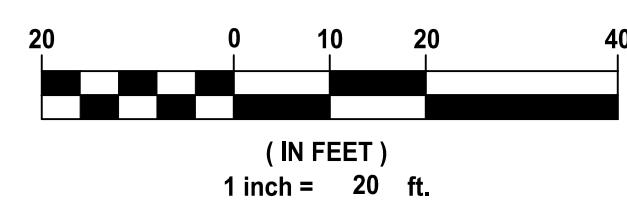
FINAL DEVELOPMENT PLANS
GENERAL NOTES AND LEGEND



SURVEY DATA PROVIDED HEREON IS FROM ANDERSON SURVEY COMPANY (DATED 11-23-22)



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AGC Engineers, INC.

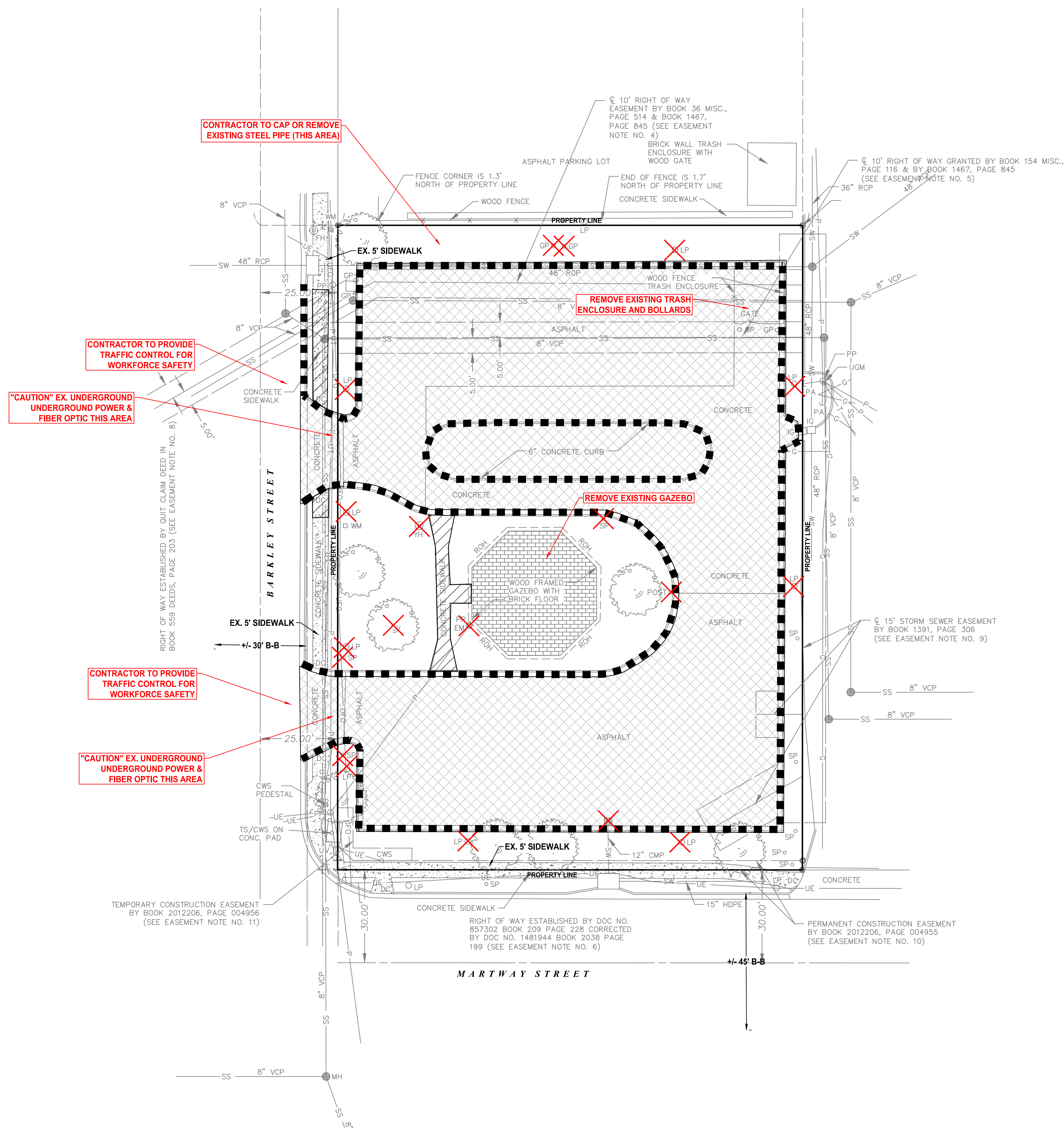
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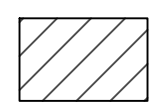
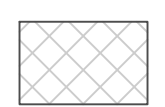


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SWIG - 5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS

FINAL DEVELOPMENT PLANS
EXISTING CONDITIONS



LEGEND

-  SIDEWALK REMOVAL
-  EXISTING PAVEMENT REMOVAL
-  EXISTING CURB REMOVAL
-  REMOVAL ITEM

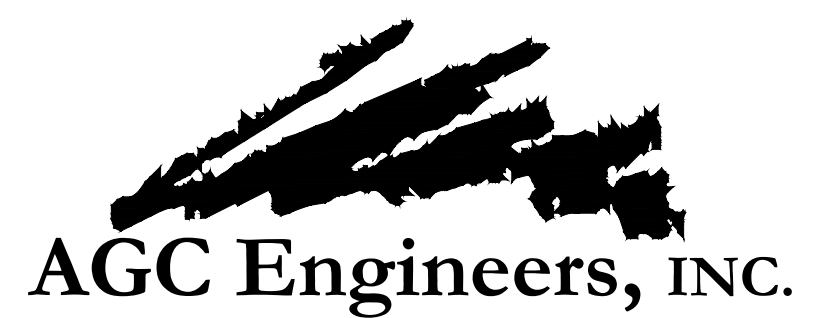
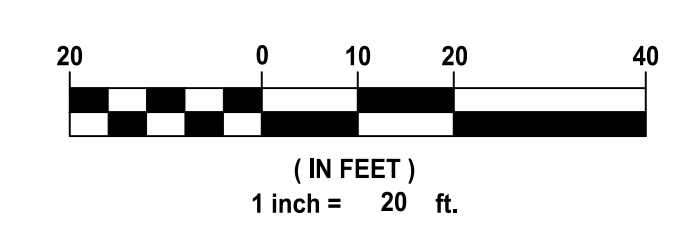
NOTE:
EXISTING PAVEMENT MAY BE RECLAIMED AND USED IN SUBGRADE.
MATERIAL GRADATION SHALL BE MAXIMUM OF 2" MINUS.

EXISTING

- CB CATCH BASIN
- CMP CORRUGATED METAL PIPE
- CWS CROSSWALK SIGNAL
- DC DEPRESSED CURB
- EM ELECTRIC METER
- FH FIRE HYDRANT
- GATE GATE
- GP GUARD POST
- HDPE HIGH DENSITY POLYETHYLENE
- IG IRON GRATE
- LP LIGHT POLE
- MH MANHOLE
- PA POLE ANCHOR
- PP POWER POLE
- RCP REINFORCED CONCRETE PIPE
- ROH ROOF OVERHANG
- SSMH SANITARY SEWER MANHOLE
- SP SIGN POST
- SWMH STORM WATER MANHOLE
- TS TRAFFIC SIGNAL
- UGM UNDERGROUND UTILITY MARKER
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- WM WATER METER
- YH YARD HYDRANT
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- P OVERHEAD POWER LINE
- T OVERHEAD TELEPHONE LINE
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- UT UNDERGROUND TELEPHONE LINE



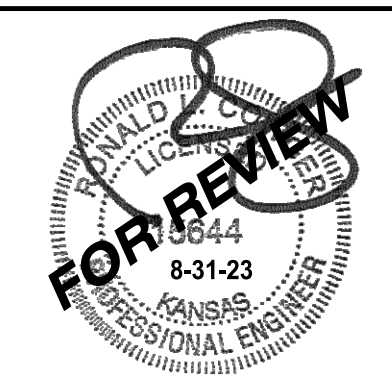
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SWIG - 5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS

FINAL DEVELOPMENT PLANS
DEMOLITION PLAN

SITE DATA

LOCATION: 5959 BARKLEY STREET, MISSION, KANSAS

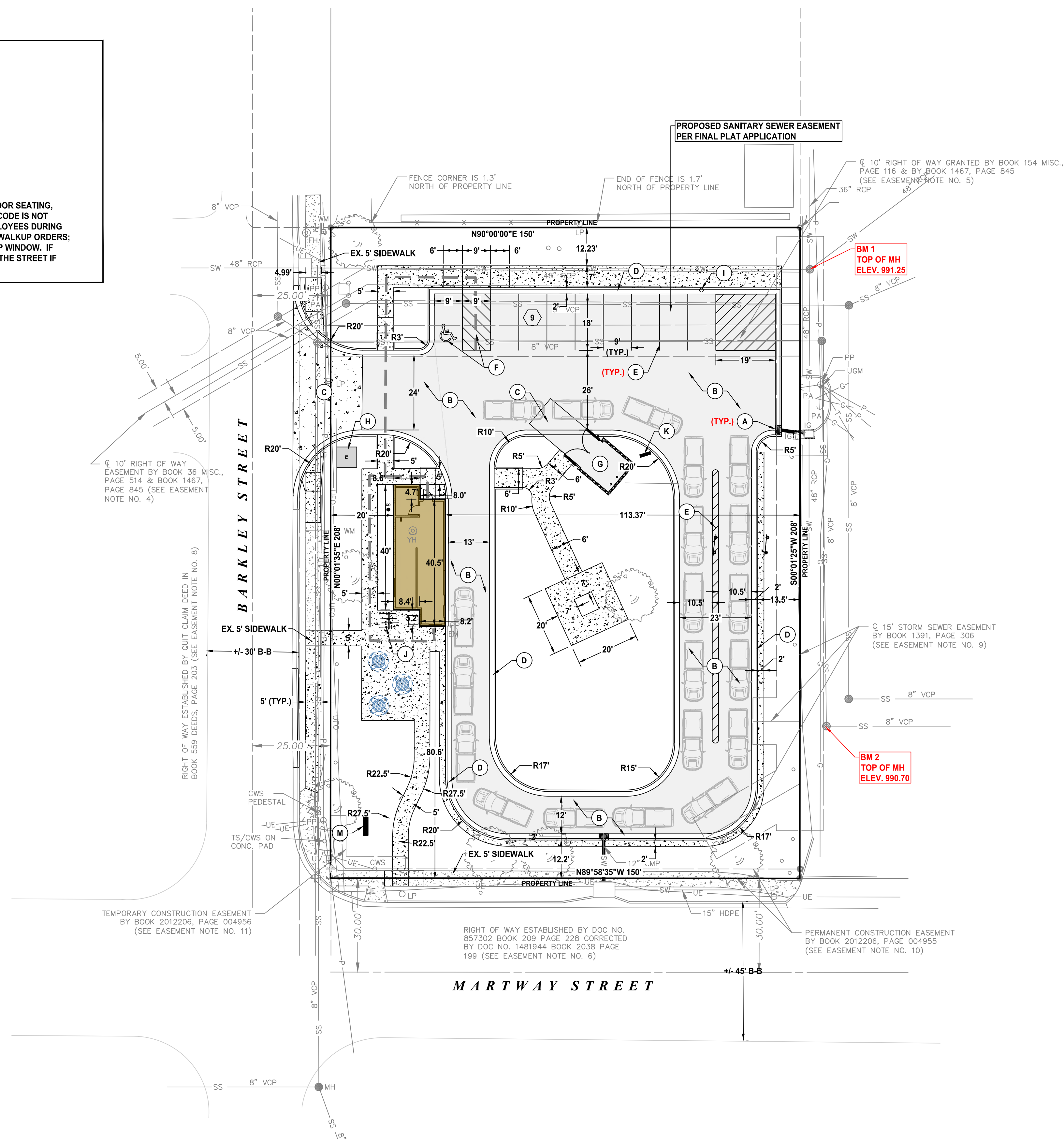
PARCEL AREA: 0.71 ACRES

ZONING: C-2A (PEDESTRIAN ORIENTED BUSINESS)

PROPOSED BUILDING: 1 STORY, 650 SF FLOOR AREA RATIO - 0.021

PROPOSED PARKING: 8 SPACES + 1 ADA PROVIDED

REQUIRED PARKING: THIS PROJECT IS A DRIVE-THROUGH WITHOUT INDOOR SEATING, UNLIKE A SONIC WHICH IS A DRIVE-IN; THEREFORE, THE CITY'S PARKING CODE IS NOT APPLICABLE TO THIS USE. SWIG TYPICALLY HAS A MAXIMUM SIX (6) EMPLOYEES DURING MAX SHIFT. THE PROPOSED ONSITE PARKING ALLOWS FOR ADDITIONAL WALKUP ORDERS; BUT, THE INTENT IS TO PEDESTRIAN AND BICYCLE USERS AT THE WALKUP WINDOW. IF NECESSARY, THERE IS EXISTING PUBLIC PARKING ON BARKLEY ACROSS THE STREET IF OVERFLOW PARKING IS REQUIRED.



LEGEND:

- ADA PEDESTRIAN ROUTE
- ⬡ PARKING STALL COUNTS

KEY LEGEND

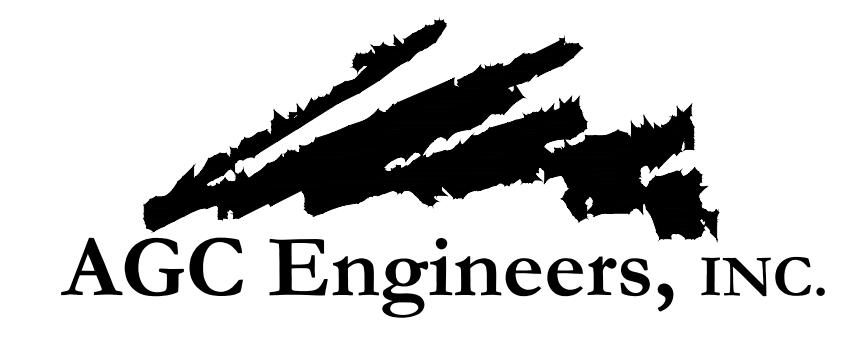
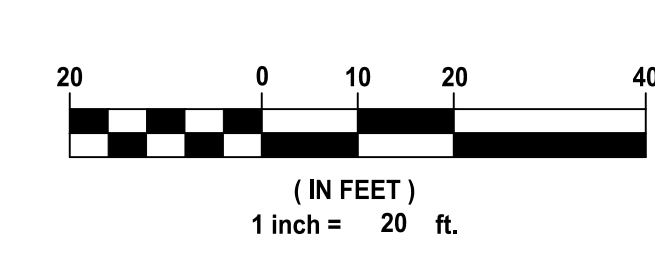
- (A) CURB INLET (RE: UTILITY PROFILES)
- (B) ASPHALT PAVEMENT
- (C) HEAVY DUTY CONCRETE
- (D) CG-1 CURB & GUTTER (RE: SPOT ELEVATION PLANS)
- (E) STRIPING - 4" YELLOW
- (F) STRIPING - (RE: ADA ACCESSIBLE STRIPING LAYOUT)
- (G) TRASH ENCLOSURE (RE: ARCH)
- (H) ELECTRICAL TRANSFORMER - EVERYGY MAY BE ABLE TO SERVE SWIG BUILDING FROM A POLE MOUNTED TRANSFORMER TO THE NORTHWEST - CONTRACTOR TO COORDINATE (RE: MEP)
- (I) EV CHARGING STATION / SIGNAGE
- (J) BIKE RACK (SEE DETAIL SHEET 16)
- (K) "NO IDLING" SIGN
- (L) LIGHT POLE (RE: MEP)
- (M) MONUMENT SIGN (RE: ARCH)

NOTES:

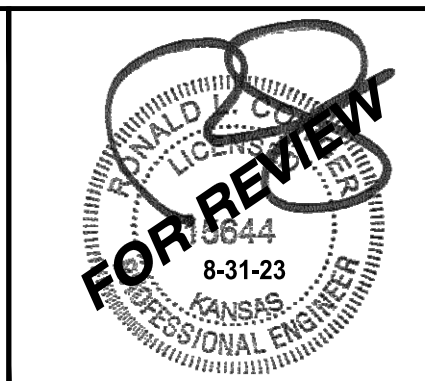
1. CONTRACTOR SHALL COORDINATE WITH SUPERSTAR HOLDINGS, LLC. FOR ANY TENANT - SPECIFIC SIGNAGE, MENU BOARDS, CLEARANCE POSTS, ETC. NOT AVAILABLE TO CIVIL ENGINEER AT TIME OF SITE DESIGN.
2. CONTRACTOR SHALL REVIEW LANDLORD WORK ORDER LETTER PRIOR TO BEGINNING CONSTRUCTION.
3. CONTRACTOR SHALL CONSTRUCT IMPROVEMENTS IN PUBLIC RIGHT-OF-WAY (DRIVE ENTRY, SIDEWALK, ETC.) TO CITY SPECIFICATIONS.



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SWIG - 5959 BARKLEY STREET
 MISSION, JOHNSON COUNTY, KANSAS
 FINAL DEVELOPMENT PLANS
 SITE PLAN


ADA NOTES & LEGEND

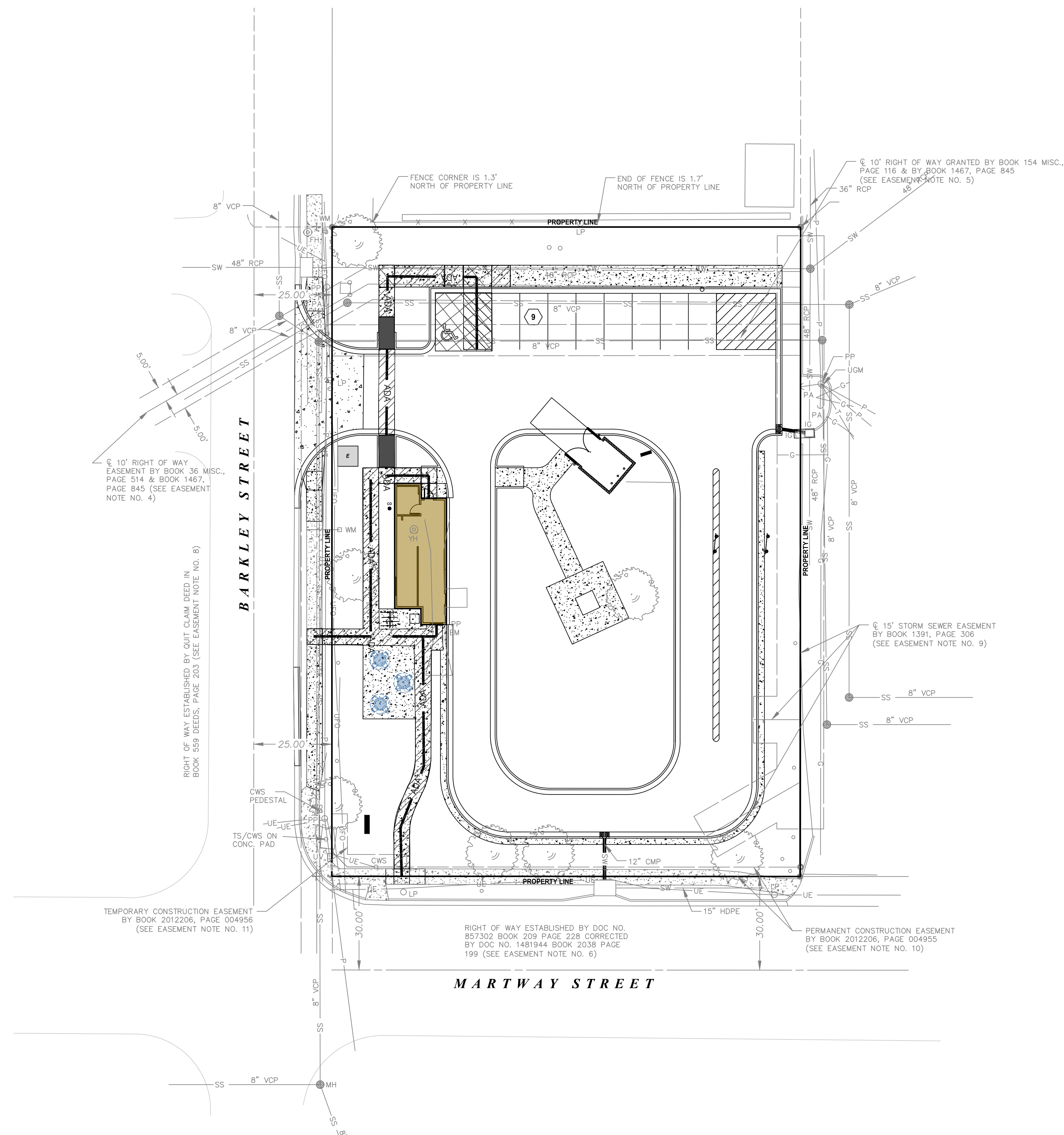
1. ADA ROUTES AND AREAS WILL BE FIELD VERIFIED BY THE OWNER. AREAS OUT OF COMPLIANCE WILL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS SOLE EXPENSE.
2. 1/8" MAXIMUM DEPTH TO TOP OF SEALANT AND 1/8" MAXIMUM PROTRUSION TO TOP OF SEALANT ALONG ADA ACCESS ROUTES.

— ADA — ADA ROUTE

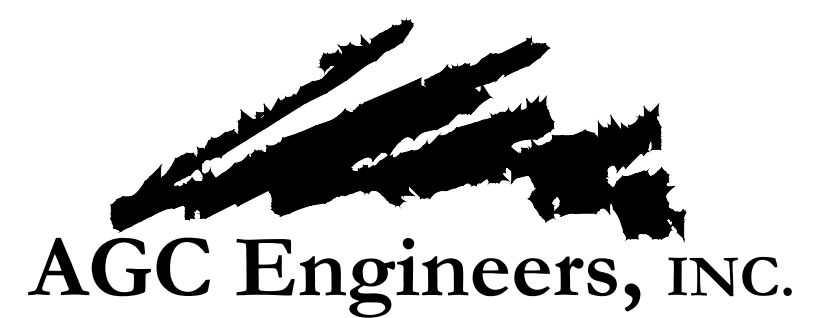
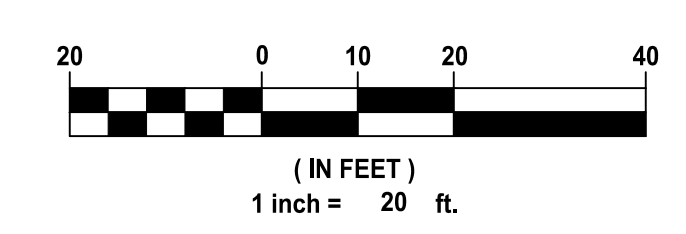
 MAX 2% SLOPE IN ANY DIRECTION (AS MEASURED WITH A 2' SMART LEVEL).

 MAX 2% MAXIMUM CROSS SLOPE AND MAX 5% LONGITUDINAL (RUNNING) SLOPE WHEN MEASURED WITH A 2' SMART LEVEL. WHEN ADJACENT STREET GRADES EXCEED 5% AND SIDEWALKS ARE CONTAINED WITHIN THE PUBLIC R/W, THEN SIDEWALK LONGITUDINAL SLOPE CAN MATCH STREET GRADES.

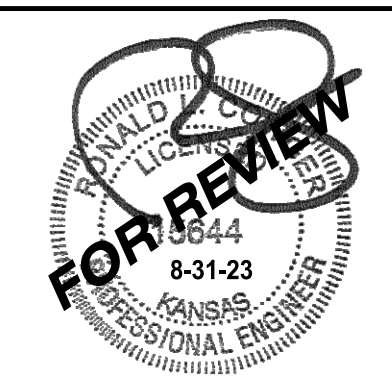
 MAXIMUM 2% CROSS SLOPE AND MAXIMUM 1:12 (LONGITUDINAL) RUNNING SLOPE WHEN MEASURED WITH A 2' SMART LEVEL.



BY	REVISION	DATE
ACA	ADDRESS CITY COMMENTS DATED 8-23-23	8-31-23
ACA	SUBMITTED TO CITY FOR REVIEW	7-24-23



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SWIG - 5959 BARKLEY STREET
 MISSION, JOHNSON COUNTY, KANSAS

FINAL DEVELOPMENT PLANS
ADA COMPLIANCE PLAN

6

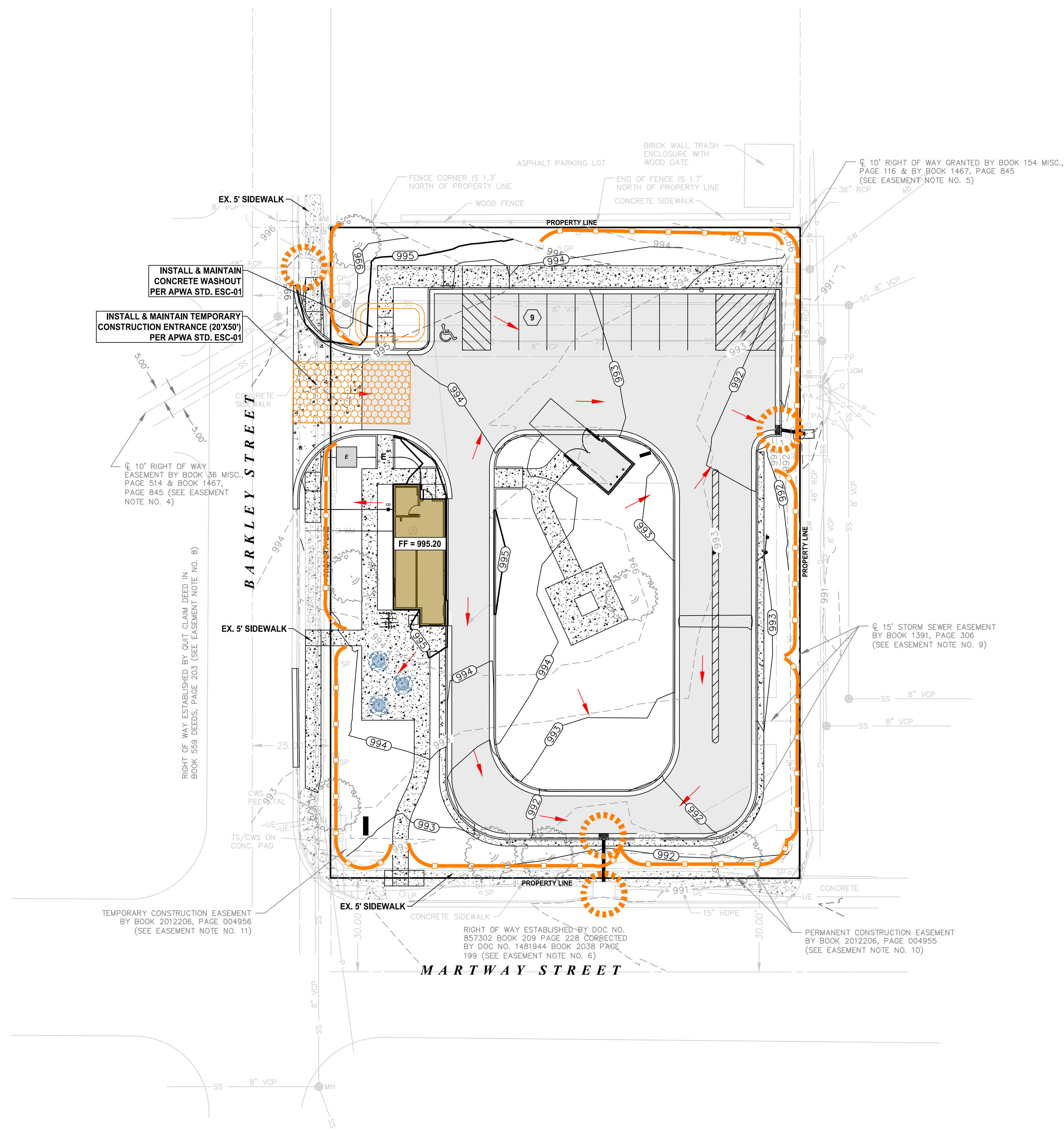
LEGEND:

EROSION CONTROL

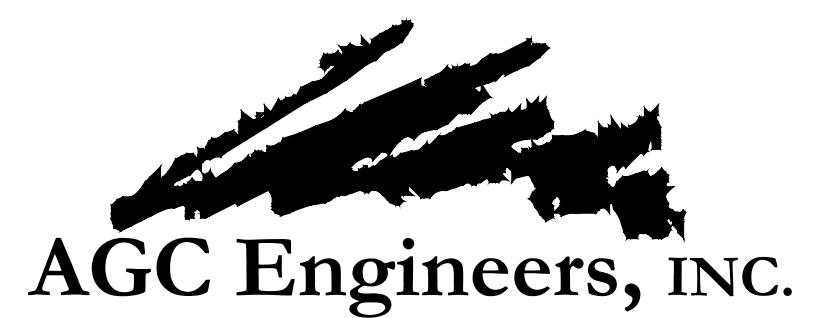
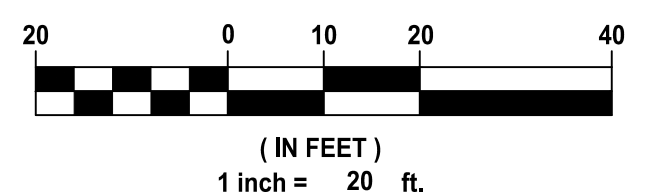
-  INLET PROTECTION PER APWA STD. DWG ESC-06
-  SILT FENCE PER APWA STD. DWG ESC-03

NOTES:

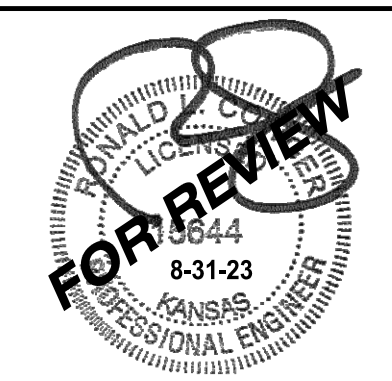
1. INSTALL TEMPORARY CONSTRUCTION ENTRANCE AND PERIMETER SILT FENCE BEFORE GRADING.
2. REMOVE TEMPORARY BMPs AFTER PAVING IS COMPLETED AND PERMANENT GRASS IS ESTABLISHED.
3. DISTURBED AREA = 0.73 AC



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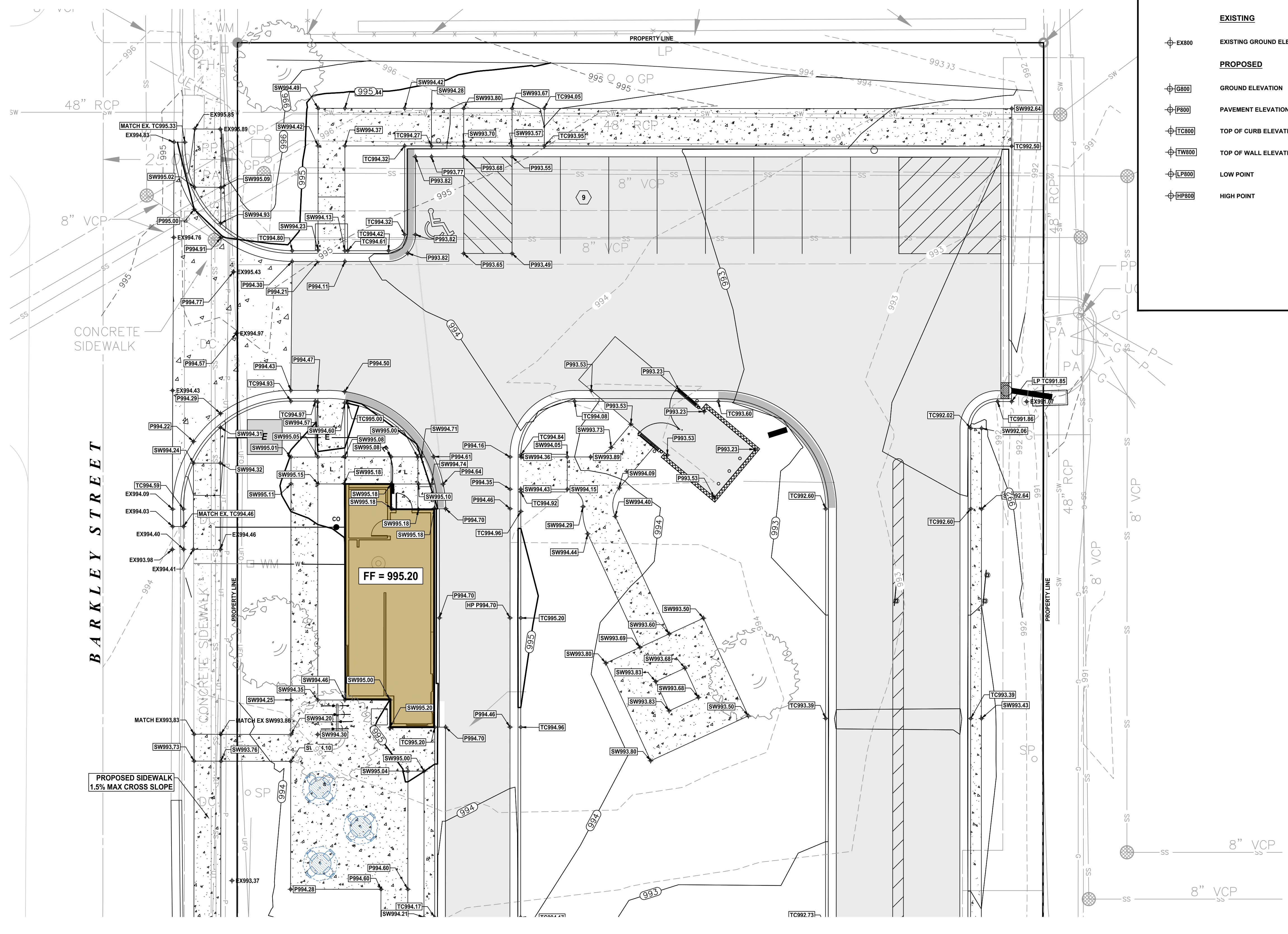


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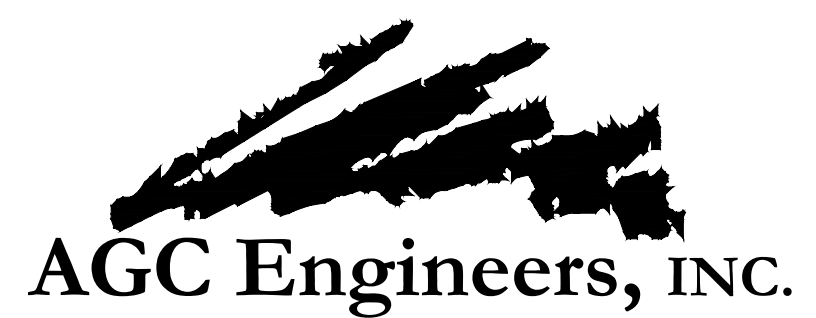
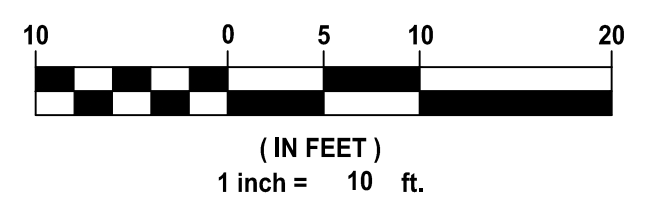
FINAL DEVELOPMENT PLANS
 GRADING AND EROSION CONTROL PLAN

7

EXISTING		PROPOSED	
⊕ EX800	EXISTING GROUND ELEVATION	⊕ G800	GROUND ELEVATION
⊕ P800	PAVEMENT ELEVATION	⊕ SW800	SIDEWALK ELEVATION
⊕ TC800	TOP OF CURB ELEVATION	⊕ SWTC800	SIDEWALK/TOP OF CURB
⊕ TW800	TOP OF WALL ELEVATION	⊕ SWIP800	SIDEWALK/TOP OF PAVEMENT
⊕ LP800	LOW POINT	⊕ FF800	FINISHED FLOOR
⊕ HP800	HIGH POINT	====	CG-1 CURB AND GUTTER
		=====	CG-1 MODIFIED CURB AND GUTTER
		R	RAMP
		L	LANDING
		T	TRANSITION
		---	GRADE BREAK



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ACA	ADDRESS CITY COMMENTS DATED 8-23-23	8-31-23
ACA	SUBMITTED TO CITY FOR REVIEW	7-24-23

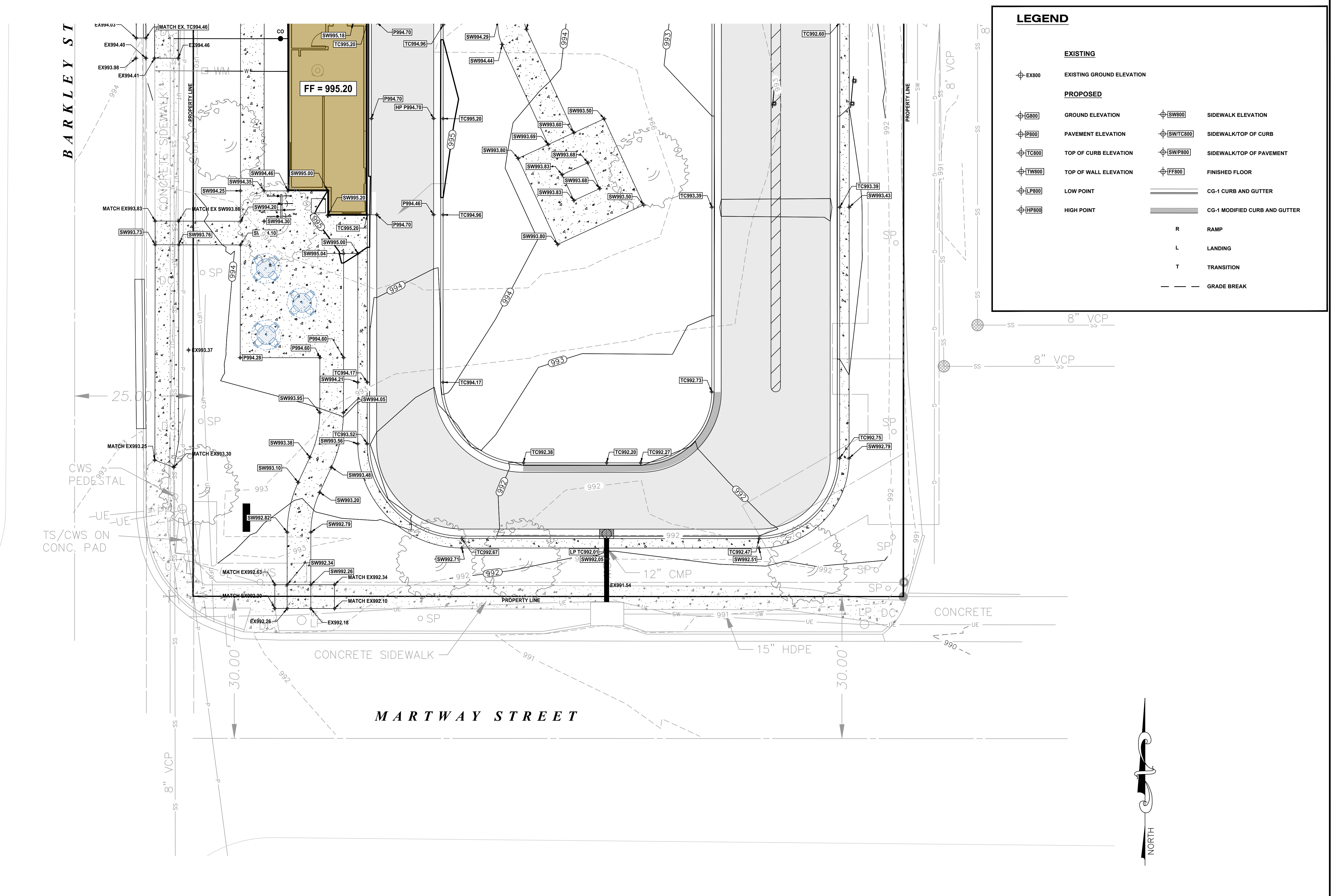


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SWIG - 5959 BARKLEY STREET
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 FINAL DEVELOPMENT PLANS
 SPOT ELEVATION PLAN

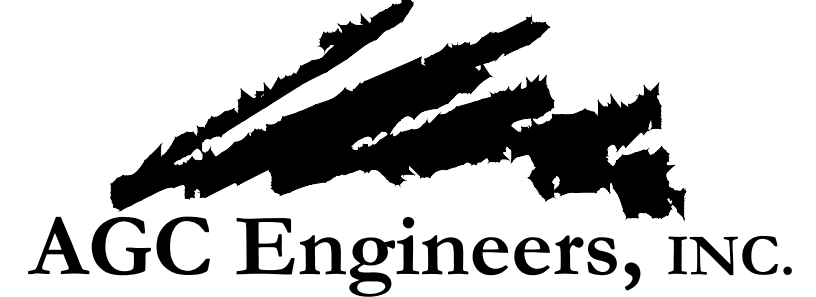
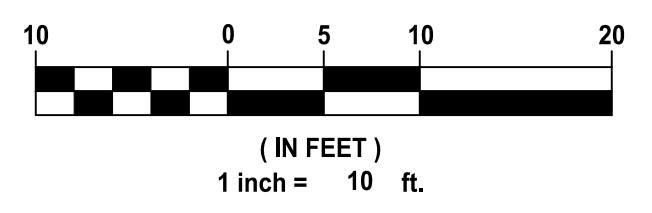
N:\Land Projects\5959 Barkley Street Development - SWIG (Mission)\Drawings\Drawings-Final Development Plans\SPOT ELEVATION PLAN.dwg, SPOT ELEVATION PLAN, 8/31/2023 11:05:05 AM, ANSI (full bleed) 24.00 x 36.00 (inches), 1:1



LEGEND

EXISTING		PROPOSED	
⊕ EX800	EXISTING GROUND ELEVATION	⊕ [SW800]	SIDEWALK ELEVATION
⊕ [G800]	GROUND ELEVATION	⊕ [SWTC800]	SIDEWALK/TOP OF CURB
⊕ [P800]	PAVEMENT ELEVATION	⊕ [SWP800]	SIDEWALK/TOP OF PAVEMENT
⊕ [TC800]	TOP OF CURB ELEVATION	⊕ [FF800]	FINISHED FLOOR
⊕ [TW800]	TOP OF WALL ELEVATION	====	CG-1 CURB AND GUTTER
⊕ [LP800]	LOW POINT	=====	CG-1 MODIFIED CURB AND GUTTER
⊕ [HP800]	HIGH POINT	R	RAMP
		L	LANDING
		T	TRANSITION
		---	GRADE BREAK

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SWIG - 5959 BARKLEY STREET
 MISSION, JOHNSON COUNTY, KANSAS

FINAL DEVELOPMENT PLANS
 SPOT ELEVATION PLAN

9

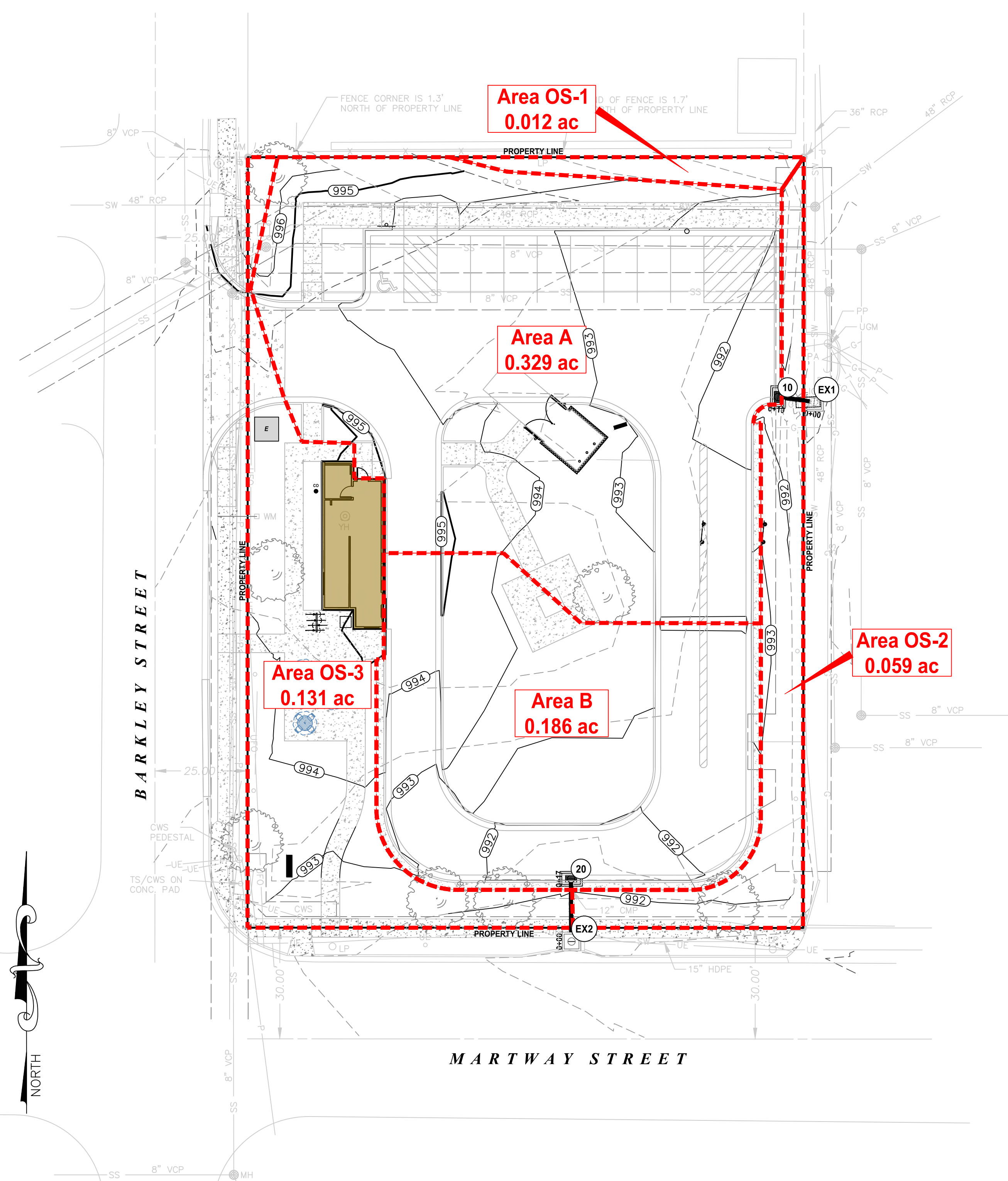
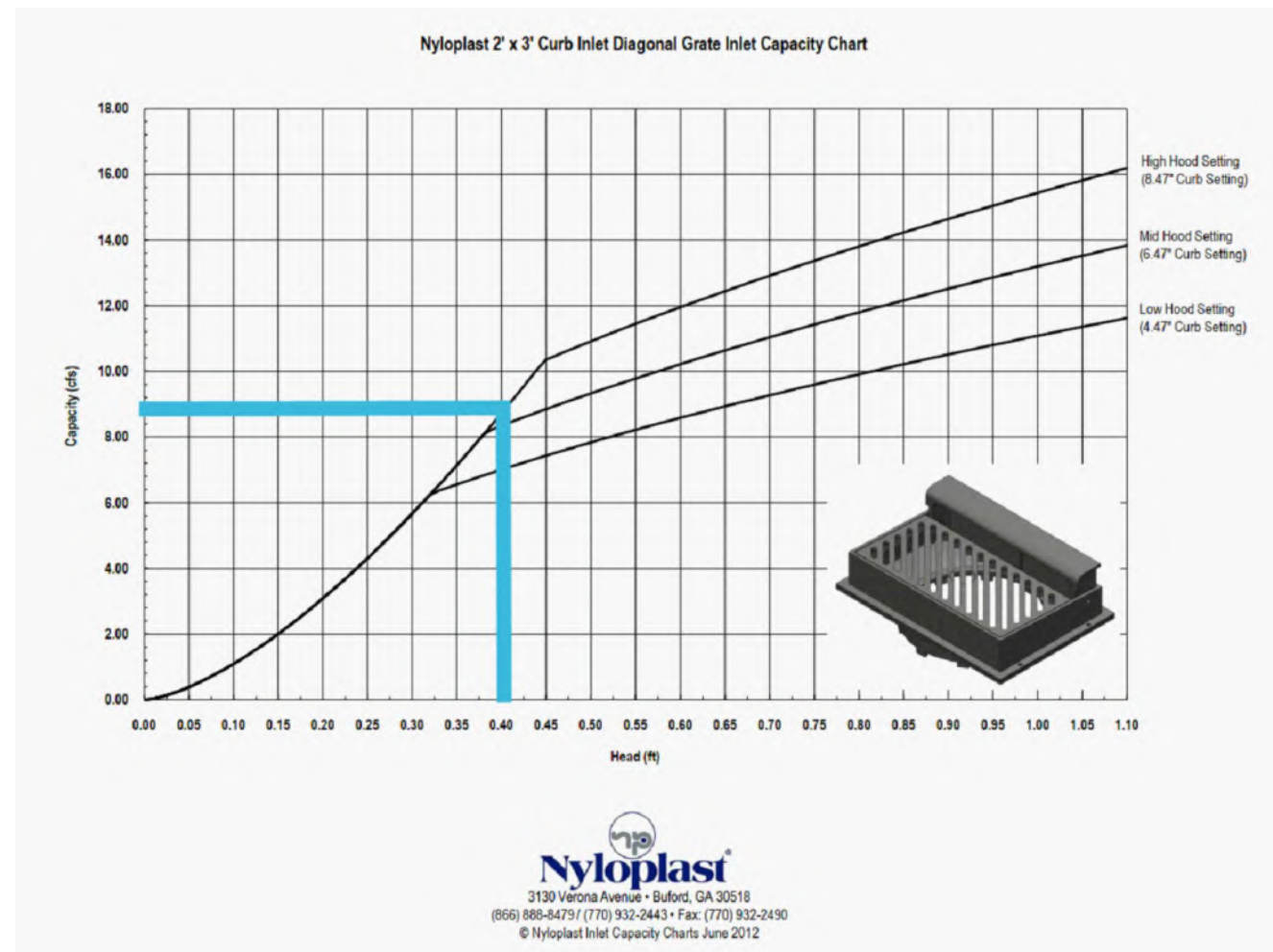
CURB INLET DESIGN TABLE																											
Return Frequency 10 yr										Gutter Capacity					Inlet Capacity												
Inlet Struct. #	Overland Flow (Ti)			Gutter Flow (Ti)			Inlet Time			K	I	Area	Q	Back of Curb to Back of Curb Gutter Type (1 = CG-1 or 2 = CG-2)	Allowable Gutter Spread	Street Cross Slope	Max. Gutter Depth	Max. Gutter Area	Gutter Cap.	Gutter Bypass (positive # = bypass)	Slope	Str. Size	100% Cap(cfs)	80% Cap(cfs)	Inlet Bypass (positive # = bypass)		
	L (ft)	S (%)	C	L (ft)	S (%)	n	Ti (min)	Tt (min)	Tc (min)																	Inlet (in/hr)	Inlet Label
10	100	2	0.9	50	1.5	0.014	2.86	0.17	5.00	1.00	7.35	A	0.33	2.2	28	1	10.5	2.08	0.24	1.04	3.6	-1.4	1.5	2x3 nylc	8.00	6.40	-4.25
20	30	2	0.9	70	1.5	0.014	1.57	0.24	5.00	1.00	7.35	B	0.19	1.2	28	1	10.5	2.08	0.24	1.04	3.6	-2.3	sag	2x3 nylc	8.00	6.40	-5.17

0.51

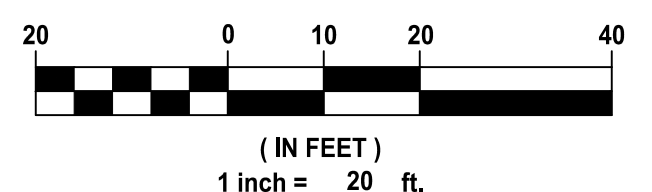
Return Frequency 100 yr																											
Return Frequency 100 yr										Gutter Capacity					Inlet Capacity												
Inlet Struct. #	Overland Flow (Ti)			Gutter Flow (Ti)			Inlet Time			K	I	Area	Q	Back of Curb to Back of Curb Gutter Type (1 = CG-1 or 2 = CG-2)	Allowable Gutter Spread	Street Cross Slope	Max. Gutter Depth	Max. Gutter Area	Gutter Cap.	Gutter Bypass (positive # = bypass)	Slope	Str. Size	100% Cap(cfs)	80% Cap(cfs)	Inlet Bypass (positive # = bypass)		
	L (ft)	S (%)	C	L (ft)	S (%)	n	Ti (min)	Tt (min)	Tc (min)																	Inlet (in/hr)	Inlet Label
10	100	2	0.9	50	1.5	0.014	0.28639	0.1735	5	1.25	10.32	A	0.33	3.8	28	1	10.5	2.08	0.24	1.04	3.6	0.2	1.5	2x3 nylc	8.00	6.40	-2.63
20	30	2	0.9	70	1.5	0.014	0.15686	0.2429	5	1.25	10.32	B	0.19	2.2	28	1	10.5	2.08	0.24	1.04	3.6	-1.4	sag	2x3 nylc	8.00	6.40	-4.24

PIPE DESIGN TABLE																							
Return Frequency 10 yr										Pipe Capacity													
Line	Inlet Struct. #	Inlet Type	Inlet Tc (min)	Pipe Tc (min)	K	C	I	A	A	Pipe dia. (in)	Mannings slope (%)	Pipe Capacity (cfs)	Full Pipe Capacity (cfs)	Full Pipe Velocity (fps)	Grav. Flow Pipe Velocity (fps)	Pressure Flow Pipe Velocity (fps)	Grav. Pipe Flow Depth (feet)	Minor Head Loss Coefficient "K"	Grav. kV/2g (feet)	Pressure kV/2g (feet)	Length to downstream struct. (ft)	Pressure Slope of Friction (%)	
																							Pipe Inlet (in/hr)
1	10	CI	5.00	5.00	1.00	0.90	7.35	0.33	0.33	2.2	12	0.012	1.00	3.9	4.9	5.0	2.7	0.53	1.00	0.40	0.12	10	0.31%
Ex-1	CI																						
2	20	CI	5.00	5.00	1.00	0.90	7.35	0.19	0.19	1.2	12	0.012	1.00	3.9	4.9	4.3	1.6	0.38	1.00	0.29	0.04	17	0.10%
Ex-2	GI																						

Return Frequency 100 yr																							
Return Frequency 100 yr										Pipe Capacity													
Line	Inlet Struct. #	Inlet Type	Tc for pipe calculations (min)	Pipe Tc (min)	K	C	I	A	A	Pipe dia. (in)	Mannings slope (%)	Pipe Capacity (cfs)	Full Pipe Capacity (cfs)	Full Pipe Velocity (fps)	Grav. Flow Pipe Velocity (fps)	Pressure Flow Pipe Velocity (fps)	Grav. Pipe Flow Depth (feet)	Minor Head Loss Coefficient "K"	Grav. kV/2g (feet)	Pressure kV/2g (feet)	Length to downstream struct. (ft)	Pressure Slope of Friction (%)	
																							Pipe Inlet (in/hr)
1	10	CI	5.00	5.00	1.25	0.90	10.32	0.33	0.33	3.8	12	0.012	1.00	3.9	4.9	5.6	4.8	0.79	1.0	0.5	0.36	10	0.95%
Ex-1	CI																						
2	20	CI	5.00	5.00	1.25	0.90	10.32	0.19	0.19	2.2	12	0.012	1.00	3.9	4.9	5.0	2.8	0.53	1.0	0.4	0.12	17	0.31%
Ex-2	GI																						



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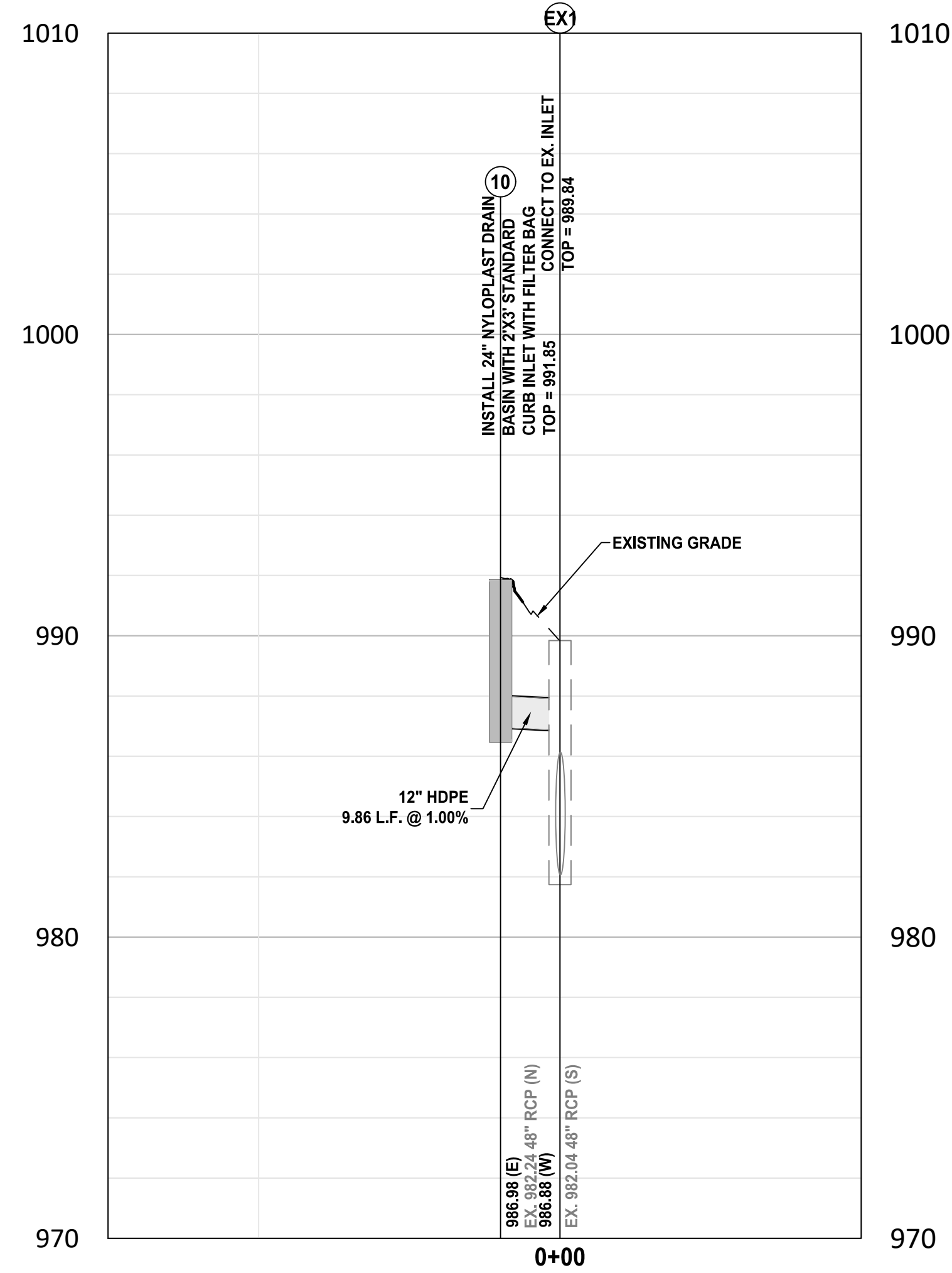
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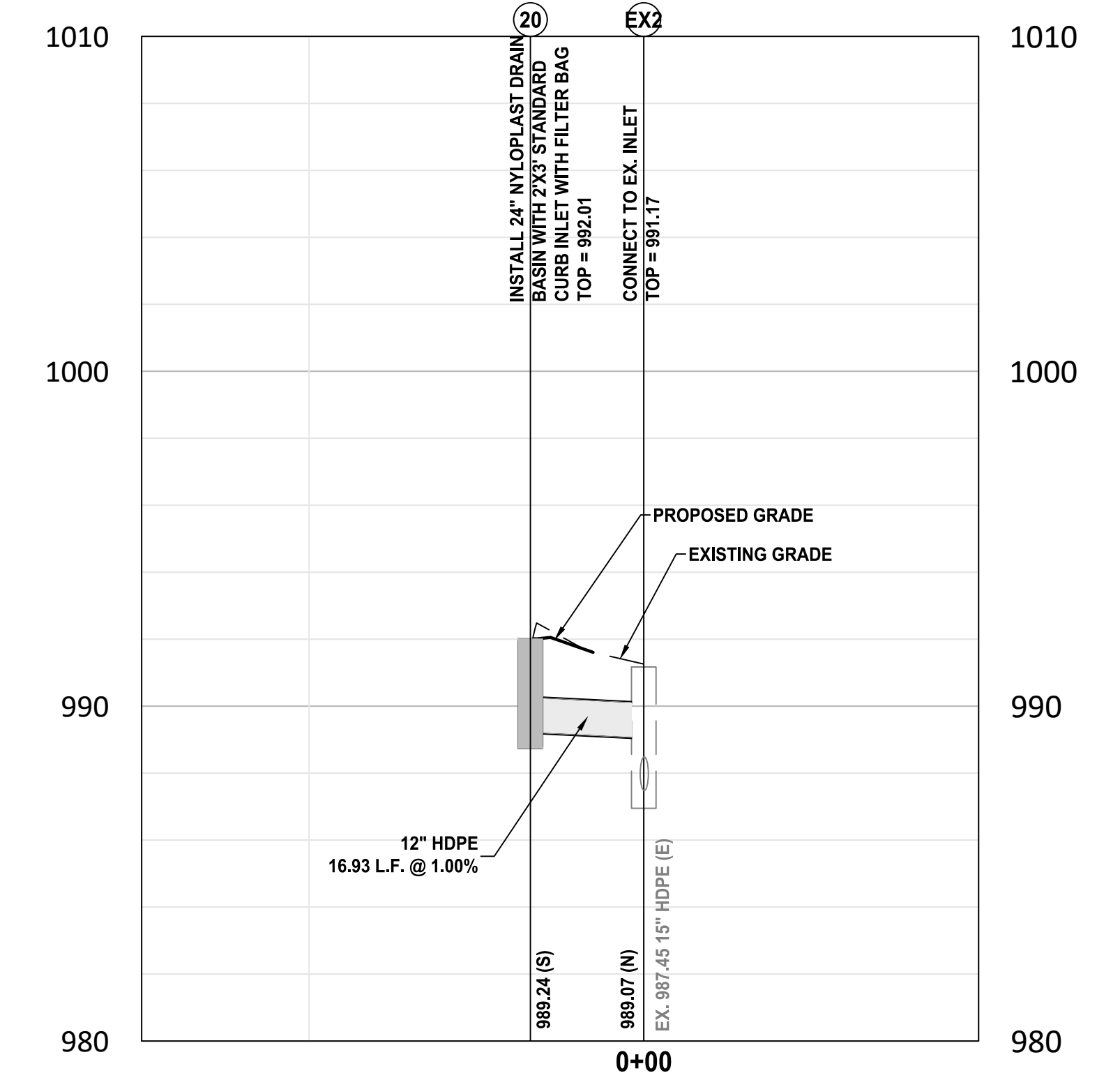


SWIG - 5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS
FINAL DEVELOPMENT PLANS
DRAINAGE AREA MAP & CALCS

LINE 1



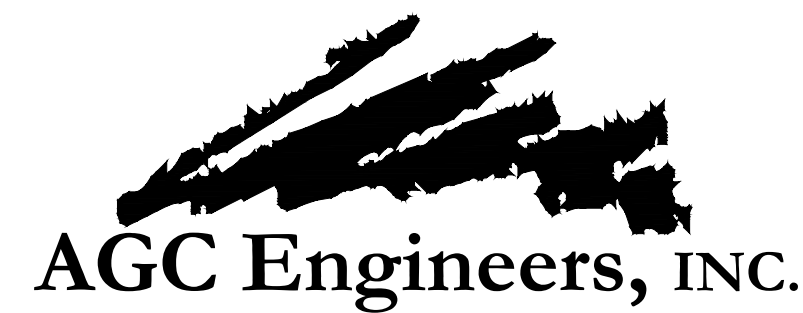
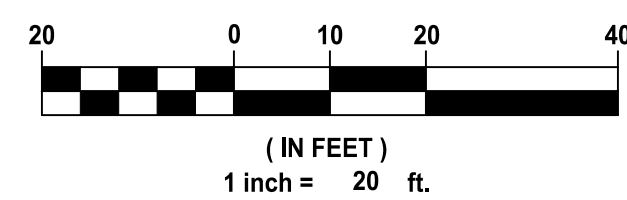
LINE 2



NOTE:

1. ALL PIPE DISTANCES ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
2. FLEXSTORM PURE FILTER BAG INSERTS OR APPROVED EQUAL SHALL BE INSTALLED ON ALL STORM INLETS PER MANUFACTURER'S SPECIFICATIONS. CONTRACTOR SHALL BE RESPONSIBLE TO CLEAN (OR REPLACE) ALL FILTER BAG INSERTS ONCE THE SITE IS PAVED AND VEGETATION IS FULLY ESTABLISHED.
3. ALL INLETS TO HAVE ONE FOOT SUMP.

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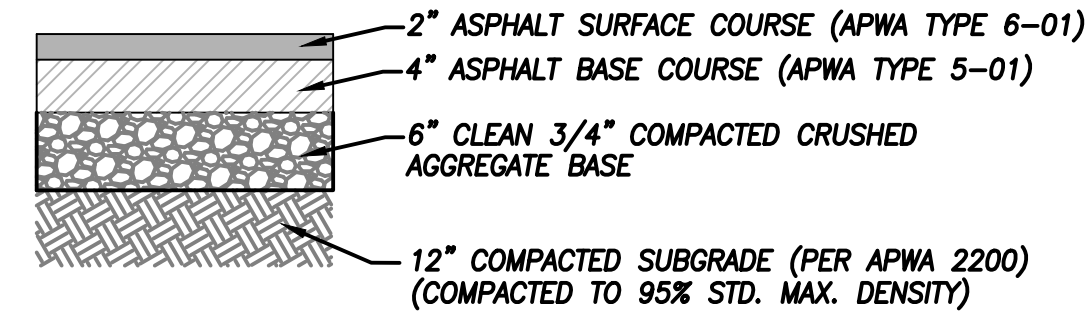
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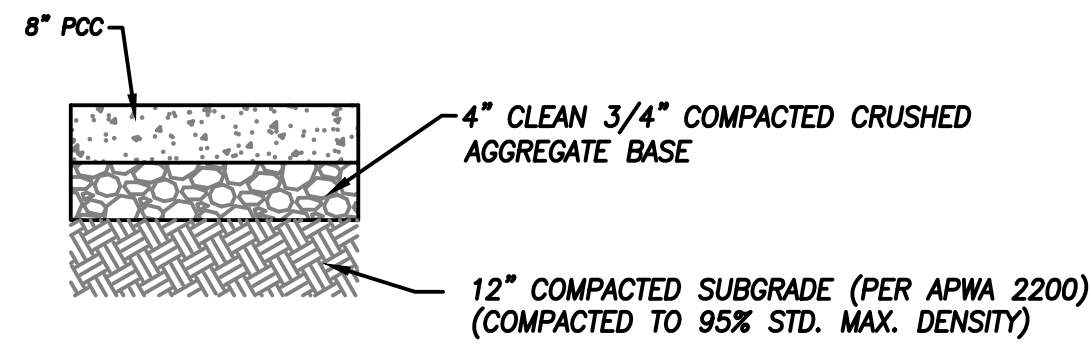
SWIG - 5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS

FINAL DEVELOPMENT PLANS
UTILITY PROFILES



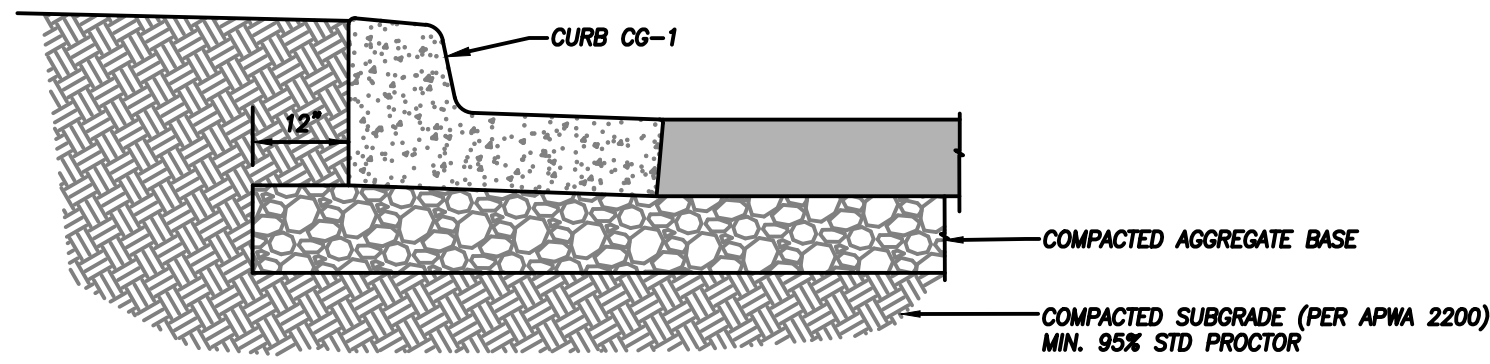
STANDARD DUTY ASPHALT PAVEMENT SECTION

NOT TO SCALE



HEAVY DUTY CONCRETE SECTION

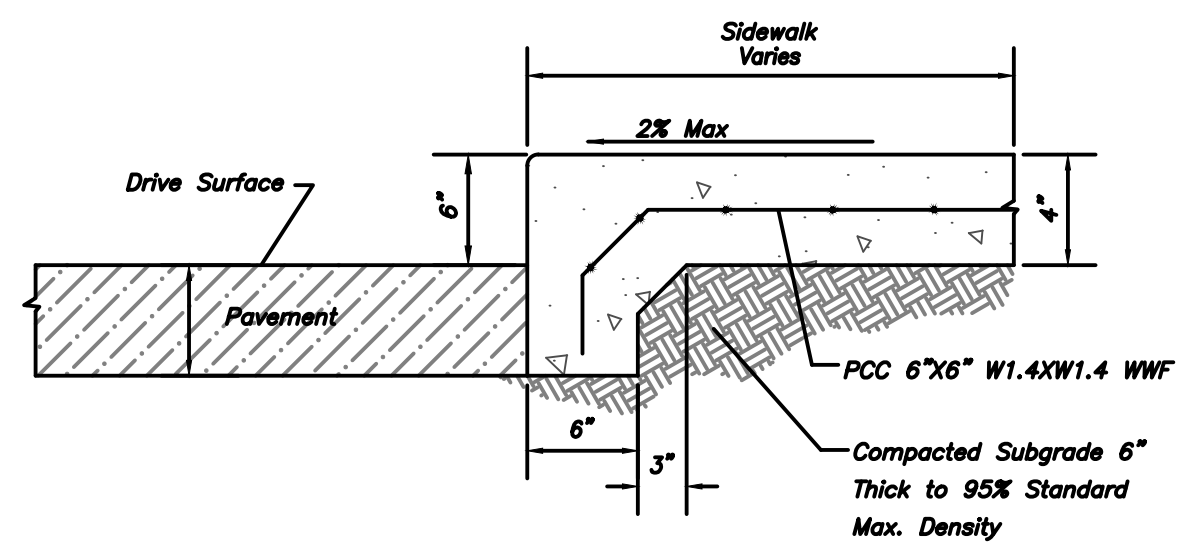
NOT TO SCALE



- EXTEND BASE SECTION 12" BEHIND CURB
- SEE PAVEMENT SECTIONS FOR TYPE & THICKNESS

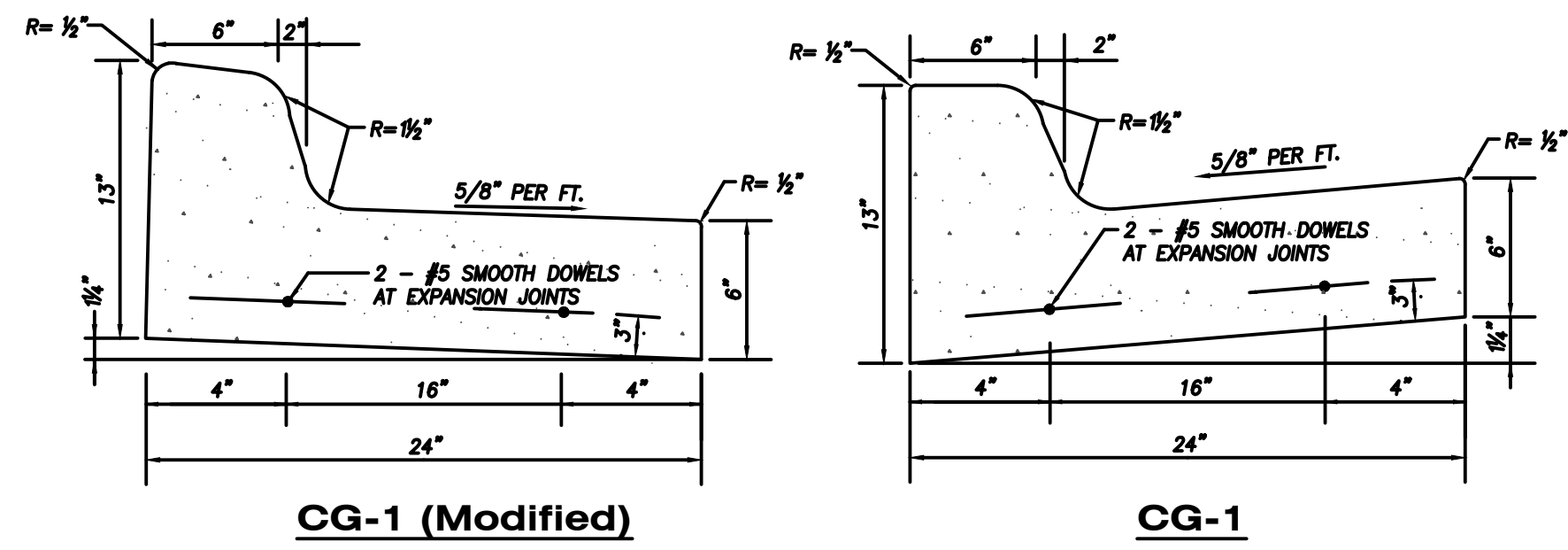
CURB & GUTTER BASE SECTION

NOT TO SCALE



INTEGRAL SIDEWALK / CURB DETAIL

NOT TO SCALE

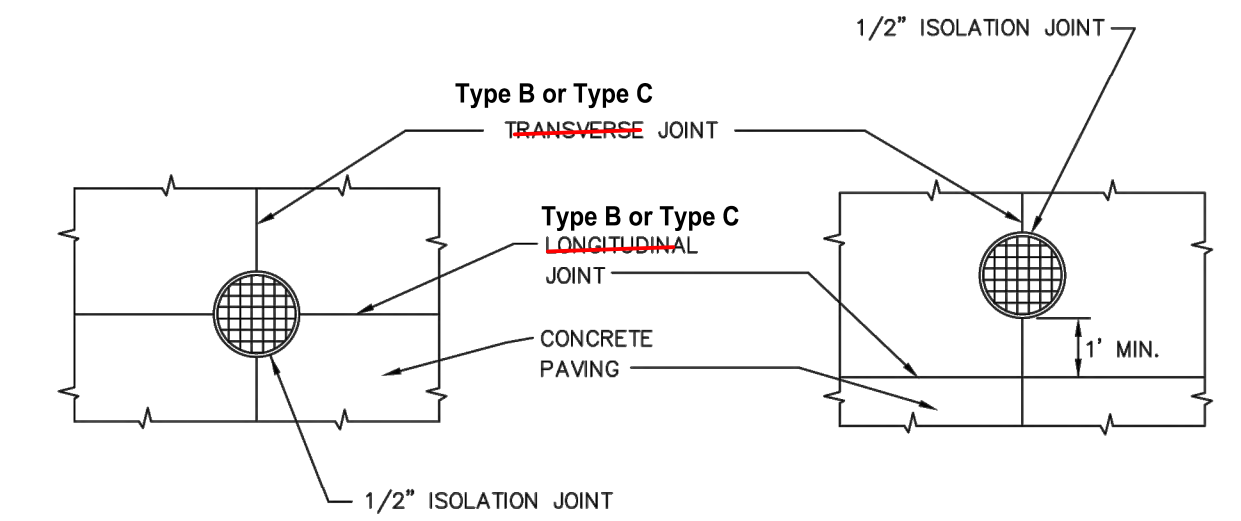
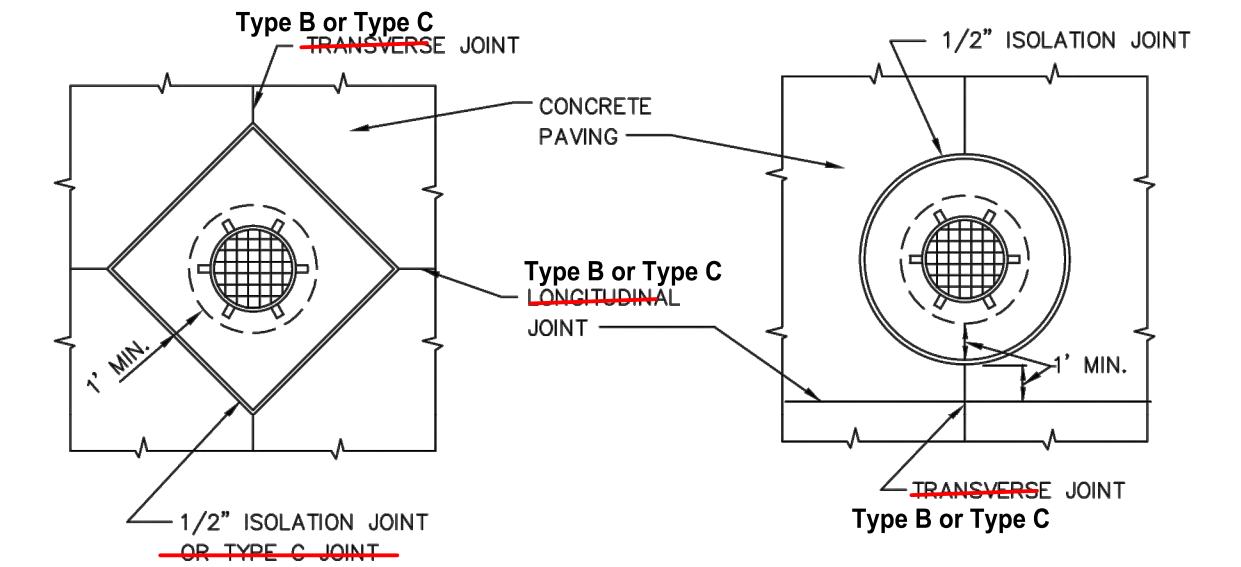


NOTES:

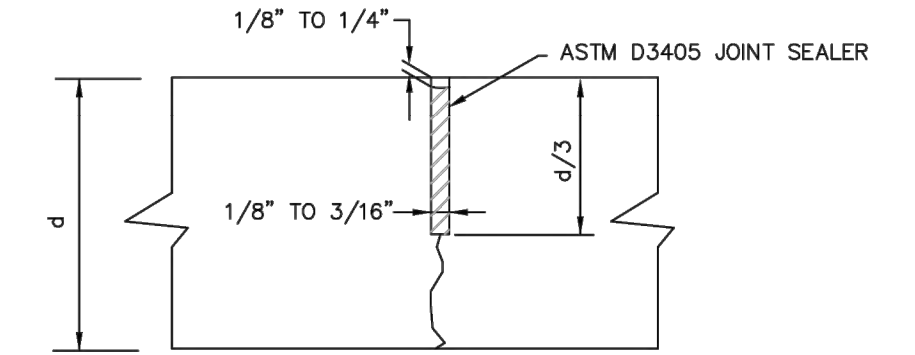
- EXPANSION JOINTS SHALL BE FORMED BY A ONE-HALF (1/2) INCH THICK PREFORMED JOINT FILLER, CUT TO THE CONFIGURATION OF THE FULL SIZE OF THE CURB AND GUTTER SECTION AND BEING SECURED SO THAT THEY ARE NOT MOVED BY DEPOSITING AND COMPACTING THE CONCRETE AT THESE JOINTS. THE EDGES OF THESE JOINTS SHALL BE ROUNDED WITH AN EDGING TOOL ONE-EIGHTH (1/8) INCH RADIUS.
- EXPANSION JOINTS SHALL BE PLACED WHERE CURB AND GUTTER ABUTS OTHER STRUCTURES AND AT ALL TANGENT POINTS TO CURBS. EXPANSION JOINTS SHALL NOT BE SPACED MORE THAN 50 FEET APART ON STRAIGHT RUNS FOR HAND LAID CURB AND GUTTER AND NOT MORE THAN 100 FEET APART FOR MACHINE LAID CURB AND GUTTER PROVIDED 3/4 INCH THICK JOINT FILLER IS USED. ALL JOINTS SHALL BE FORMED AT RIGHT ANGLES TO THE ALIGNMENT OF THE CURB AND GUTTER.
- CONTRACTION JOINTS SHALL BE CONSTRUCTED BY SAWING THROUGH THE CURB AND GUTTER TO A DEPTH OF NOT LESS THAN ONE AND ONE-FOURTH (1 1/4) INCHES BELOW THE SURFACE AND TO A WIDTH NOT TO EXCEED THREE-EIGHTHS (3/8) INCH OR THEY MAY BE FORMED BY INSERTING A REMOVABLE METAL TEMPLATE IN THE FRESH CONCRETE, OR BY OTHER METHODS APPROVED BY THE ENGINEER. SEALING OF JOINTS IS NOT REQUIRED. CONTRACTION OR CONSTRUCTION JOINTS SHALL BE LOCATED APPROXIMATELY 10 FEET APART.
- EXTEND 6" THICK AGGREGATE BASE MINIMUM 12" BEHIND BACK OF CURB.

CURB AND GUTTER

NOT TO SCALE



CONCRETE PAVING ISOLATION JOINTS

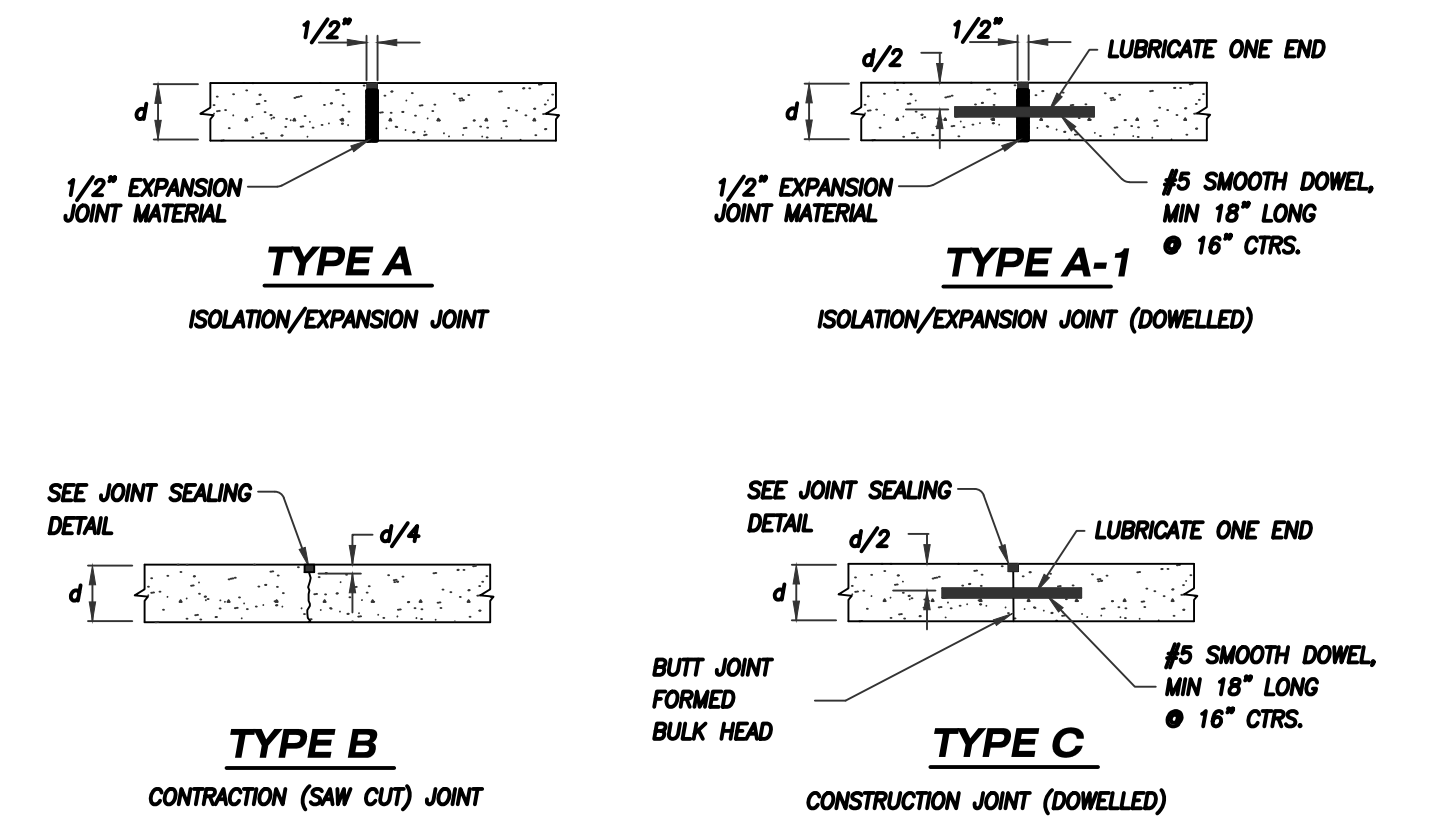


TYPICAL SECTION

NOTES:

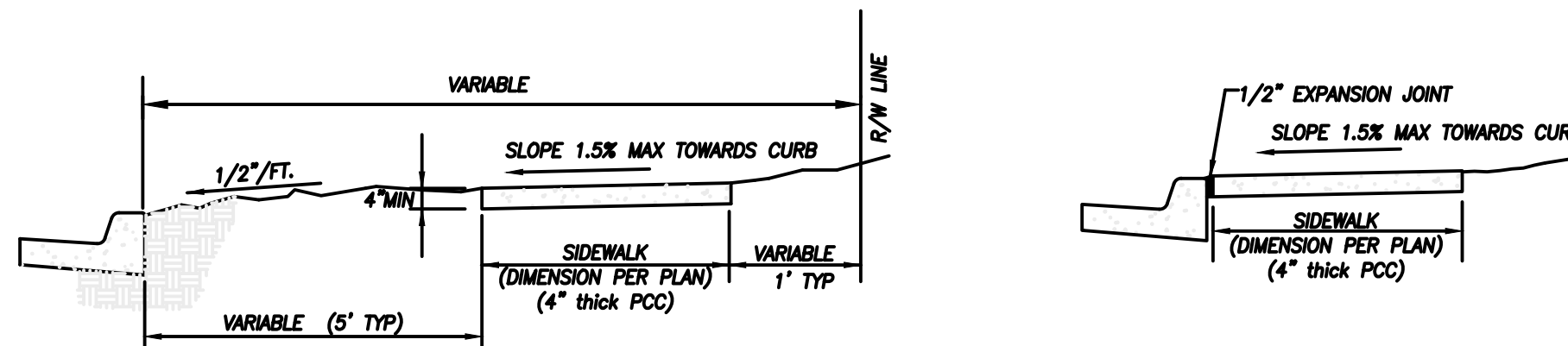
- JOINT SEALING MATERIAL SHALL CONFORM TO ASTM D3405 AND SHALL BE APPLIED IN ACCORDANCE WITH MATERIAL MANUFACTURER'S RECOMMENDATIONS.
- d = DEPTH OF SLAB

CONCRETE PAVING JOINT SEALING



CONCRETE JOINT DETAILS

NOT TO SCALE



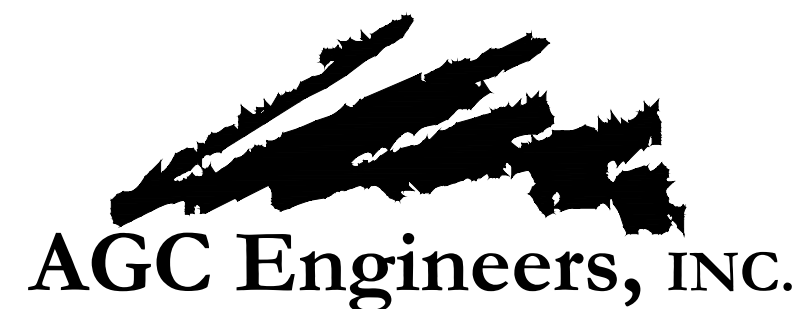
NOTES:

- JOINTS SHALL BE FORMED AT RIGHT ANGLES TO THE ALIGNMENT OF THE SIDEWALK AND TO THE DEPTHS INDICATED BELOW.
- THE SIDEWALK SHALL BE MARKED OFF INTO SQUARE SECTIONS (PICTURE FRAMED) BY CONTRACTION JOINTS. CONTRACTION JOINTS SHALL BE ONE-EIGHTH (1/8) INCH WIDE BY ONE (1) INCH DEEP AND SHALL BE FORMED BY TOOLING.
- EXPANSION JOINTS SHALL BE FORMED BY A ONE-HALF (1/2) INCH THICK PREFORMED JOINT FILLER, EXTENDING THE FULL DEPTH OF THE SLAB, AND SECURED SO THAT THEY ARE NOT MOVED BY DEPOSITING AND COMPACTING THE CONCRETE AT THESE JOINTS.
- EXPANSION JOINTS SHALL BE PLACED WHERE SIDEWALK ABUTS OTHER STRUCTURES AND SHALL NOT BE SPACED MORE THAN 50 FT APART ON STRAIGHT RUNS FOR HAND LAID SIDEWALK AND NOT MORE THAN 100 FT APART ON STRAIGHT RUNS FOR MACHINE LAID SIDEWALKS.
- SIDEWALK TO BE INSTALLED ON COMPACTED SUBGRADE (MIN 95% STD PROCTOR). CONTRACTOR MAY ELECT TO INSTALL AGGREGATE LEVEL COURSE.

SIDEWALK DETAILS

NOT TO SCALE

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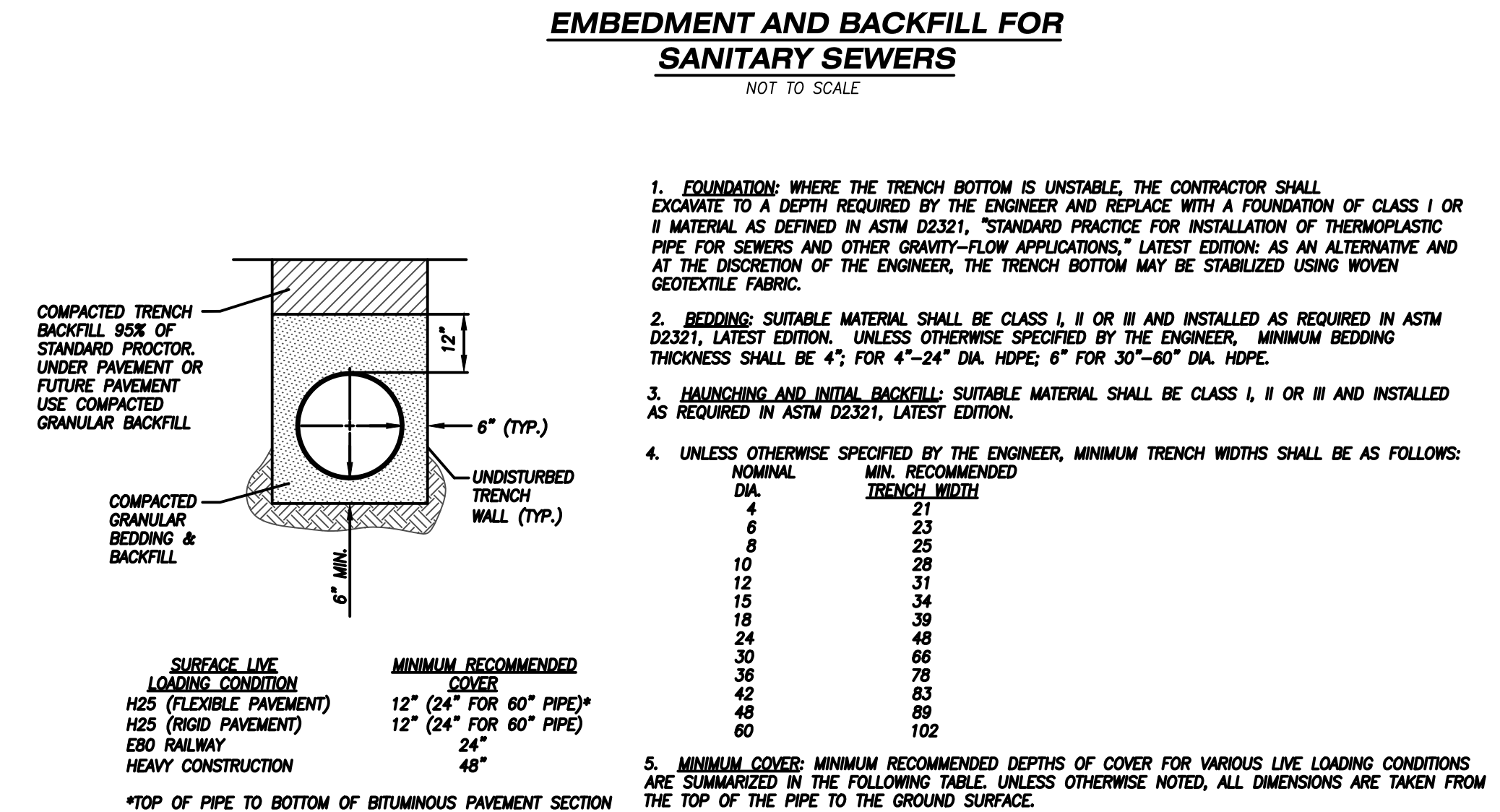
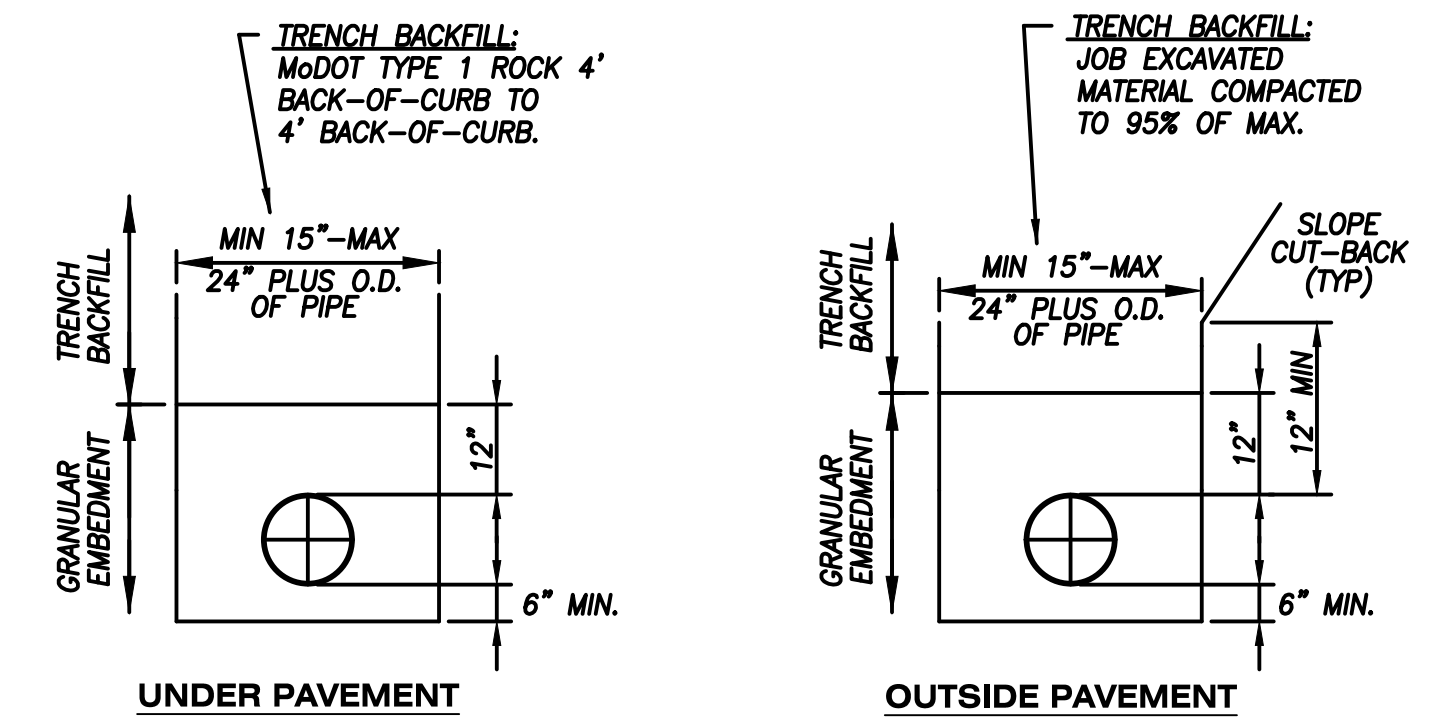
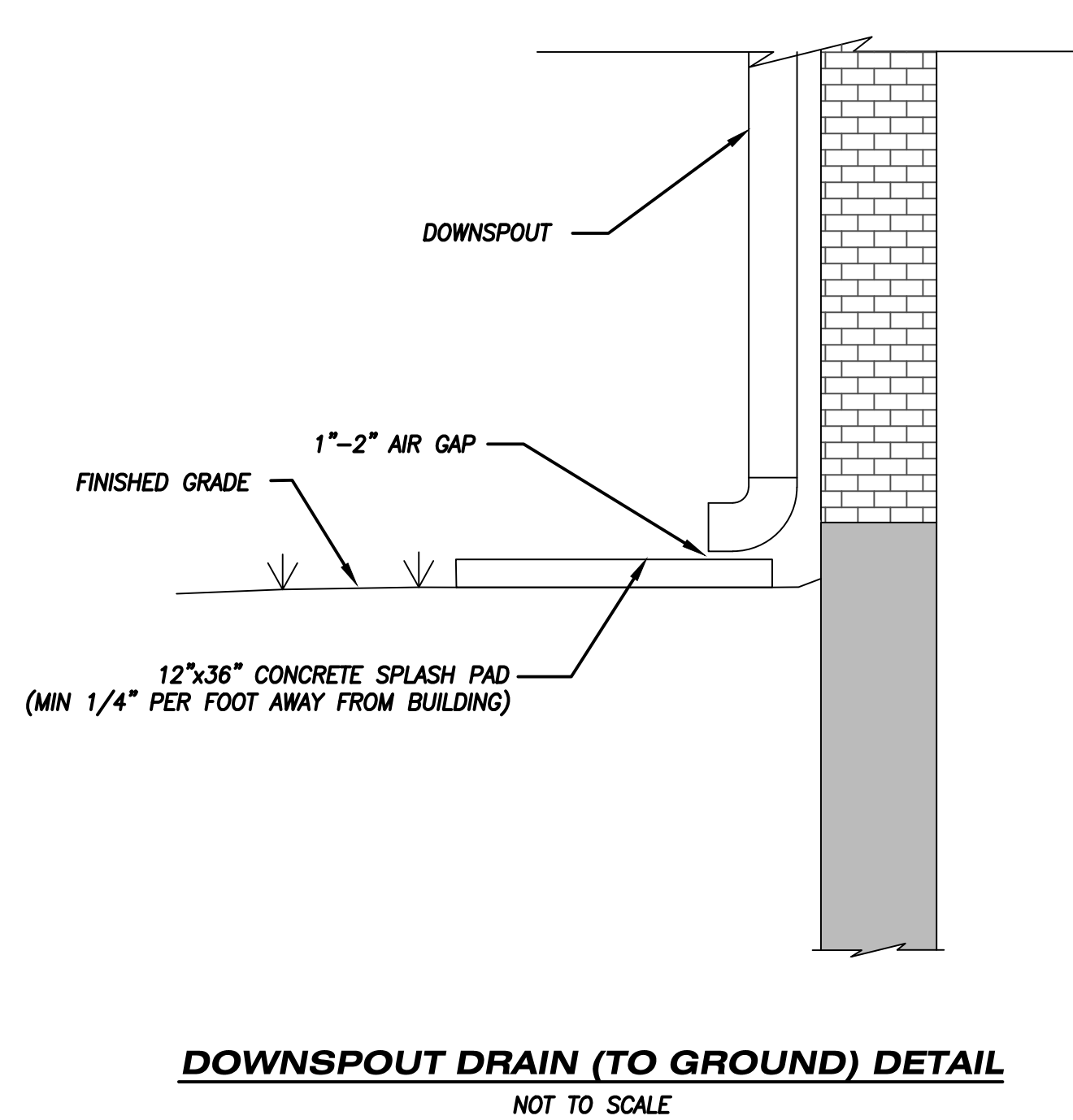
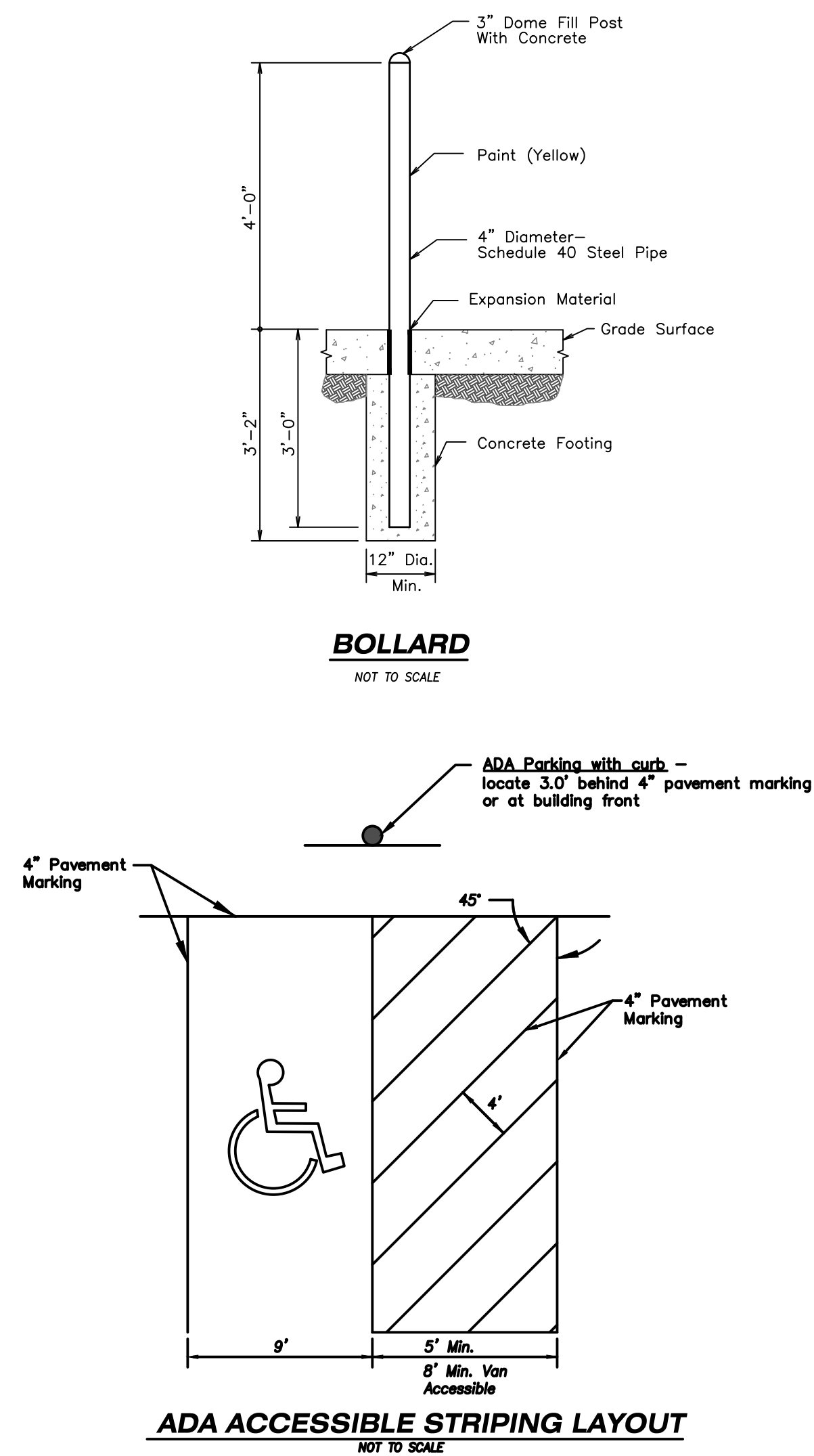
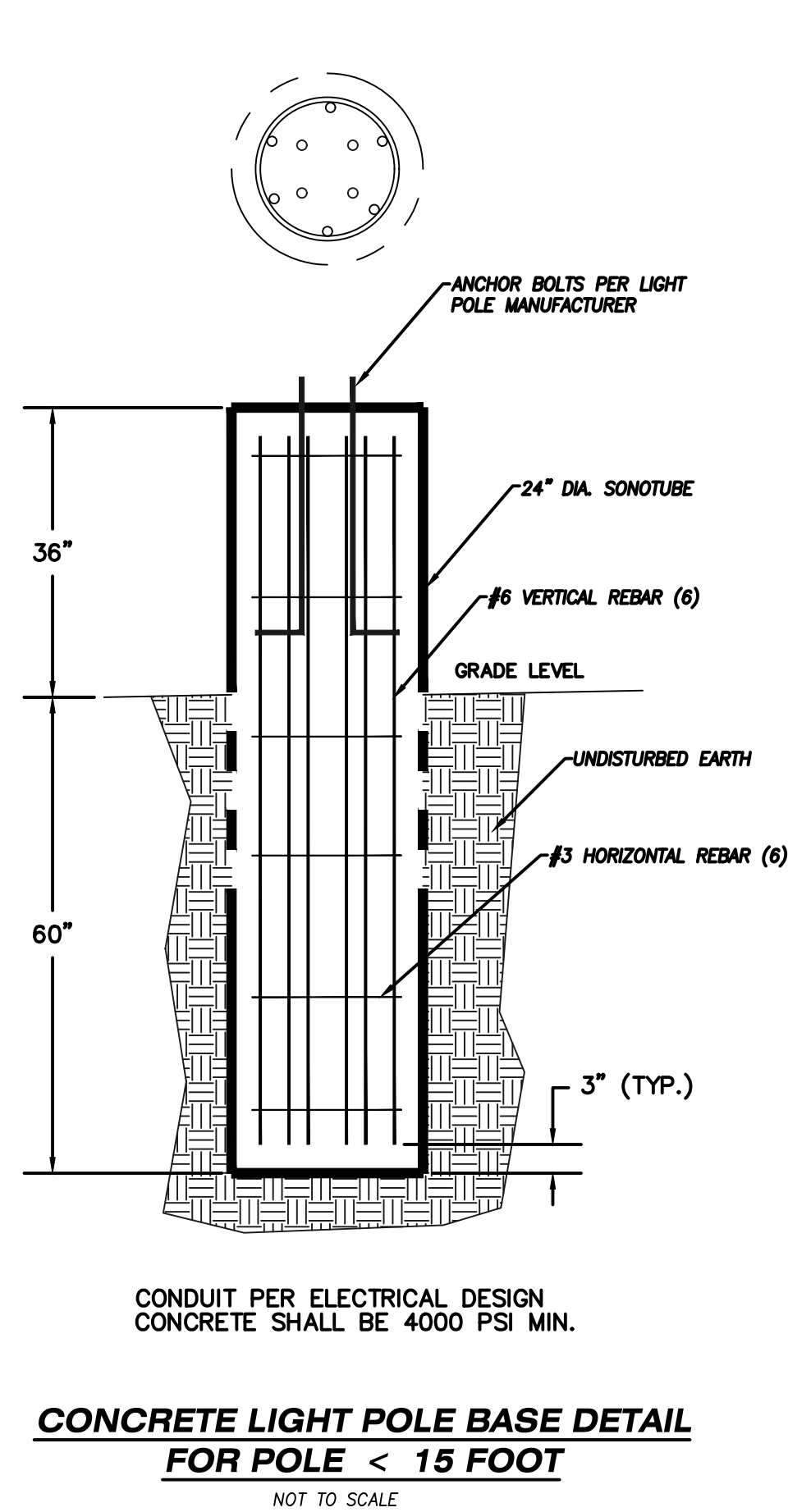
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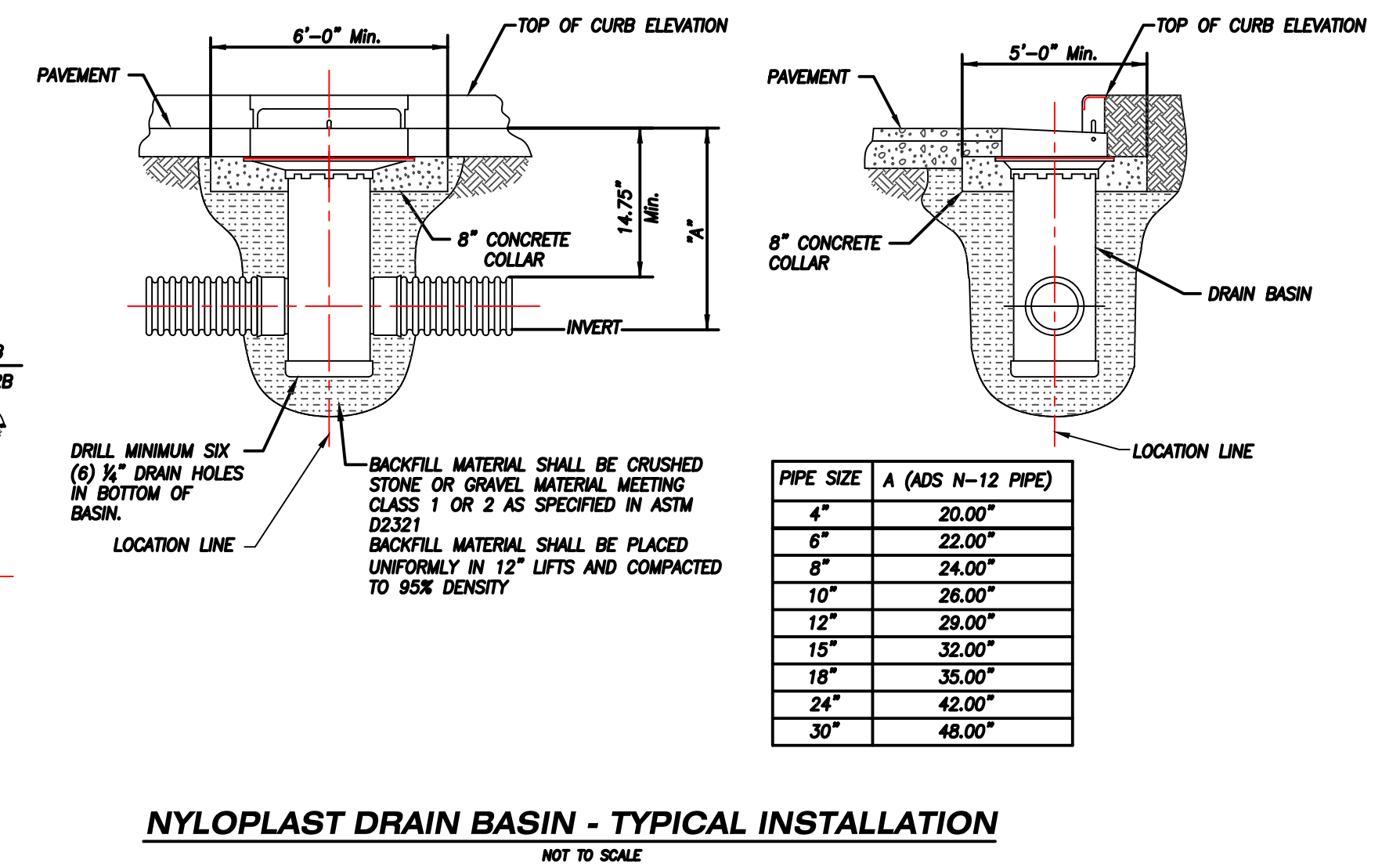
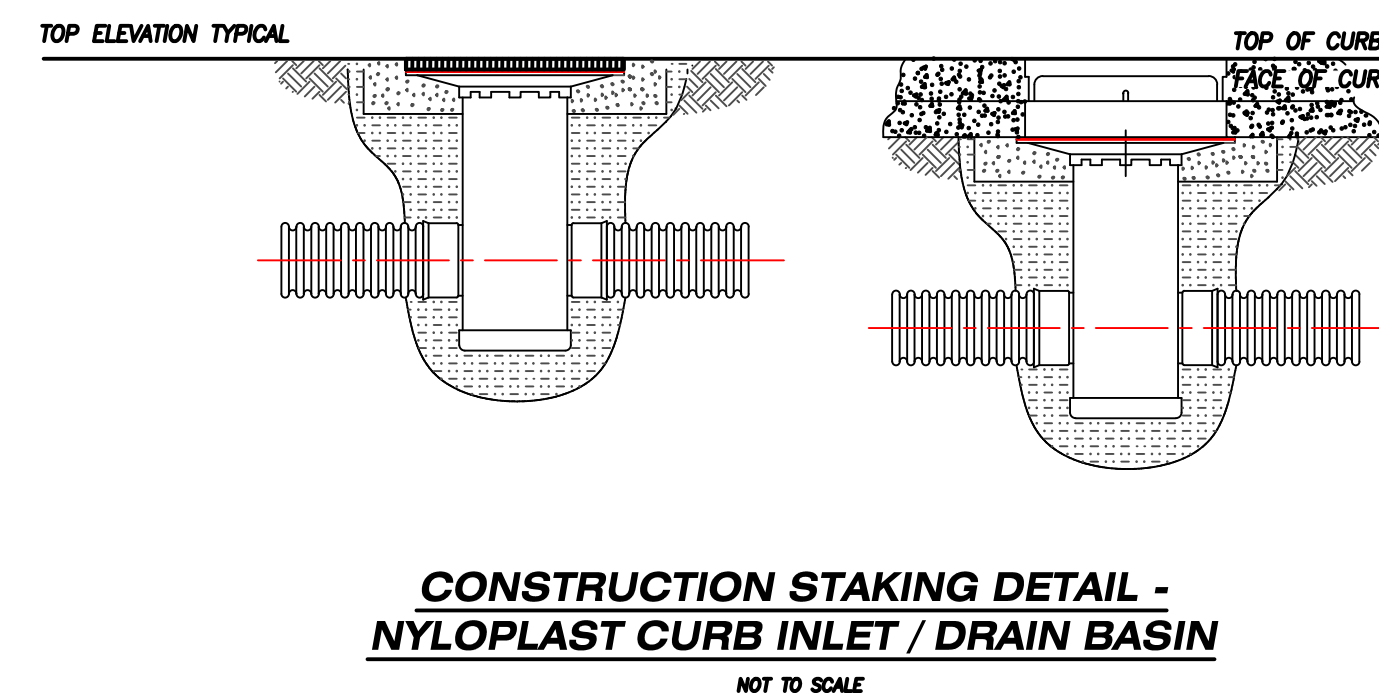
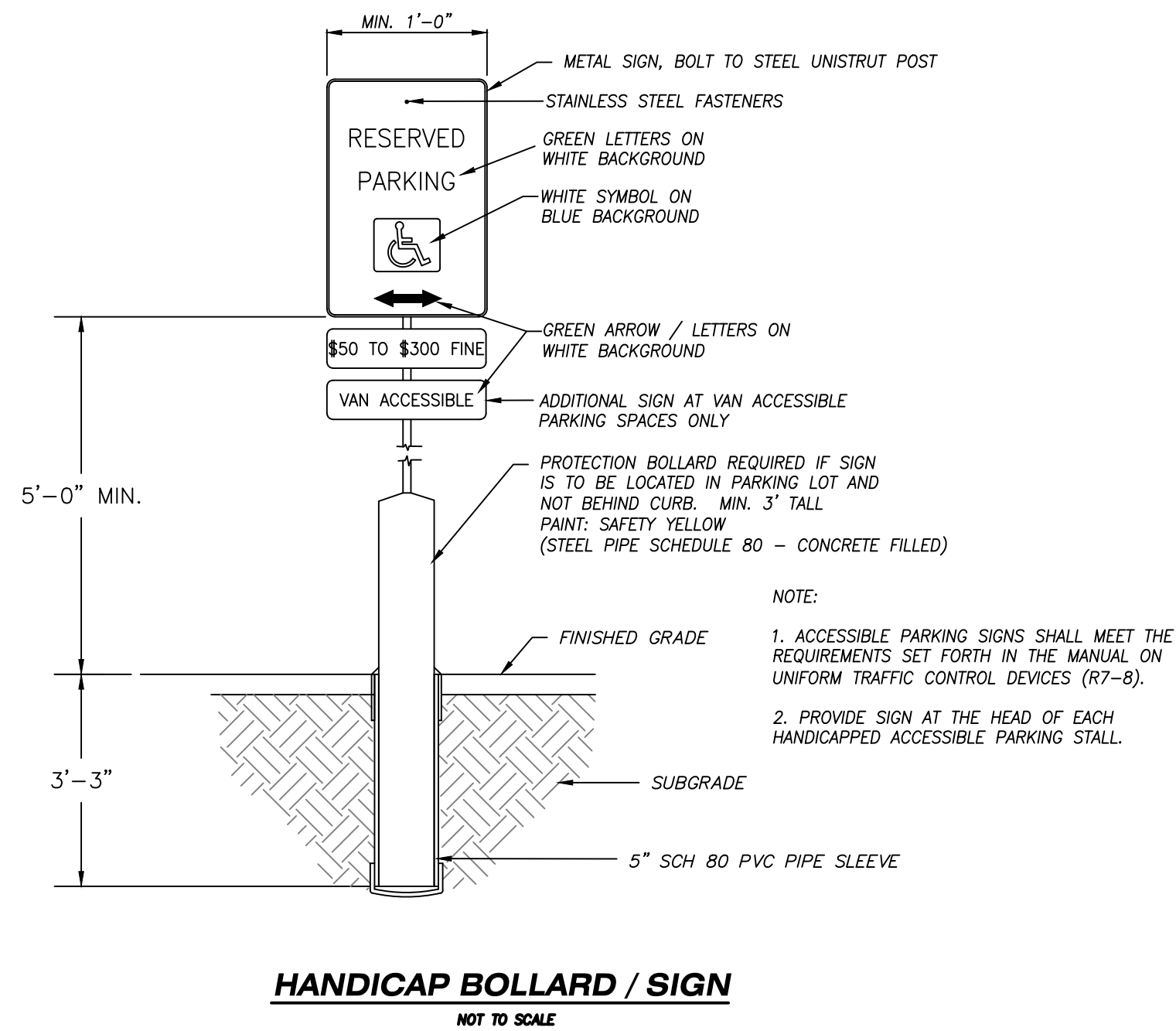
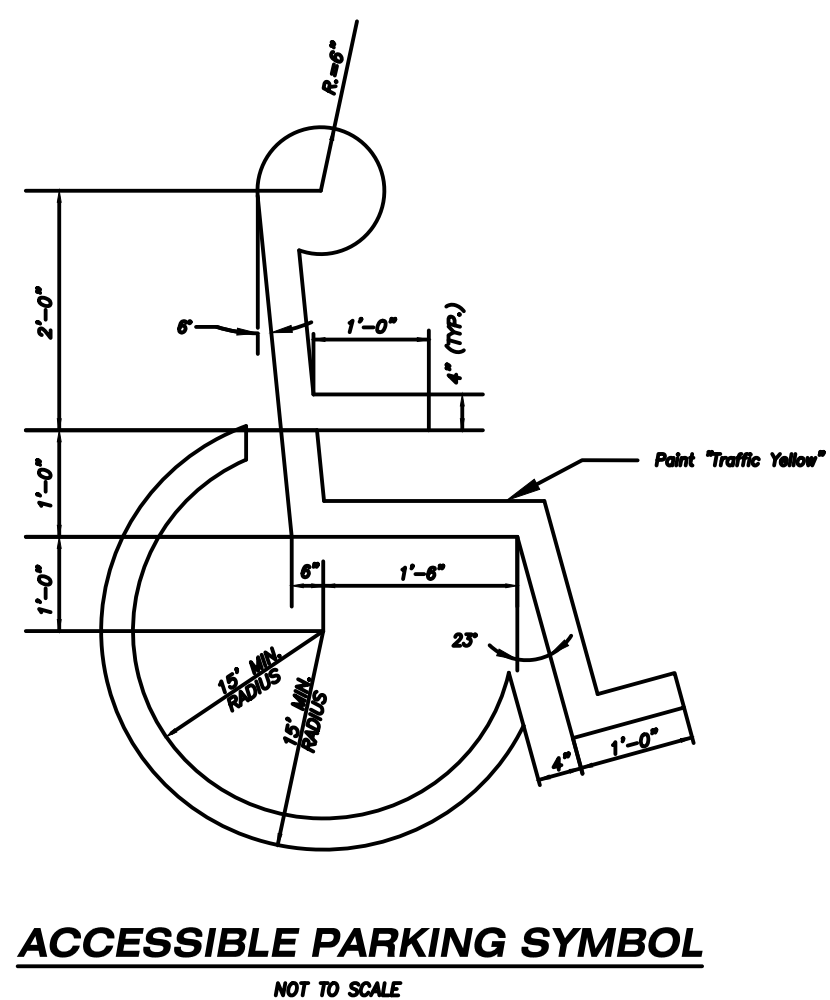
FINAL DEVELOPMENT PLANS
DETAILS



- FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH A FOUNDATION OF CLASS I OR II MATERIAL AS DEFINED IN ASTM D2321, "STANDARD PRACTICE FOR INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY-FLOW APPLICATIONS," LATEST EDITION; AS AN ALTERNATIVE AND AT THE DISCRETION OF THE ENGINEER, THE TRENCH BOTTOM MAY BE STABILIZED USING WOVEN GEOTEXTILE FABRIC.
- BEDDING: SUITABLE MATERIAL SHALL BE CLASS I, II OR III AND INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION, UNLESS OTHERWISE SPECIFIED BY THE ENGINEER. MINIMUM BEDDING THICKNESS SHALL BE 4"; FOR 4"-24" DIA. HDPE; 6" FOR 30"-60" DIA. HDPE.
- HAUNCHING AND INITIAL BACKFILL: SUITABLE MATERIAL SHALL BE CLASS I, II OR III AND INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION.
- UNLESS OTHERWISE SPECIFIED BY THE ENGINEER, MINIMUM TRENCH WIDTHS SHALL BE AS FOLLOWS:

NOMINAL DIA.	MIN. RECOMMENDED TRENCH WIDTH
4	21
6	23
8	25
10	28
12	31
15	34
18	39
24	48
30	66
36	78
42	83
48	89
60	102
- MINIMUM COVER: MINIMUM RECOMMENDED DEPTHS OF COVER FOR VARIOUS LIVE LOADING CONDITIONS ARE SUMMARIZED IN THE FOLLOWING TABLE. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TAKEN FROM THE TOP OF THE PIPE TO THE GROUND SURFACE.

SURFACE LIVE LOADING CONDITION	MINIMUM RECOMMENDED COVER
H25 (FLEXIBLE PAVEMENT)	12" (24" FOR 60" PIPE)*
H25 (RIGID PAVEMENT)	12" (24" FOR 60" PIPE)
E80 RAILWAY	24"
HEAVY CONSTRUCTION	48"



PIPE SIZE	A (ADS N-12 PIPE)
4"	20.00"
6"	22.00"
8"	24.00"
10"	26.00"
12"	29.00"
15"	32.00"
18"	35.00"
24"	42.00"
30"	48.00"

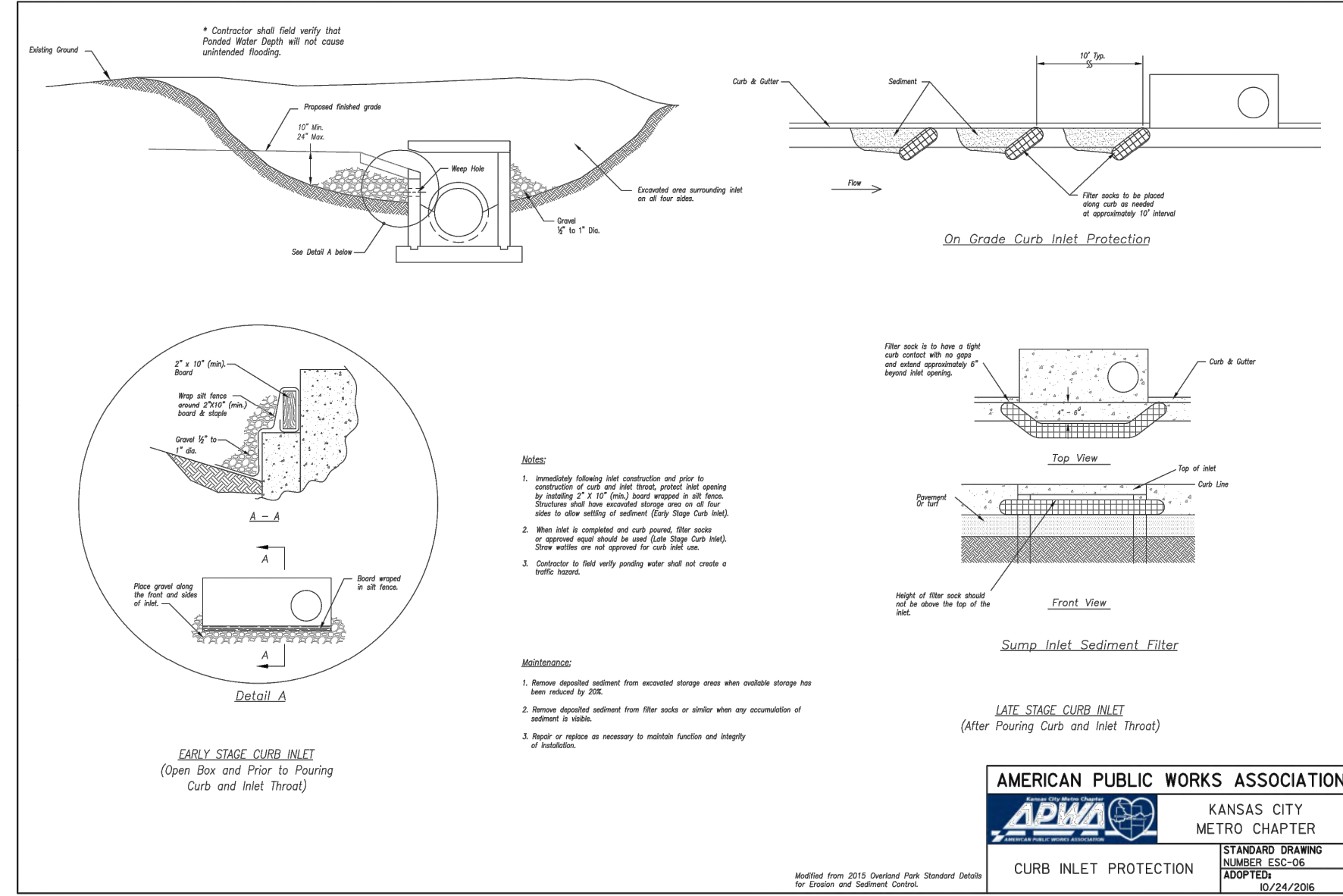
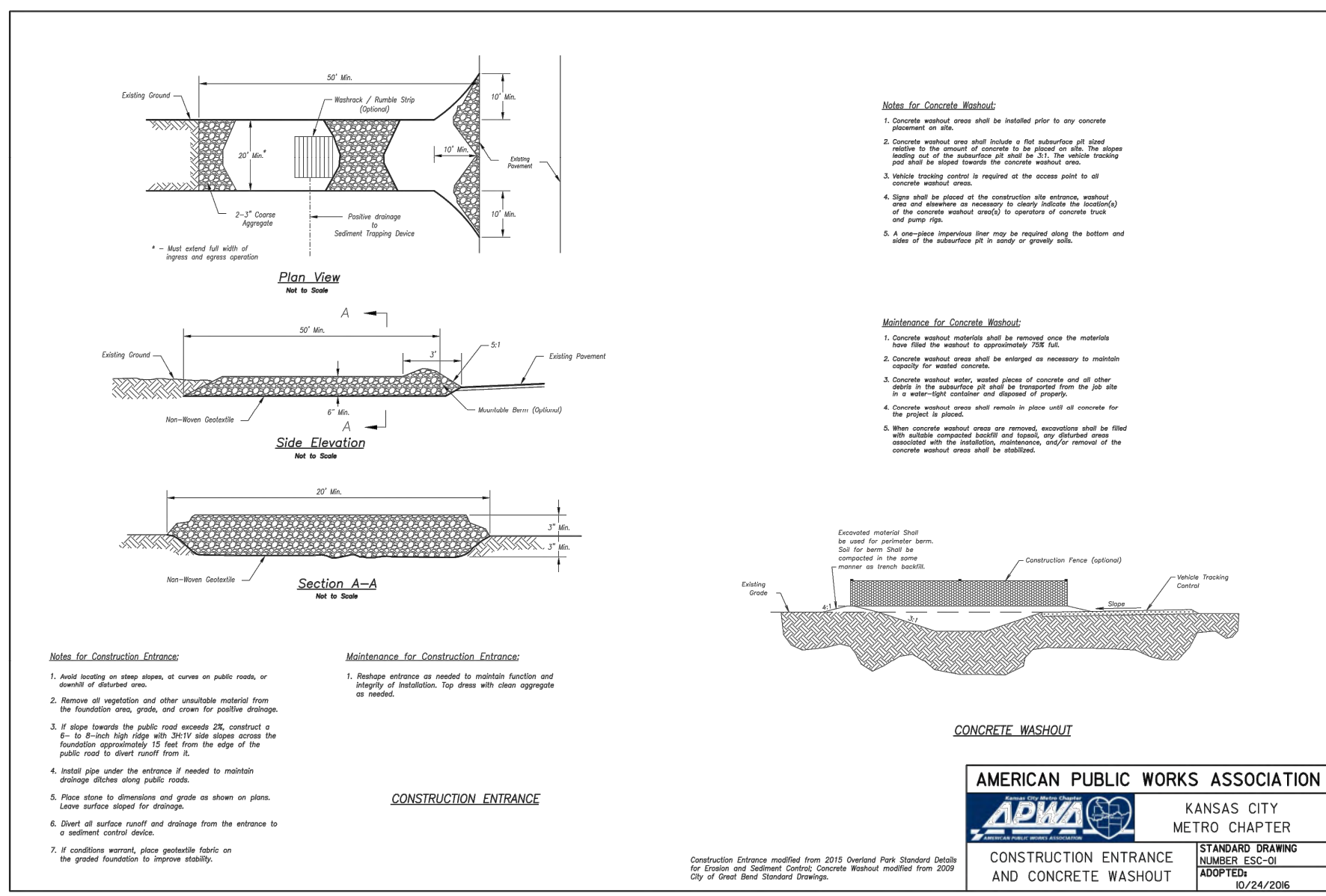
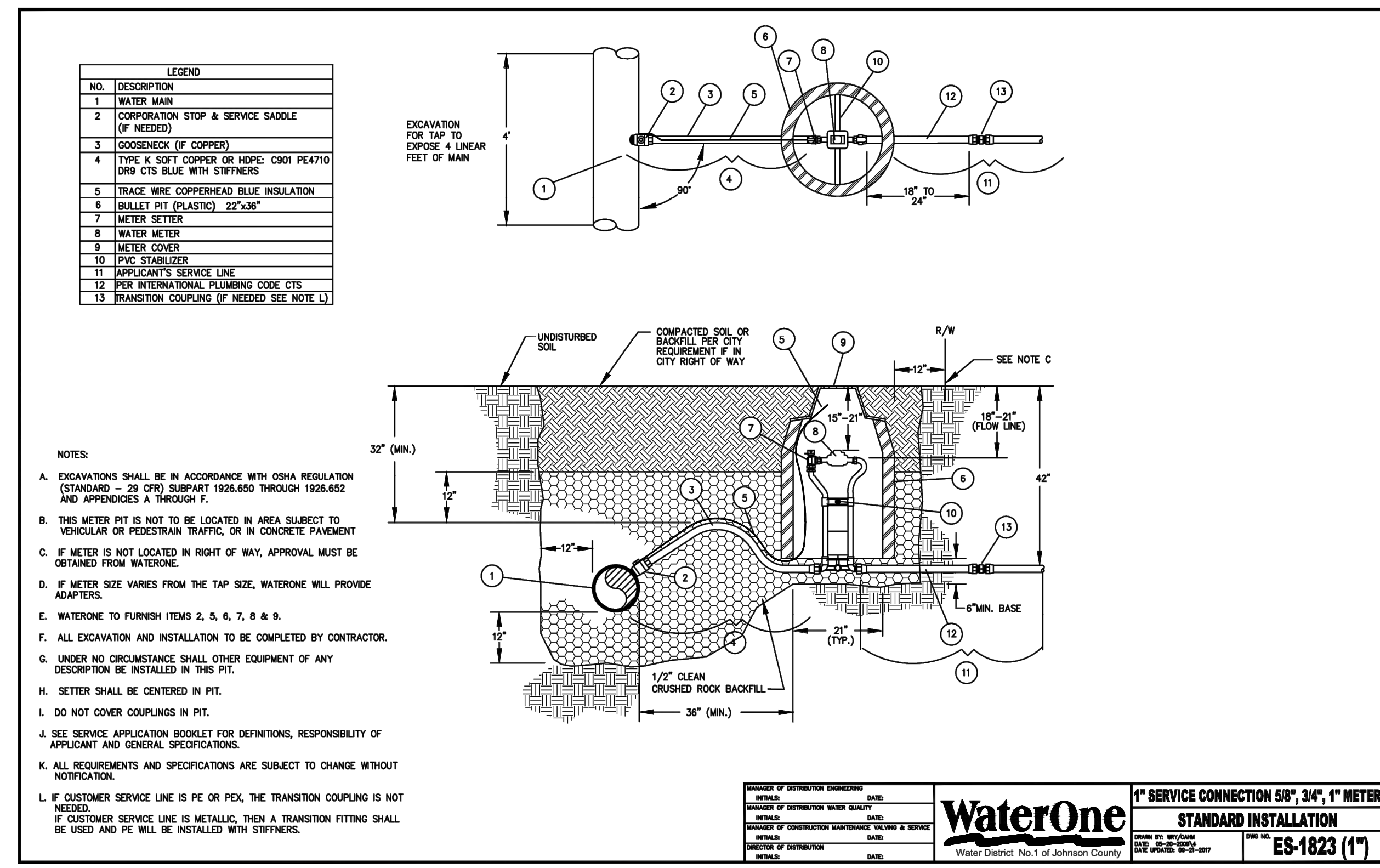
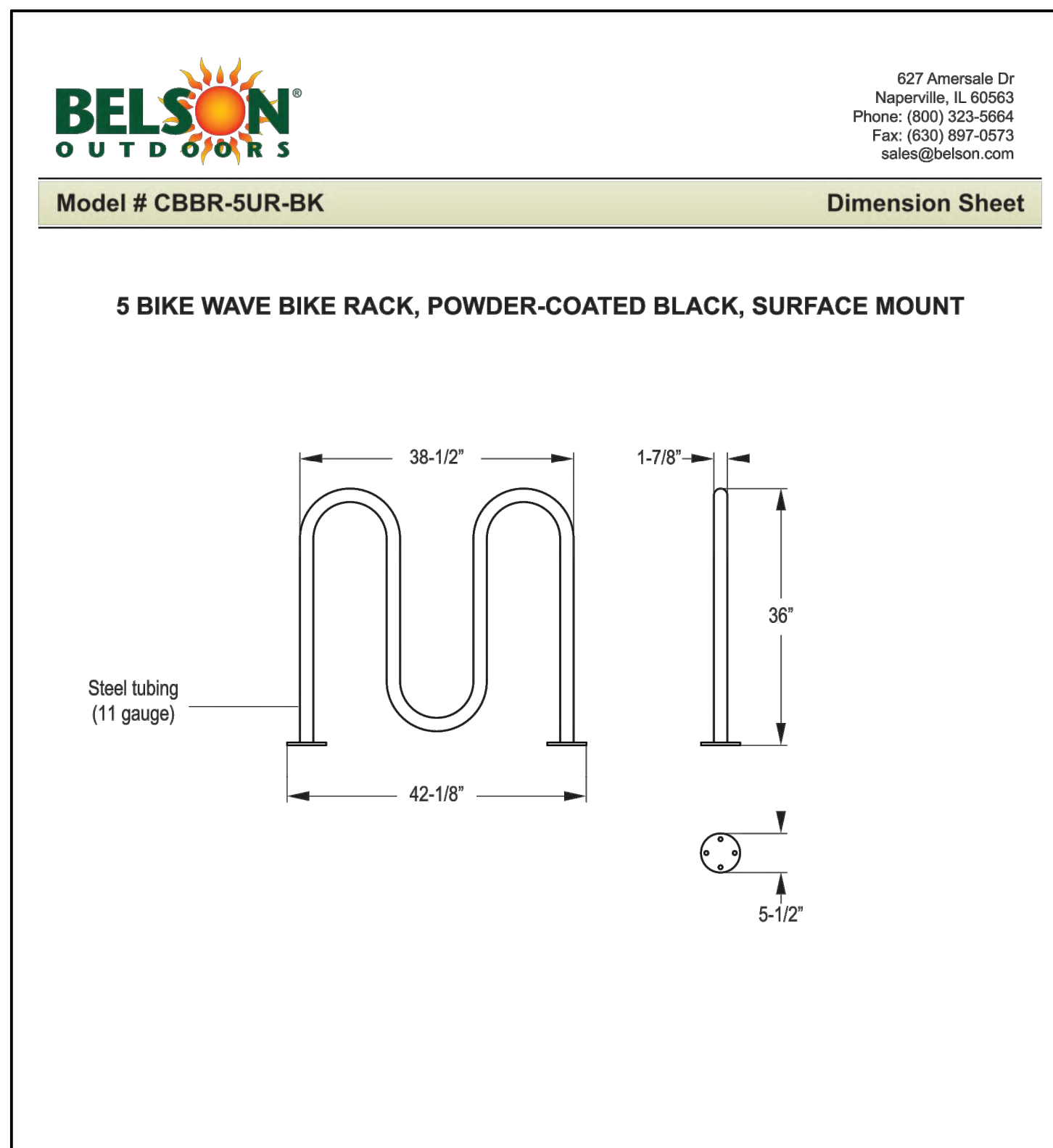
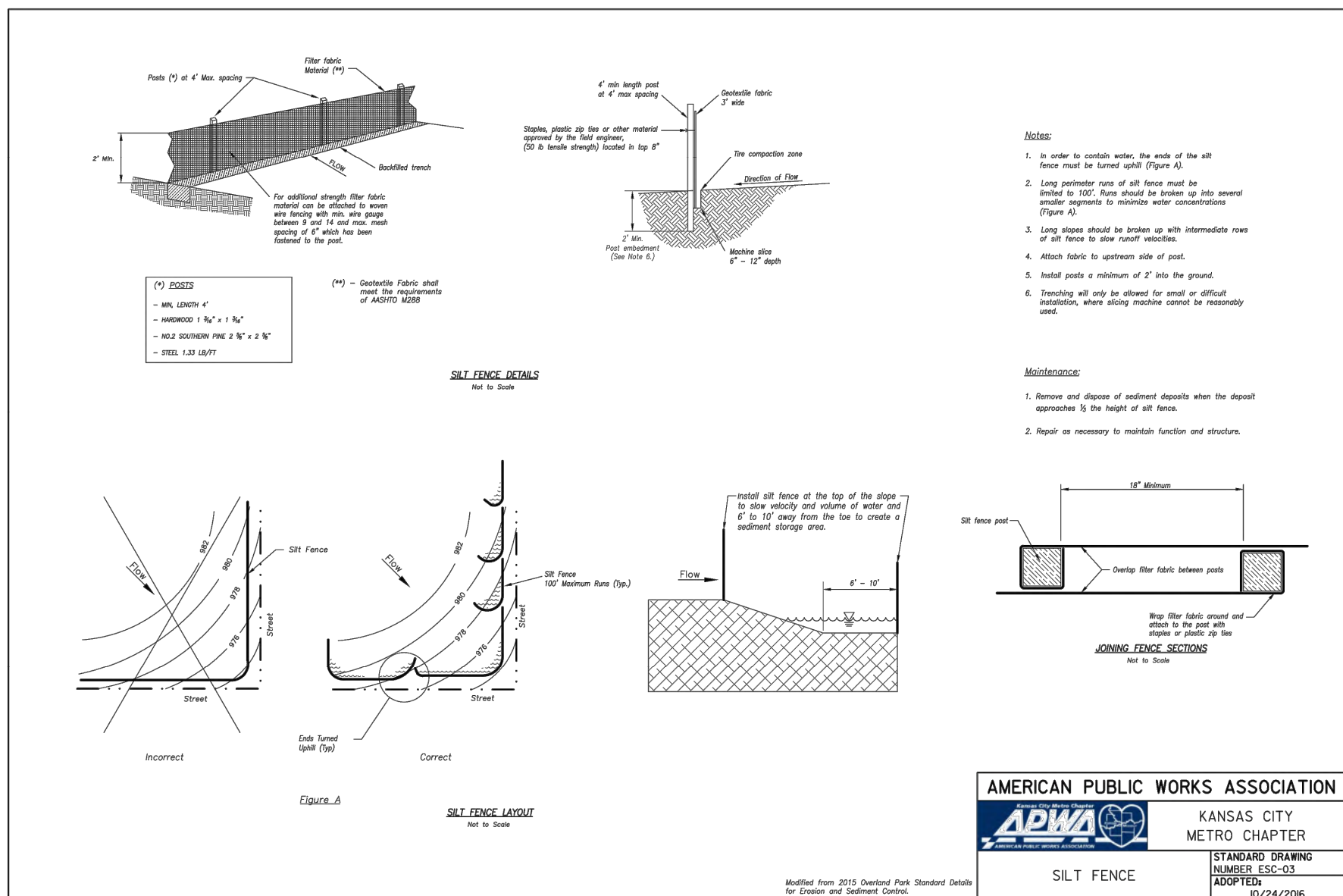
BY	REVISION	DATE
ACA	ADDRESS CITY COMMENTS DATED 8-23-23	8-31-23
ACA	SUBMITTED TO CITY FOR REVIEW	7-24-23

AGC Engineers, INC.

405 S. Leonard St., Suite D
Liberty, Missouri 64068
816.781.4200
fax 792.3666
www.agcengineers.com



SWIG - 5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS
FINAL DEVELOPMENT PLANS
DETAILS



BY	REVISION	DATE
ACA	ADDRESS CITY COMMENTS DATED 8-23-23	8-31-23
ACA	SUBMITTED TO CITY FOR REVIEW	7-24-23

405 S. Leonard St., Suite D
 Liberty, Missouri 64068

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SWIG - 5959 BARKLEY STREET
 MISSION, JOHNSON COUNTY, KANSAS

FINAL DEVELOPMENT PLANS
 DETAILS

16



SAVORY
MANAGEMENT

SWIG
BARKLEY STREET & MARTWAY STREET
MISSION, KANSAS



OLSON ARCHITECTURAL GROUP
1916 NW 79TH TERRACE
KANSAS CITY, MISSOURI 64151

REVISION SCHEDULE		
No.	Description	Date
1	Add "Native Plant" Note	08-27-23

SHEET NAME

LANDSCAPE PLAN

PROJECT NUMBER	2101
DATE	05/05/2023
DRAWN BY	PJS
CHECKED BY	SDO
SCALE	1" = 40'-0"

LS101

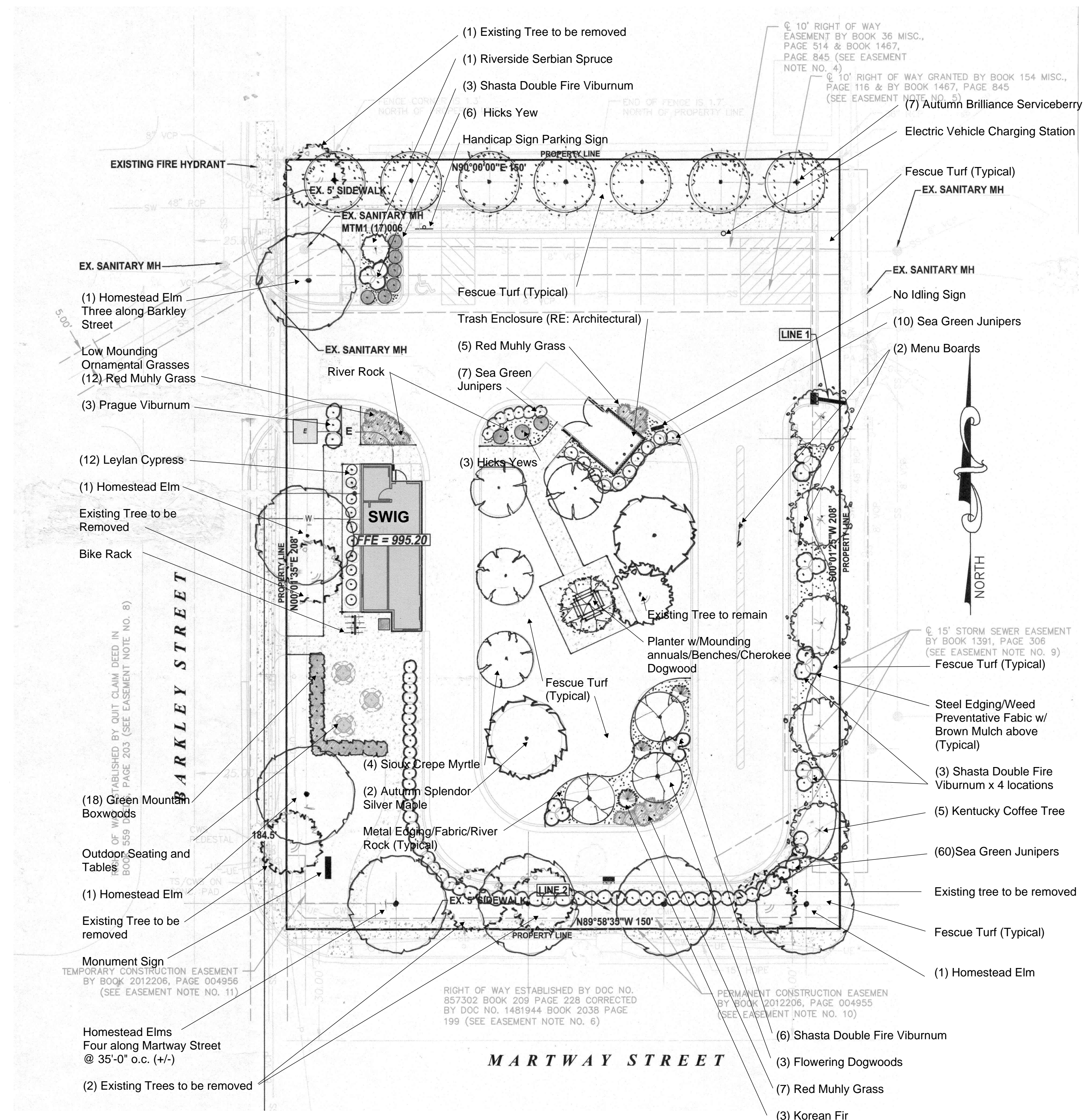
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PLANTING LEGEND

- (62) Sea Green Junipers
- (3) Korean Fir
- (22) Red Muhly Grass
- (12) Hicks Yew
- (1) Riverside Serbian Spruce
- (21) Shasta Double Fire Viburnum
- (3) Prague Viburnum
- (12) Leylan Cypress
- (15) Green Mountain Boxwoods
- (6) Existing Trees to be Removed
- (1) Existing Tree to Remain
- (7) Autumn Brilliance Silver Maple
- (5) Kentucky Coffee Tree
- (1) Cherokee Dogwood
- (3) Flowering Dogwoods
- (4) Sioux Crepe Myrtle
- (2) Autumn Splendor Silver Maple
- (7) Homestead Elm

Planting Count

Existing Trees to be removed	6
Existing Trees to remain	1
New Trees scheduled	22
New Street Trees	7
Total Trees on Site	30
Total Shrubs on Site	155
Total Plantings on Site	185



All Plantings to be native and non-invasive to the Northeast Kansas Region. Please contact OAG for conflicts or approved replacement planting materials.

1 - 08-28-23

LANDSCAPE PLAN
TYPE IV: TWO WAY STREETS

1" = 20'

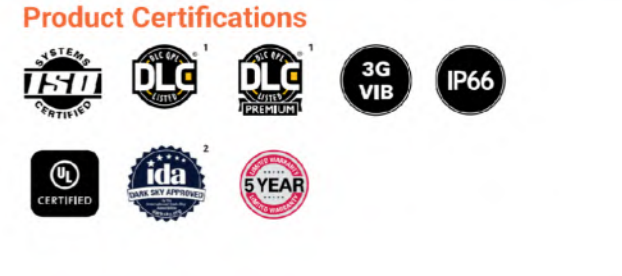
NORTH

Project	Catalog #	Type
Prepared by	Notes	Date



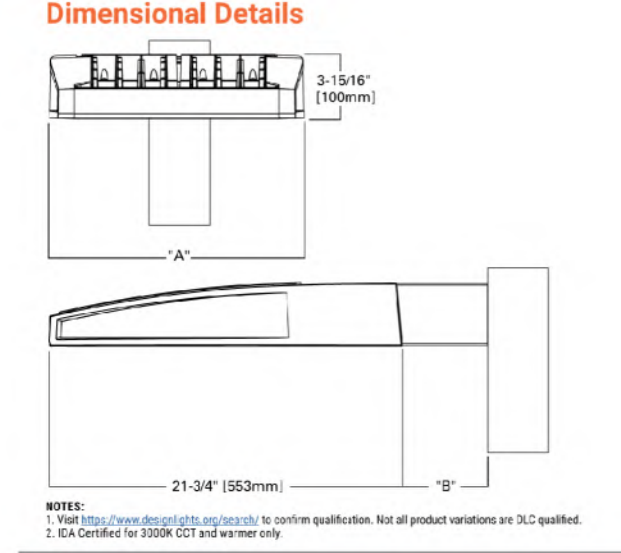
McGraw-Edison
GLEON Galleon
Area / Site Luminaire

- Interactive Menu**
- Ordering Information page 2
 - Mounting Details page 3
 - Optical Distributions page 4
 - Product Specifications page 4
 - Energy and Performance Data page 4
 - Control Options page 5



- Quick Facts**
- Lumen packages range from 4,200 - 80,800 (34W - 640W)
 - Efficacy up to 156 lumens per watt
 - Options to meet Buy American and other domestic preference requirements

- Connected Systems**
- WaveLinX
 - Enlighted



Number of Light Fixtures	WxH	Mount Area Length	Extended Area Length	Depth Mount Area Length	Depth Mount Extended Area Length
1-4	15 1/2"	7"	10"	10 5/8"	16 9/16"
5-6	21 5/8"	7"	10"	10 5/8"	16 9/16"
7-8	27 5/8"	7"	13"	10 5/8"	-
9-10	33 3/4"	7"	16"	-	-

NOTE: For all selection requirements and additional line art, see Mounting Details section.

COOPER Lighting Solutions PS600020EN page 1
November 05, 2022 3:05 PM

Project	Catalog #	Type
Prepared by	Notes	Date



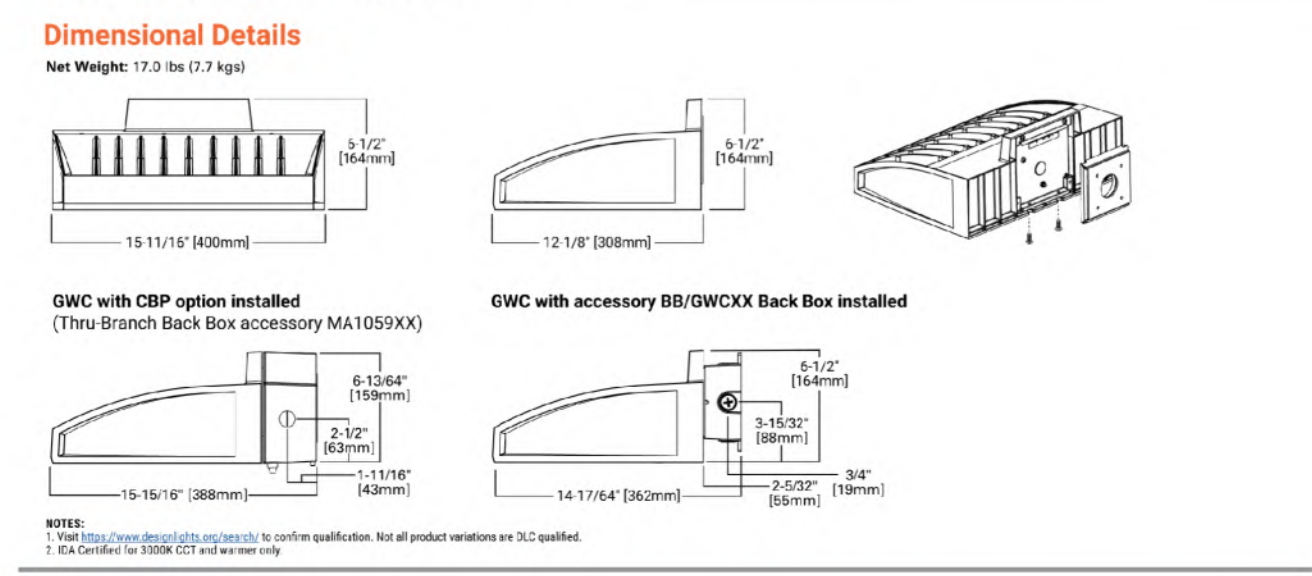
McGraw-Edison
GWC Galleon Wall
Wall Mount Luminaire

- Interactive Menu**
- Ordering Information page 2
 - Product Specifications page 2
 - Optical Configurations page 3
 - Energy and Performance Data page 4
 - Control Options page 5



- Quick Facts**
- Choice of thirteen high-efficiency, patented Acoul.ED Optics
 - Downward and inverted wall mounting configurations
 - Eight lumen packages from 3,215 up to 17,056
 - Efficacies up to 154 lumens per watt

- Connected Systems**
- WaveLinX
 - Enlighted



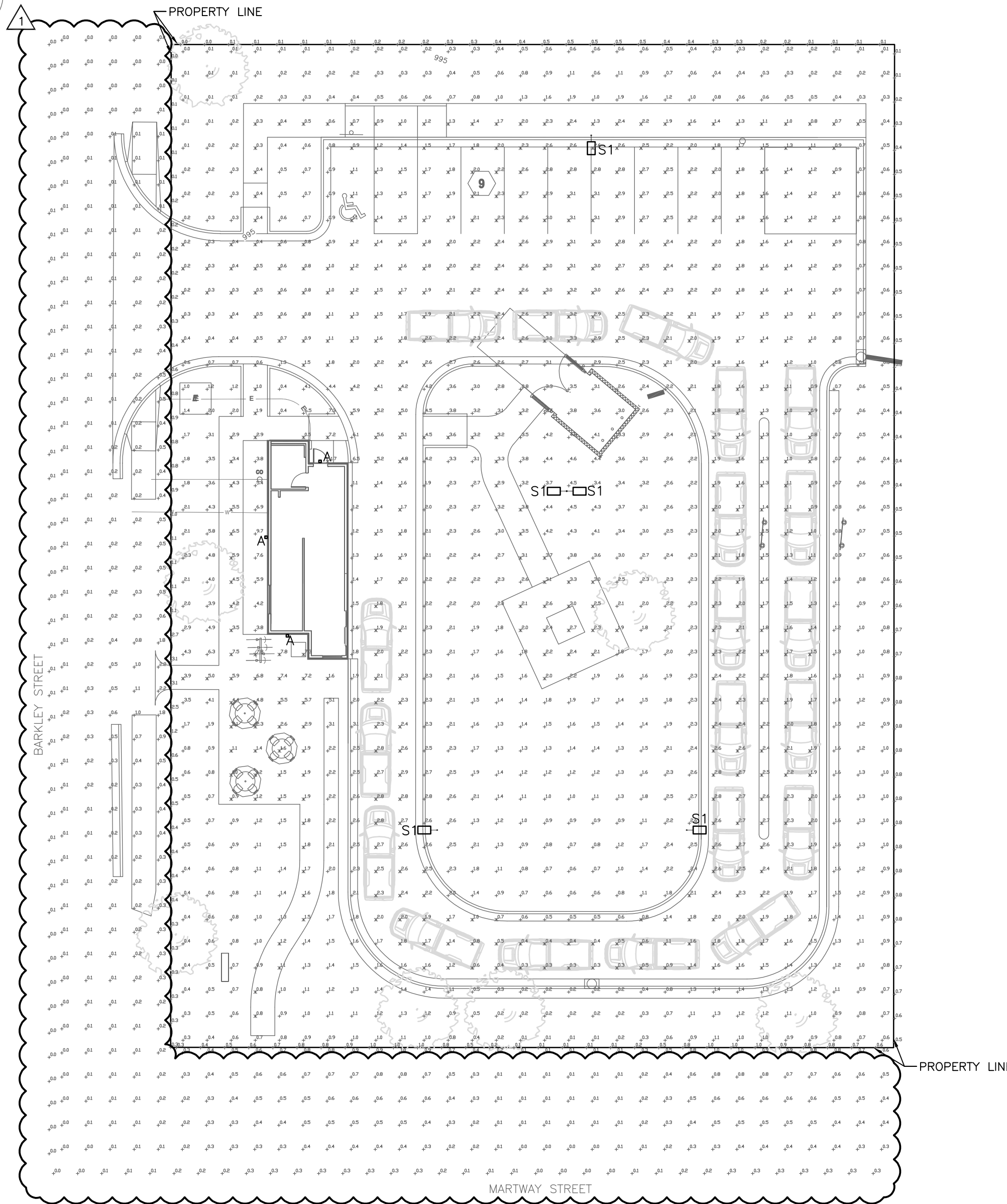
COOPER Lighting Solutions PS600046EN page 1
December 5, 2022 3:05 PM

OUTDOOR LIGHTING FEATURE SCHEDULE

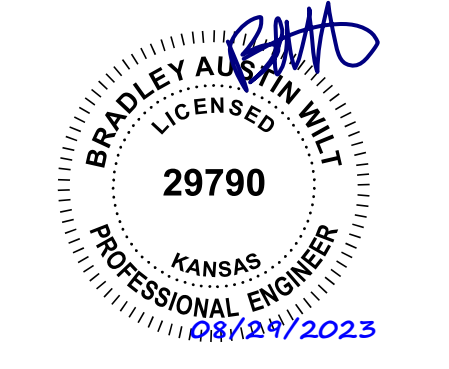
SYMBOL	TAG	QUANTITY	MANUFACTURER	CATALOG NUMBER	DESCRIPTION	HEIGHT	SPACING	WATTAGE	REMARKS
S1	8'-0"	4	COOPER LIGHTING	GLEON-GAL-10-10-10	GLEON GALLEON AREA LUMINAIRE WITH DOWNWARD OPTICS. USE EACH AND TYPE AS SHOWN.	30'	30'	100W	USE EACH AND TYPE AS SHOWN. SEE NOTES FOR FULL SCHEDULE.
A	8'-0"	2	COOPER LIGHTING	GLEON-GAL-10-10-10	GLEON GALLEON AREA LUMINAIRE WITH DOWNWARD OPTICS. USE EACH AND TYPE AS SHOWN.	30'	30'	100W	USE EACH AND TYPE AS SHOWN. SEE NOTES FOR FULL SCHEDULE.

Statistics

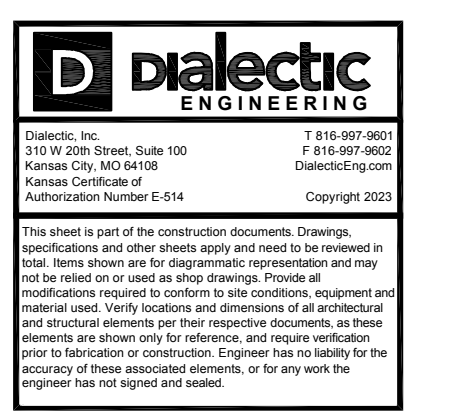
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Building Sidewalk	X	3.5 Fc	11.7 Fc	0.5 Fc	23.4:1	7.0:1
Drive Aisle	X	1.7 Fc	2.8 Fc	0.3 Fc	9.3:1	5.7:1
Drive Thru	X	2.7 Fc	5.9 Fc	1.4 Fc	4.2:1	1.9:1
Parking	X	1.8 Fc	3.3 Fc	0.2 Fc	16.5:1	9.0:1
Parking Sidewalk	X	1.1 Fc	2.4 Fc	0.3 Fc	8.0:1	3.7:1
Central Sidewalk	X	2.8 Fc	4.5 Fc	1.9 Fc	2.4:1	1.5:1
Street Centerlines	X	0.1 Fc	0.3 Fc	0.0 Fc	N/A	N/A



01 PHOTOMETRIC SITE PLAN
1"=20'



SWIG - MISSION
5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS



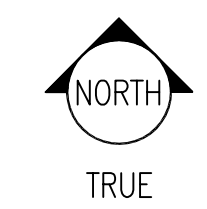
OAG
OLSON ARCHITECTURAL GROUP
1916 NW 70TH TERRACE
KANSAS CITY, MO 64151

REVISION SCHEDULE

No.	Description	Date
1	CITY COMMENTS	08/29/2023

SHEET NAME
PHOTOMETRIC SITE PLAN

PROJECT NUMBER: #####
DATE: ####/####/2023
DRAWN BY: JKS
CHECKED BY: KC
SCALE:



ES100



SAVORY
MANAGEMENT

SWIG
BARKLEY STREET & MARTWAY STREET
MISSION, KANSAS



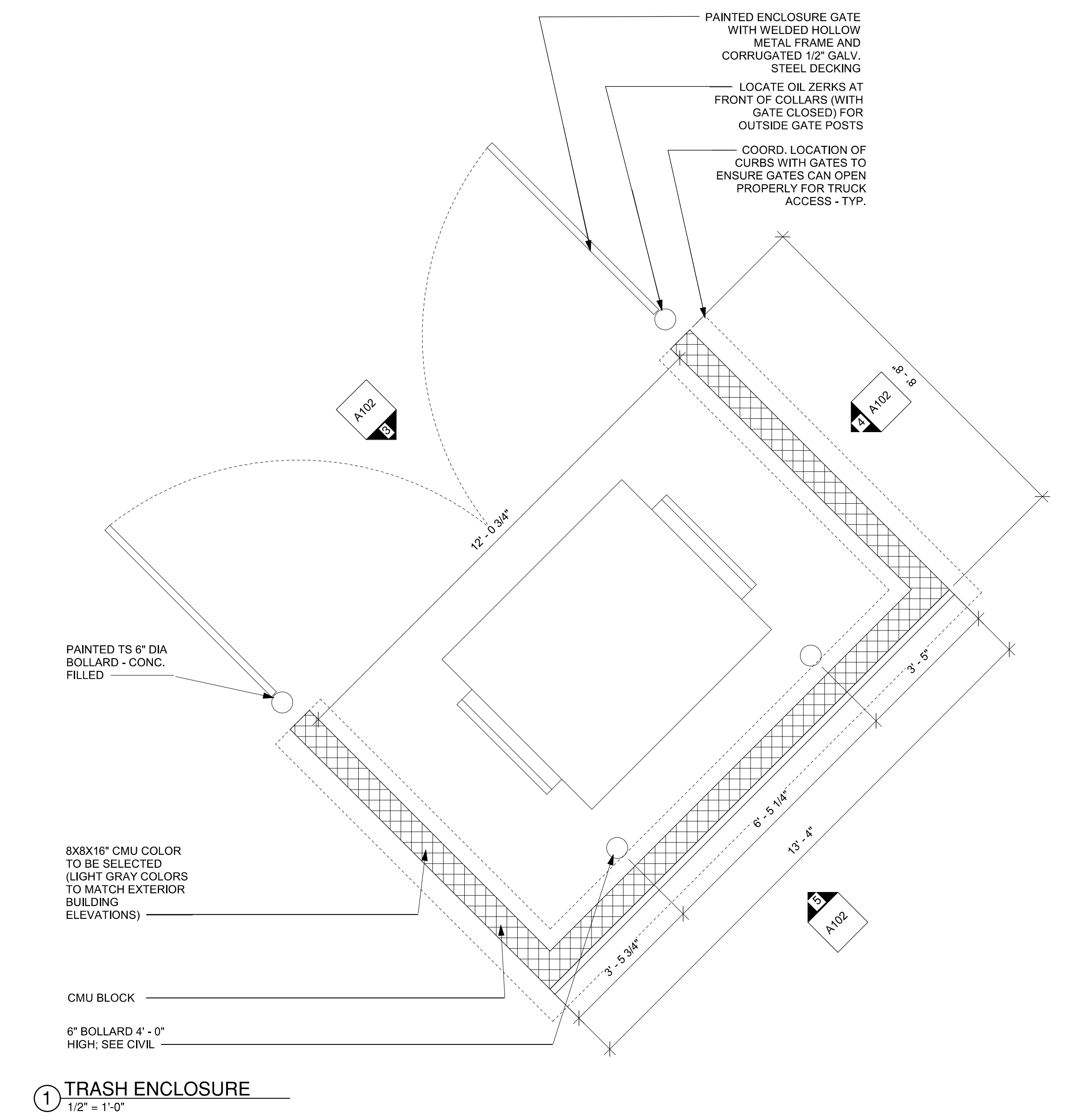
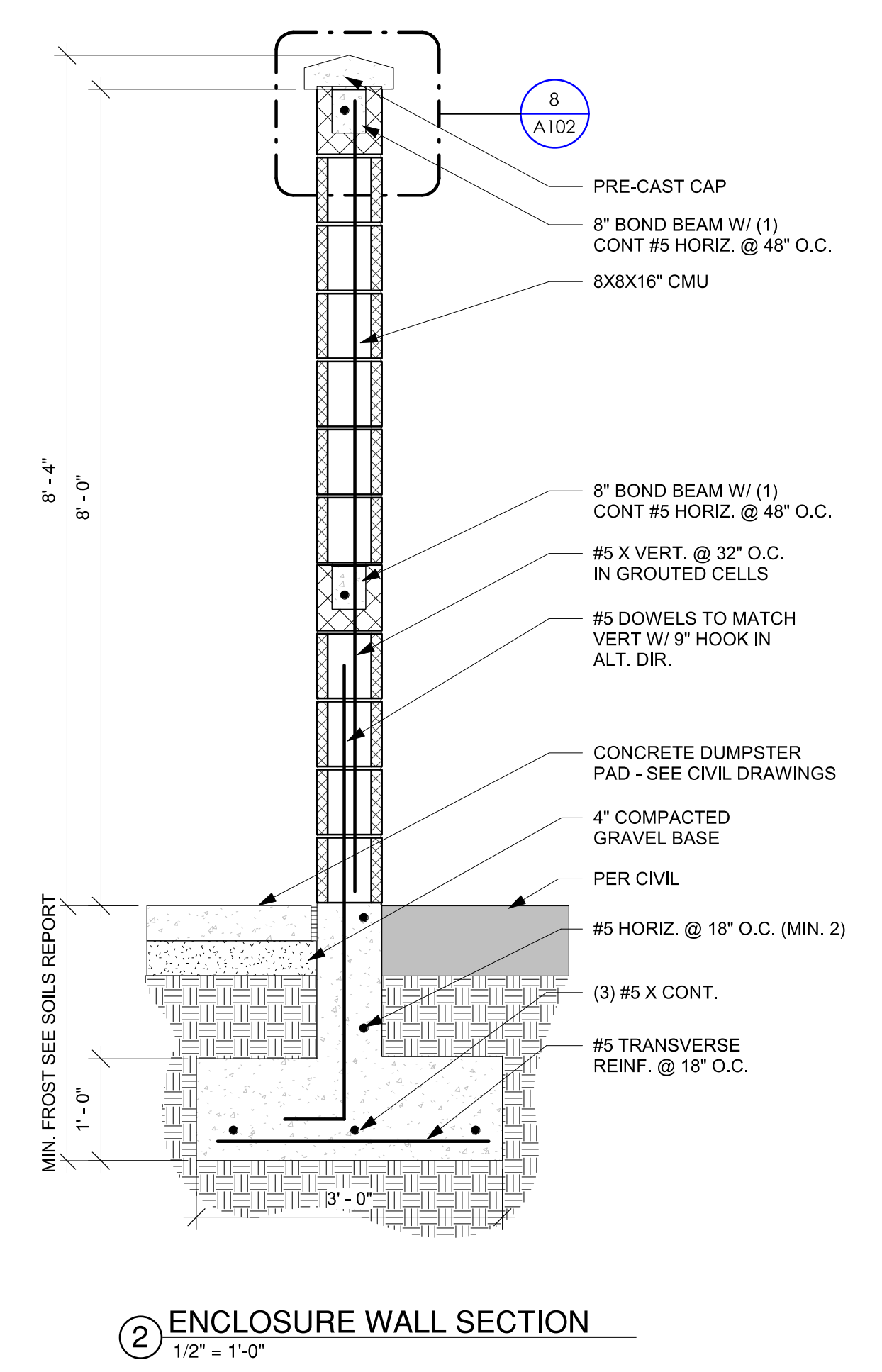
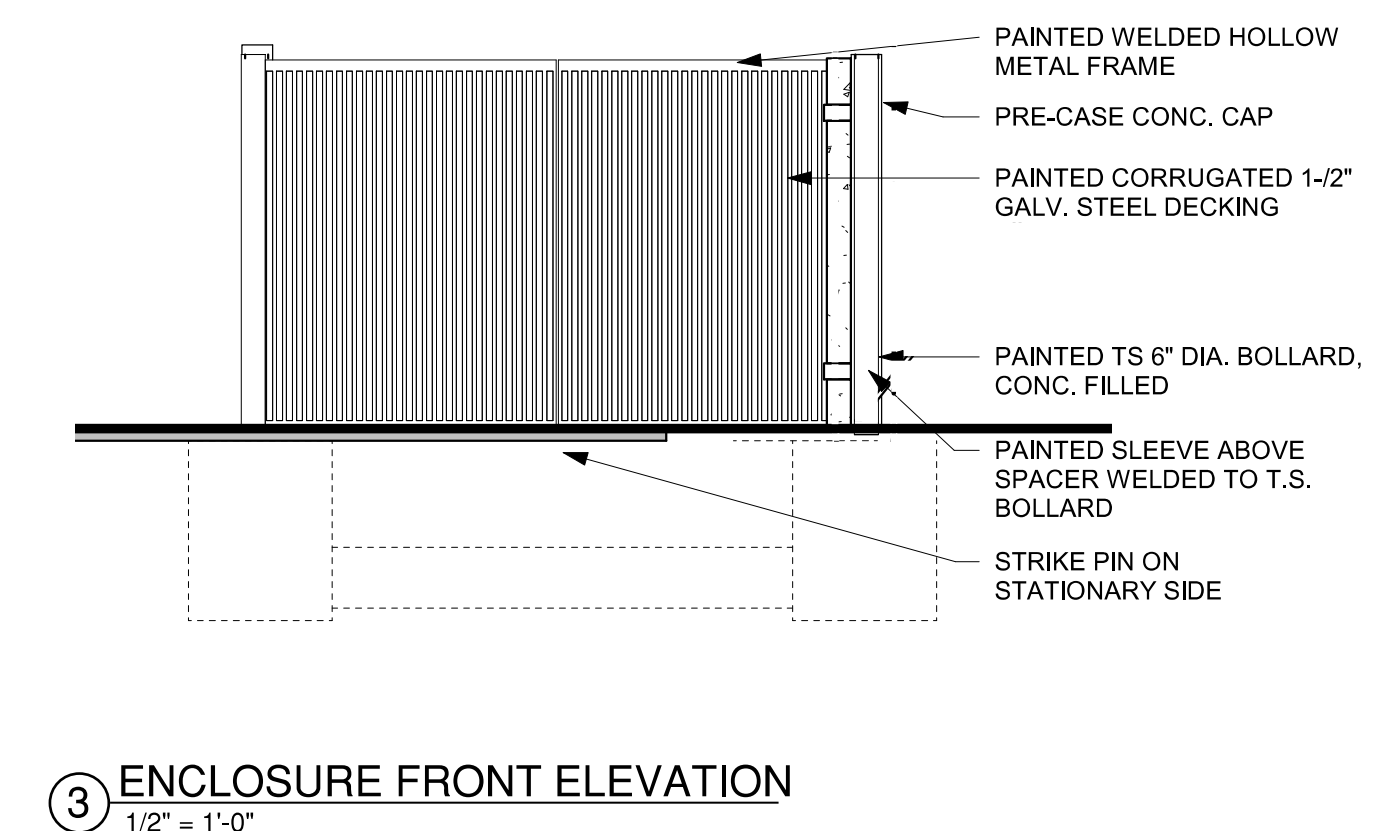
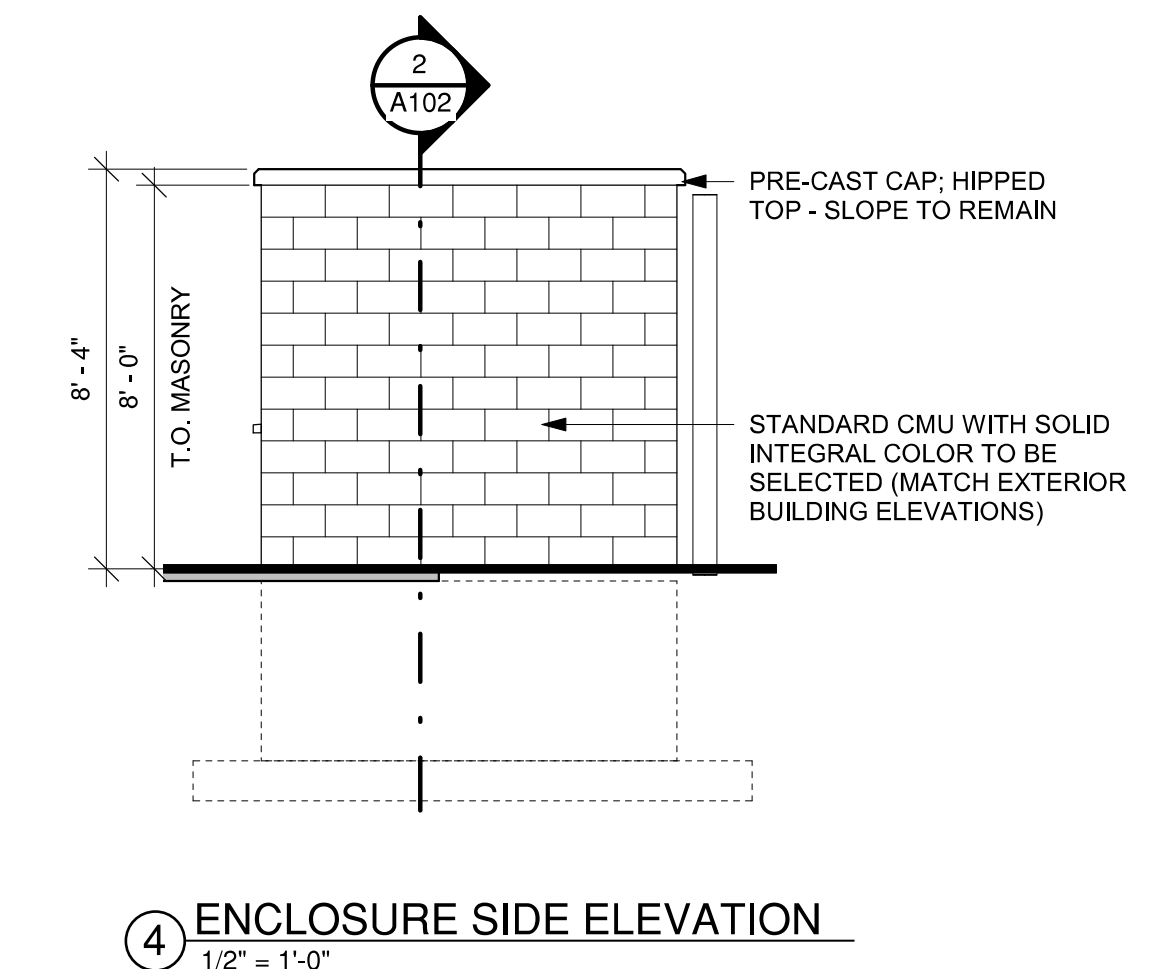
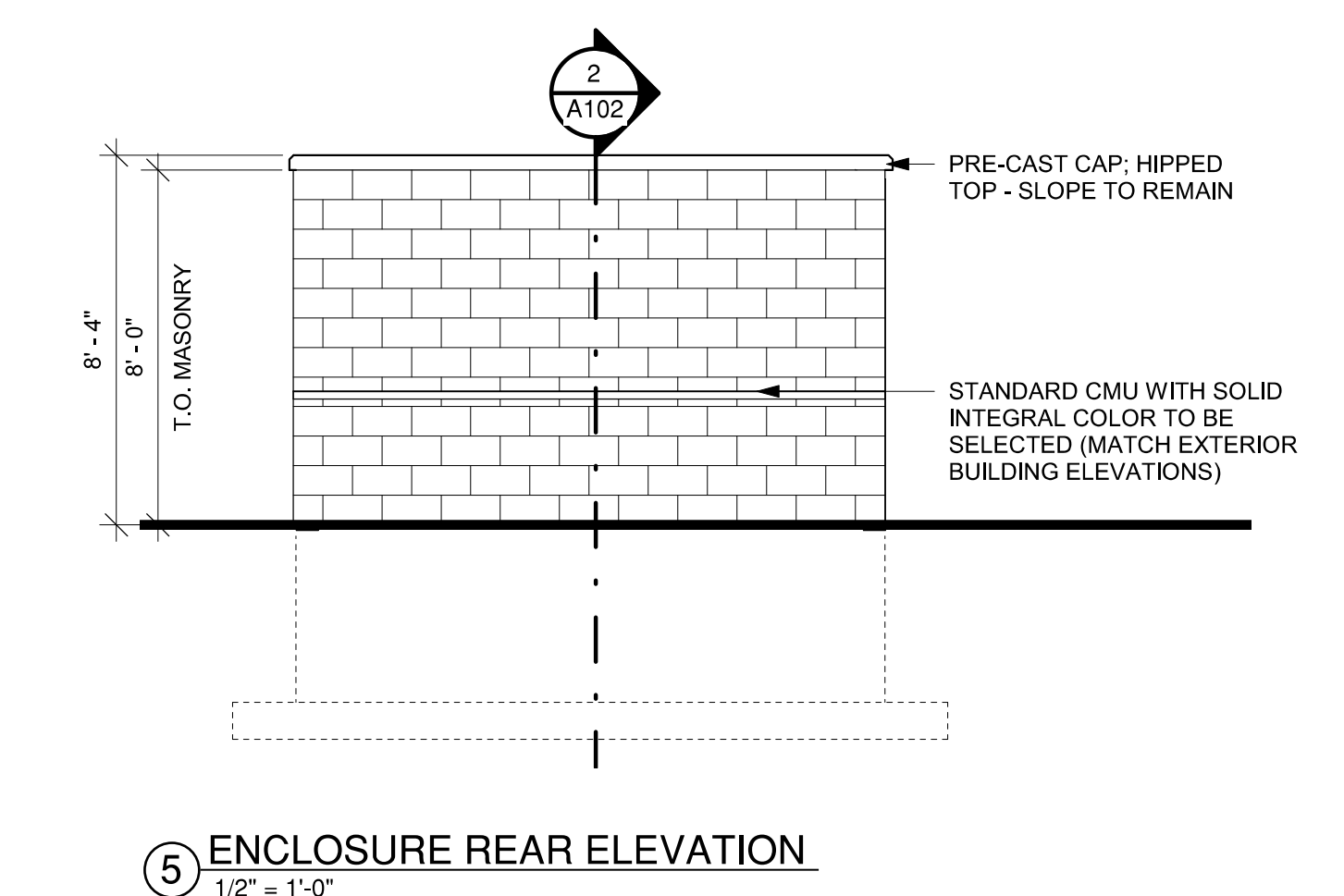
OLSON ARCHITECTURAL GROUP
1916 NW 79TH TERRACE
KANSAS CITY, MISSOURI 64151

REVISION SCHEDULE		
No.	Description	Date

SHEET NAME	
TRASH ENCLOSURE	
PROJECT NUMBER	2101
DATE	05/29/2023
DRAWN BY	PJS
CHECKED BY	SDO
SCALE	1/2" = 1'-0"

A102

10/10/2023 1:37:12 PM





SAVORY
MANAGEMENT

SWIG
BARKLEY STREET & MARTWAY STREET
MISSION, KANSAS



OLSON ARCHITECTURAL GROUP
1916 NW 79TH TERRACE
KANSAS CITY, MISSOURI 64151

REVISION SCHEDULE		
No.	Description	Date

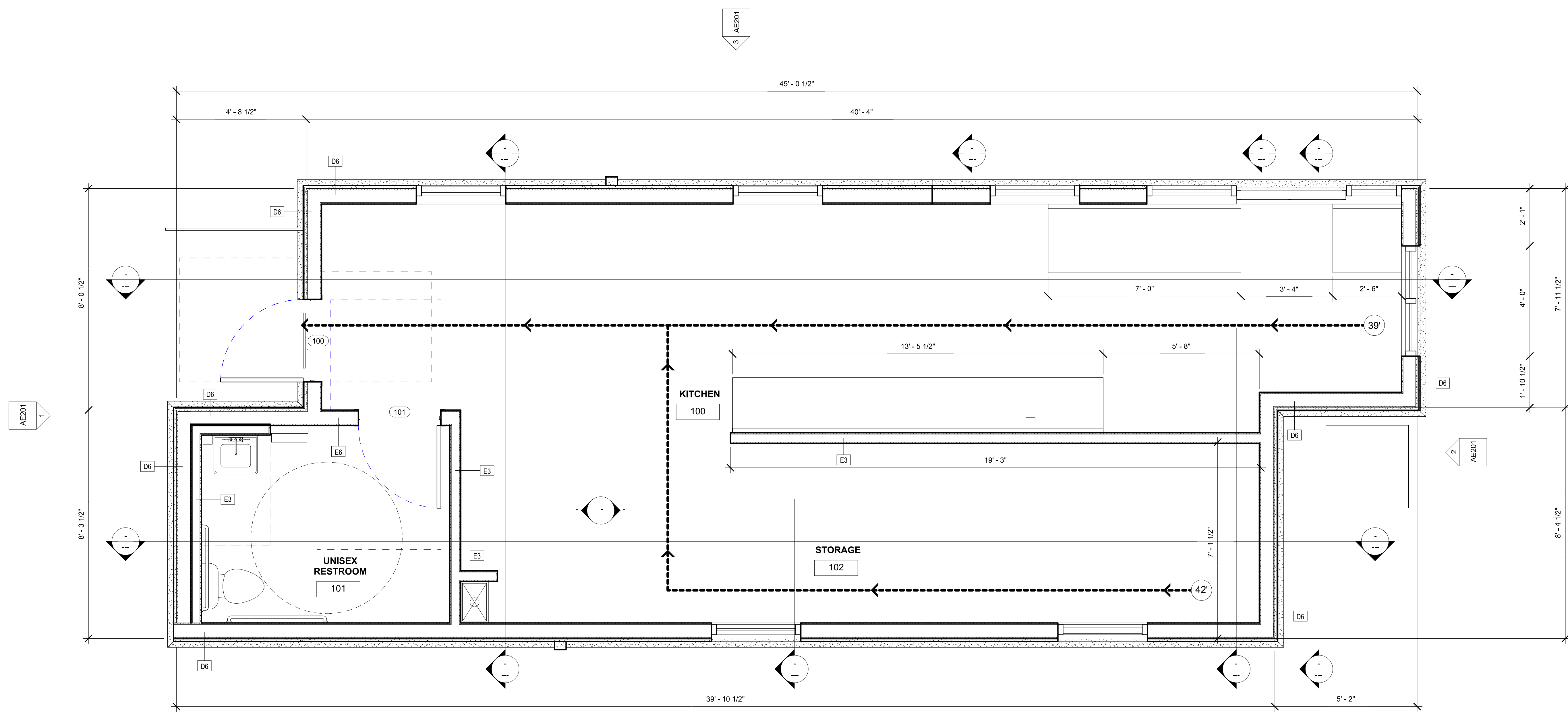
SHEET NAME

FLOOR PLAN

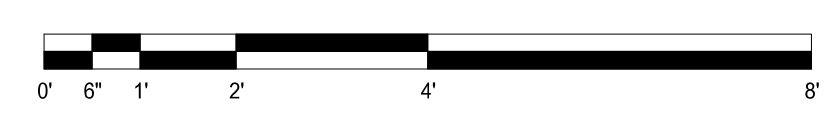
PROJECT NUMBER	2101
DATE	05/05/2023
DRAWN BY	PJS
CHECKED BY	SDD
SCALE	1/2" = 1'-0"

AE112

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1 FLOOR PLAN
1/2" = 1'-0"



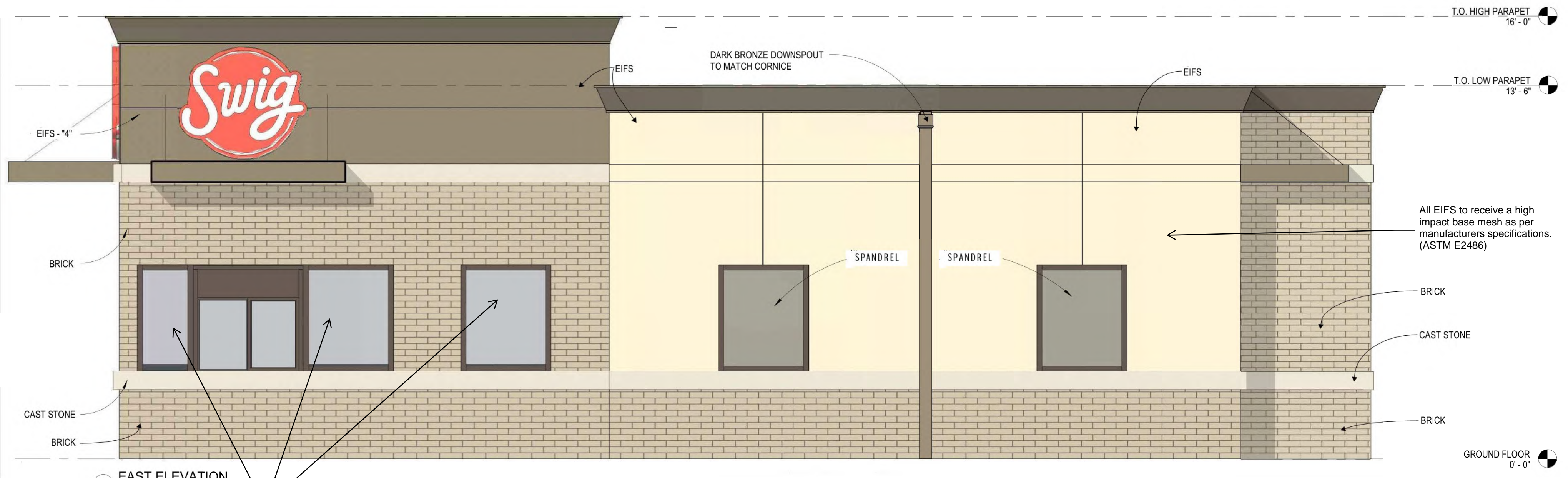


4 SOUTH ELEVATION
3/8" = 1'-0"

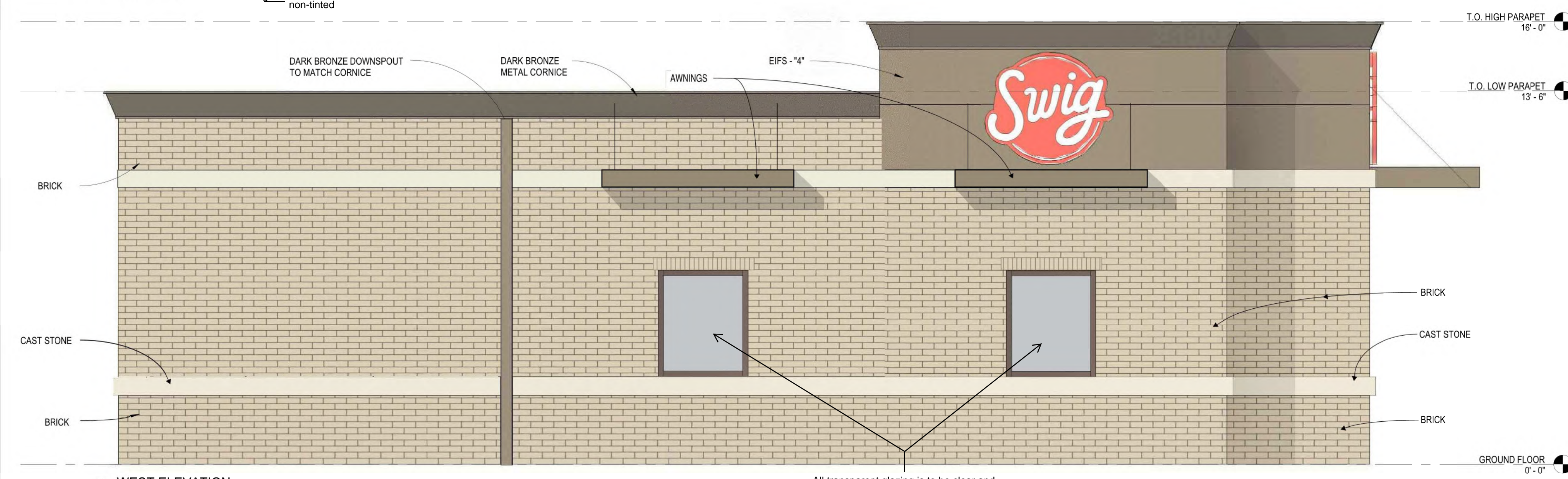


4 NORTH ELEVATION
3/8" = 1'-0"

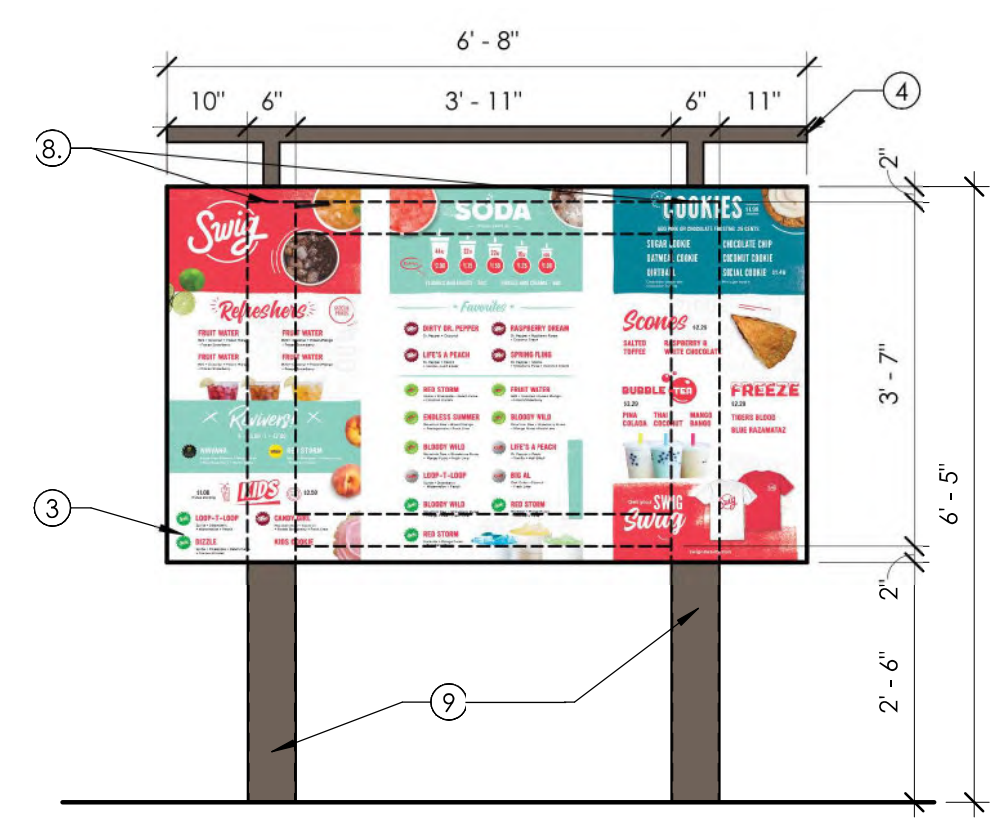
- KEYNOTES**
- 1 PREFINISHED METAL DOWNSPOUT
 - 2 BUILDING SIGNAGE BY OWNER
 - 3 MENU BOARD AND FOOTINGS BY SEPARATE SIGN PERMIT
 - 4 DARK BRONZE LIGHT FIXTURE; COORDINATE WITH ELECTRICAL
 - 5 CO2 ENCLOSURE - SEE EQUIPMENT PLAN
 - 6 ELECTRICAL PANEL OR EQUIPMENT; COORDINATE WITH ELECTRICAL
 - 7 CONTROL JOINT
 - 8 STEEL CAP AT TOP OF POSTS
 - 9 POWDER COATED STEEL TUBE FRAME, TYP.
 - 10 24" X 36" OPENING
 - 11 EXTERIOR CANOPY MANUFACTURER - ARCHITECTURAL CANOPIES; EXTRUDECK
 - 12 ROOF PROFILE
 - 13 RTU BEYOND
 - 14 TRASH ENCLOSURE



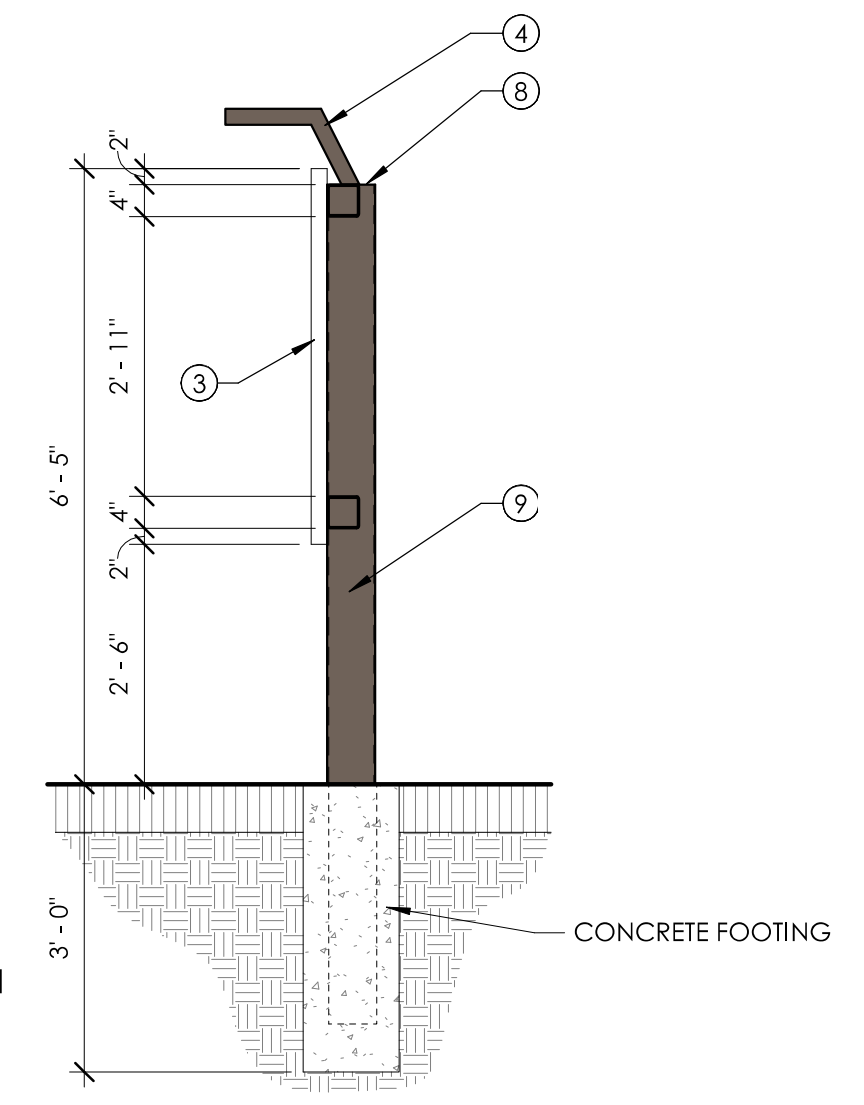
2 EAST ELEVATION
3/8" = 1'-0"



1 WEST ELEVATION
3/8" = 1'-0"



ELEVATION



SECTION

DRIVE-UP MENU BOARD - BY OWNER



SWIG
BARKLEY STREET & MARTWAY STREET
MISSION, KANSAS



OLSON ARCHITECTURAL GROUP
1916 NW 79TH TERRACE
KANSAS CITY, MISSOURI 64151

REVISION SCHEDULE

No.	Description	Date

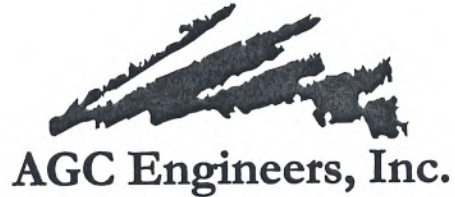
SHEET NAME
EXTERIOR ELEVATIONS

PROJECT NUMBER	2101
DATE	05/05/2023
DRAWN BY	PJS
CHECKED BY	SDD
SCALE	3/8" = 1'-0"

AE201

10/12/2023 1:37:18 PM

405 South Leonard Street, Suite D
Liberty, Missouri 64068
816-781-4200
FAX 792-3666



MEMORANDUM

TO: Public Works Department
City of Mission, Kansas

FROM: Ron Cowger (AGC Engineers, Inc)

RE: Redevelopment of 0.71-acre site located at the NE corner of Barkley Street & Martway Street

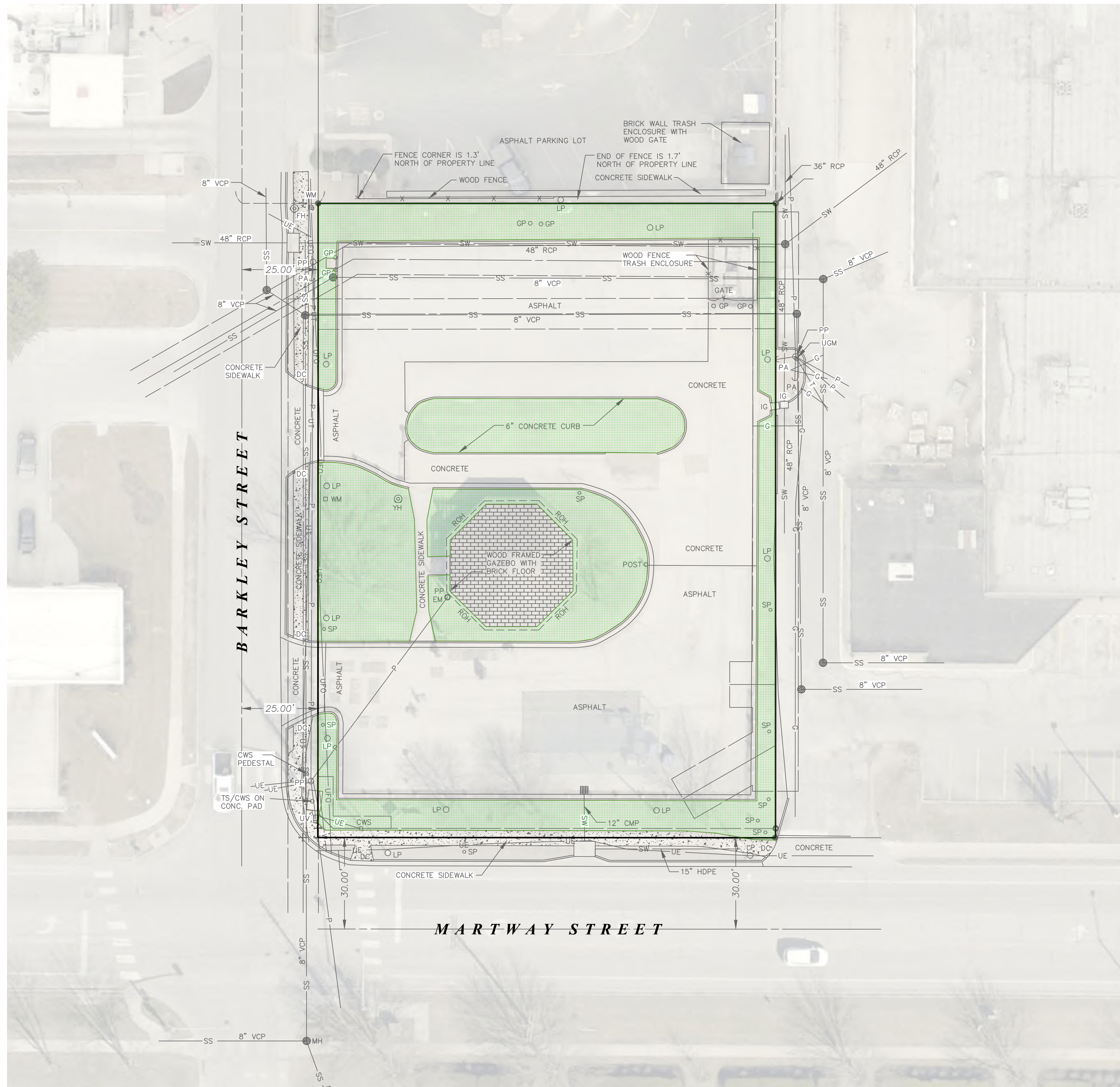
DATE: February 14, 2023 (updated May 30, 2023)

The above referenced site has previously been developed and contains 0.22 acres of pervious greenspace (based on the field survey provided by Anderson Survey Company).

The proposed redevelopment of the site as proposed will increase the greenspace to 0.29-acres. This correlates to over 40% of the site being pervious (green).

As a result of the increased pervious area, stormwater runoff should be less than currently existing and permeant stormwater management facilities will not be required.

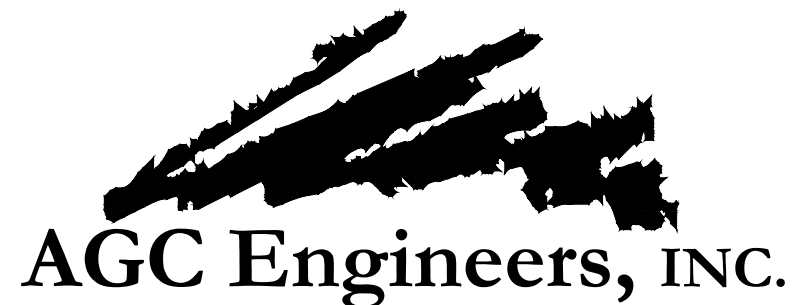
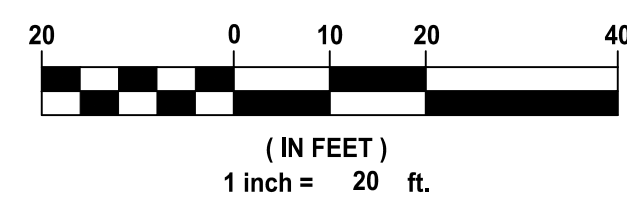




TOTAL SITE AREA - 0.71 ACRES
GREENSPACE - 0.22 ACRES OR 31%



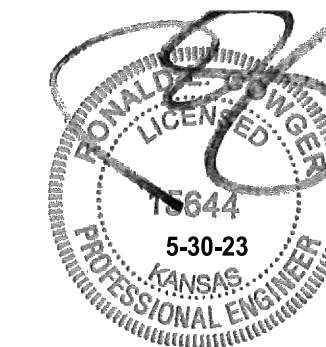
BY	REVISION	DATE
RC/ACA	SUBMITTED FOR REVIEW PER CITY COMMENTS	5-30-23
RC/ACA	SUBMITTED FOR REVIEW PER CITY COMMENTS	5-4-23
RC/ACA	SUBMITTED TO CITY FOR REVIEW	2-17-23



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 Liberty, Missouri 64068

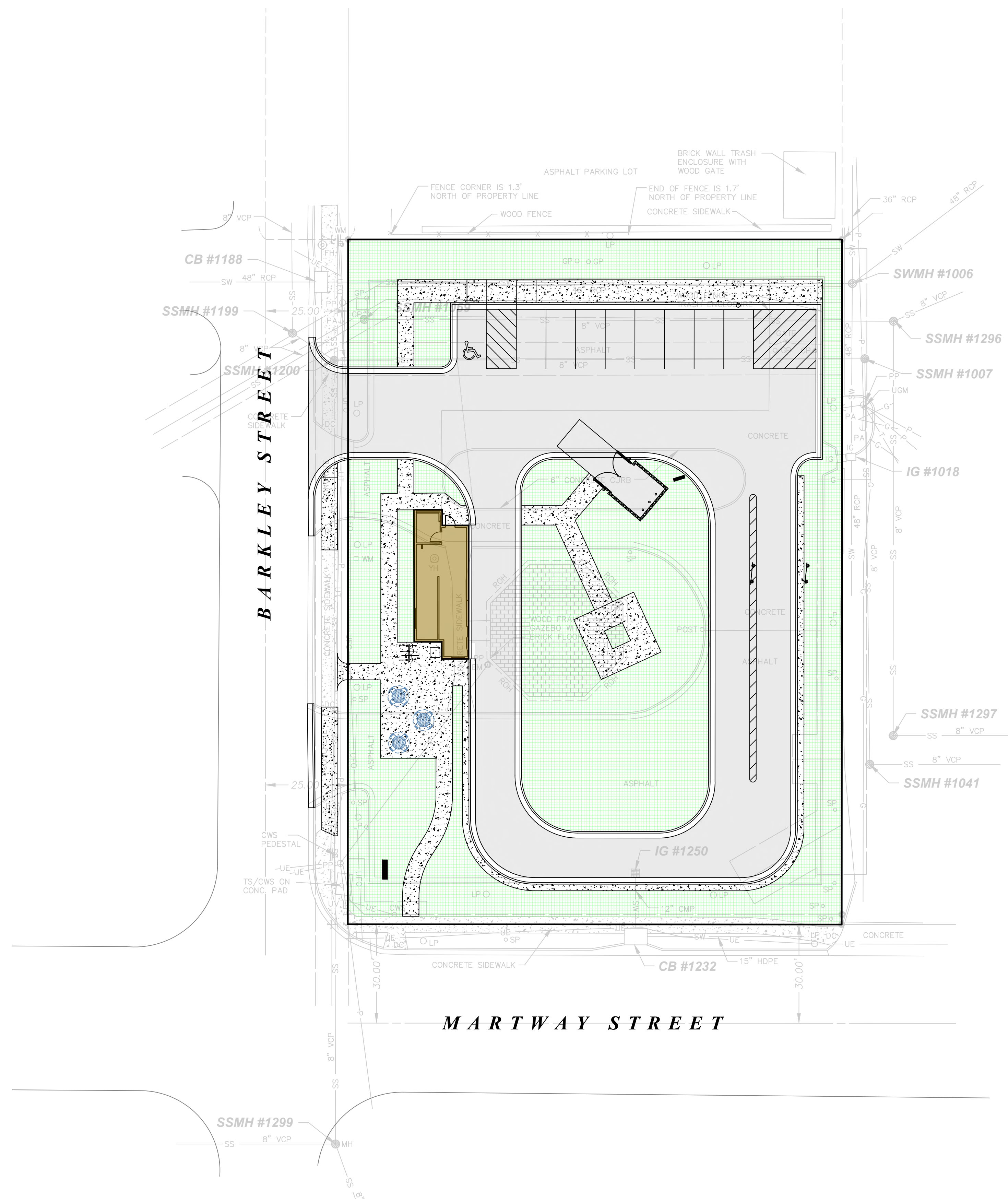
816.781.4200 ■
 fax 792.3666

www.agcengineers.com



SWIG - 5959 BARKLEY STREET
MISSION, JOHNSON COUNTY, KANSAS

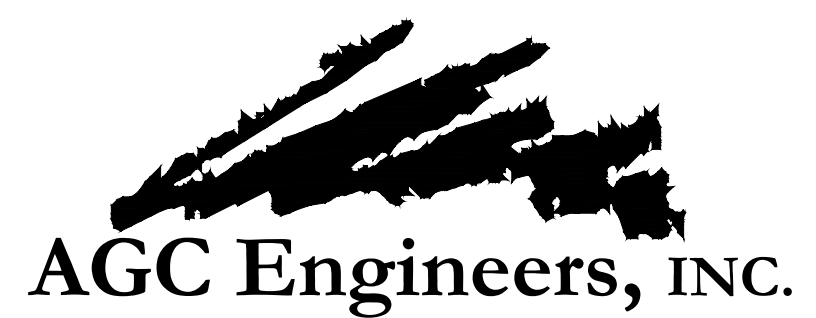
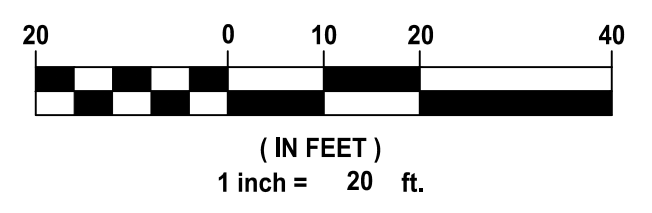
PRELIMINARY DEVELOPMENT PLANS
GREENSPACE - EXISTING CONDITIONS



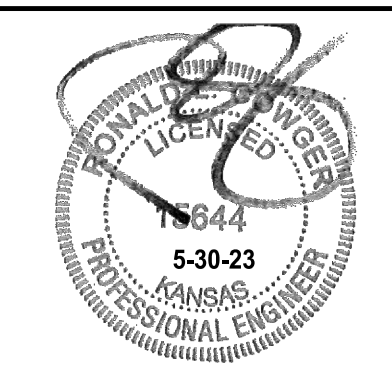
TOTAL SITE AREA - 0.71 ACRES
GREENSPACE - 0.29 ACRES OR 40%



BY	REVISION	DATE
RC/ACA	SUBMITTED FOR REVIEW PER CITY COMMENTS	5-30-23
RC/ACA	SUBMITTED FOR REVIEW PER CITY COMMENTS	5-4-23
RC/ACA	SUBMITTED TO CITY FOR REVIEW	2-17-23



405 S. Leonard St., Suite D
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 816.781.4200 ■
 fax 792.3666
 www.agcengineers.com



SWIG - 5959 BARKLEY STREET
 MISSION, JOHNSON COUNTY, KANSAS

PRELIMINARY DEVELOPMENT PLANS
 GREENSPACE - PROPOSED CONDITIONS

8

AT A GLANCE

Applicant:
Superstar Holdings, LLC

Case Number:
#23-19

Location:
5959 Barkley Street

Project Name:
Swig - 5959 Barkley Final Plat

Property ID:
KF25208-2050

Project Summary:
The applicant is requesting approval of the final plat for a drive-through soda shop on a vacant lot at 5959 Barkley Street.

Current Zoning:
C-2A

Proposed Zoning:
N/A

Current Land Use:
Vacant

Staff Contact:
Karie Kneller, City Planner

Proposed Land Use:
Drive-through Food Establishment

N/A Public Hearing Required

Legal Notice:
N/A



BACKGROUND AND PROPERTY INFORMATION

The subject property is located at 5959 Barkley street, on the northeast corner of Barkley and Martway. The property is approximately .71 acres and is currently vacant with a gazebo structure that is seasonally used to sell plants and pots in the spring. The lot is zoned “C-2A” Pedestrian Oriented Business District and surrounded by parcels zoned C-2A, C-2B, and CP-2. Adjacent uses include fast-food drive through, automobile maintenance, shopping center, and grocery store.

The property is not currently platted, and staff required a plat to be recorded with the County upon final approval of a plan for a Swig soda shop on the subject property. The final development plan for the Swig soda shop is referenced in Case #23-20.

All necessary utilities are currently provided on-site.

PROJECT PROPOSAL

Rights-of-way were previously established for the current sidewalk infrastructure. The plat establishes easements for the public utilities on site that include stormwater, water main, and sanitary sewer.

PLAN REVIEW AND ANALYSIS

Mission municipal code at §440.260 states that Preliminary Plats shall be approved by the Planning Commission if it determines that:

Municipal Code

A. Prior to consideration of any final plat, City staff shall determine whether dedication of right-of-way will be required.

Analysis: City Staff did not determine that additional right-of-way dedication was required with this plat.

B. Final plats shall be approved by the Planning Commission if it determines that:

1. The final plat substantially conforms to the approved preliminary plat and rule exceptions granted thereto.
2. The plat conforms to all applicable requirements of this Code, subject only to approved rule exceptions.
3. All submission requirements have been satisfied.

4. Approval of a final plat shall require the affirmative vote of a majority of the membership of the Planning Commission.

Analysis: The final plat conforms to the approved preliminary plat and conforms to requirements of the municipal code. Utility easements of established utilities on site are provided.

C. Following approval of the final plat by the Planning Commission, the final plat shall be submitted to the City Council for review of land proposed to be dedicated for public purposes. The City Council shall approve or disapprove the dedication of land for public purposes within thirty (30) days after the first (1st) meeting of the City Council following the date of the submission of the plat to the City Clerk. The City Council may defer action for an additional thirty (30) days for the purpose of allowing for modifications to comply with the requirements established by the City Council. No additional filing fees shall be assessed during that period. If the City Council defers or disapproves any such dedication, it shall advise the Planning Commission of the reasons therefor. No plat shall be filed with the Register of Deeds unless such plat bears the endorsement that the land dedicated to public purposes has been approved by the City Council

Analysis: The final plat, upon Planning Commission's recommended approval, will be considered by the City Council to approve the dedicated easements for public utilities.

D. Final plats shall be recorded with the Register of Deeds within eighteen (18) months following City Council approval of land dedicated to public purposes. Final plats which are not recorded within said time period shall be deemed null and void.

Analysis: Staff will ensure the plat is recorded within the stipulated timeframe.

For the Preliminary Plat, the Planning Commission approved Case # 23-09, the Preliminary Plat for the Swig Soda Shop project without conditions.

RECOMMENDATION

Staff recommends that the Planning Commission recommend approval of Case #23-19, the Final Plat of Swig - 5959 Barkley to the City Council.

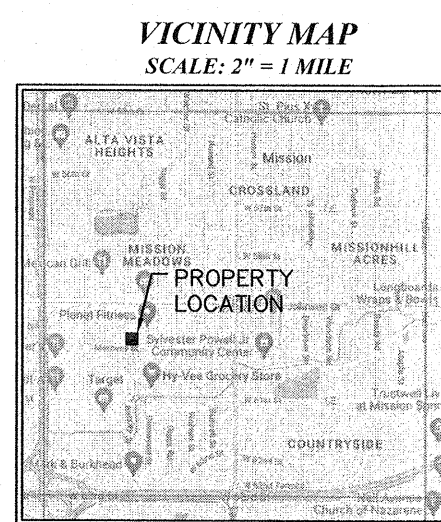
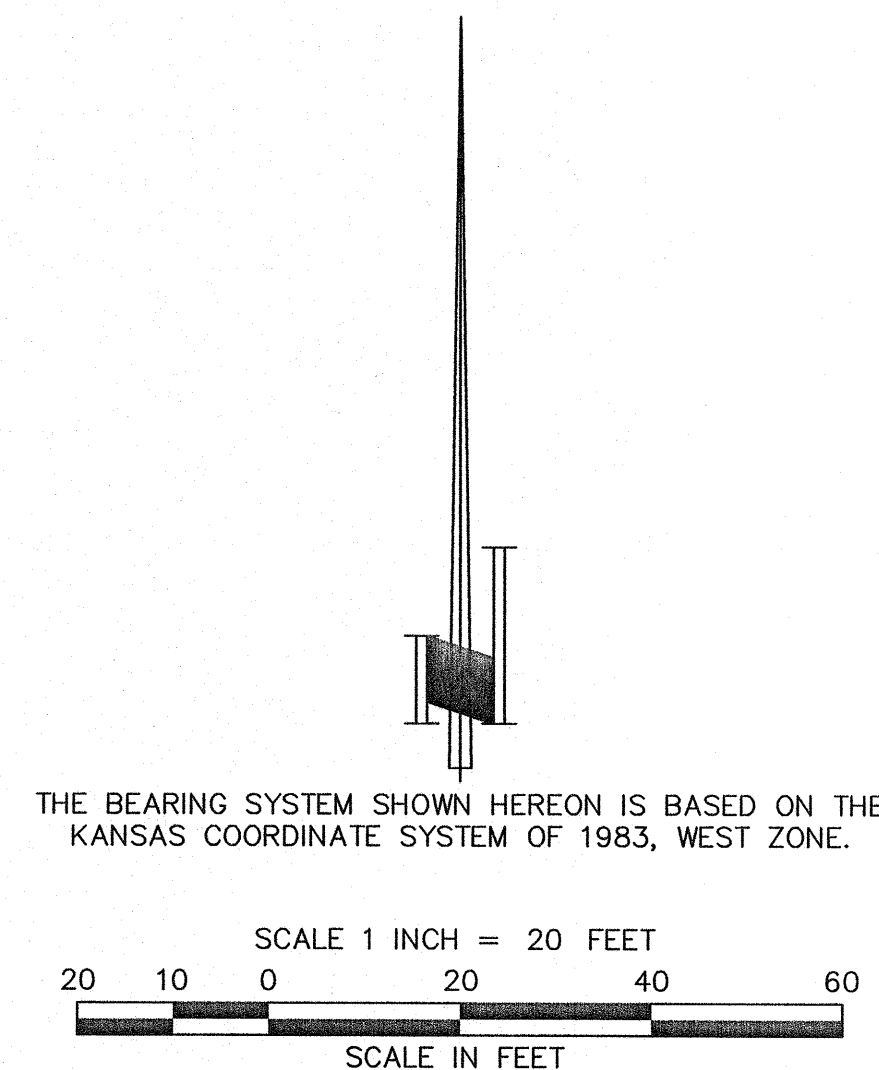
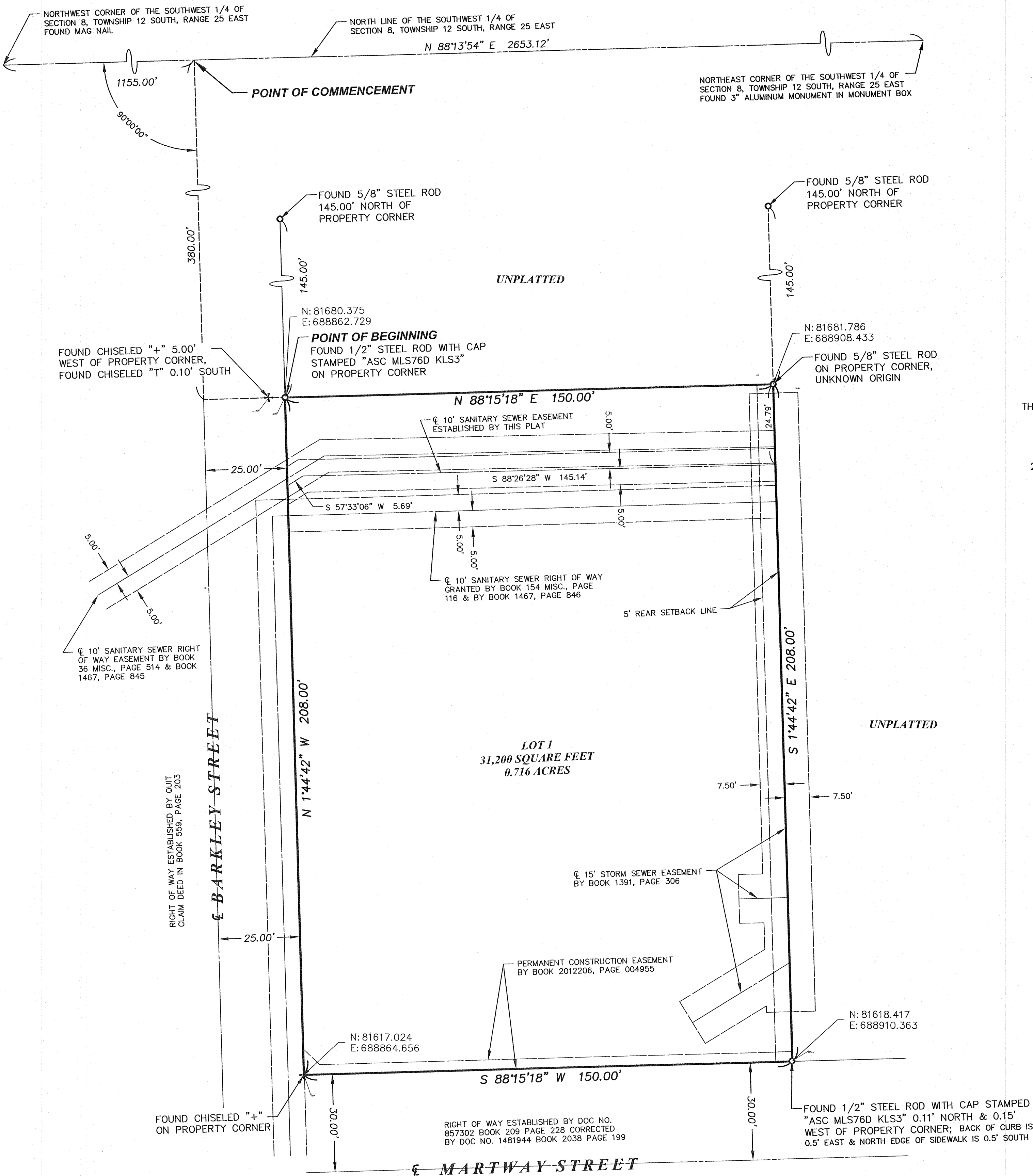
PLANNING COMMISSION ACTION

The Planning Commission will consider Case #23-12 at its September 25, 2023 meeting.

CITY COUNCIL ACTION

The City Council will consider Case #23-12 at its October 18, 2023 meeting.

FINAL PLAT OF
SWIG - 5959 BARKLEY
A MINOR SUBDIVISION IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS
PART OF THE SOUTHWEST 1/4 OF SECTION 8, TOWNSHIP 12 SOUTH, RANGE 25 EAST



DESCRIPTION:
ALL THAT PART OF THE SOUTHWEST 1/4 OF SECTION 8, TOWNSHIP 12, RANGE 25, NOW IN THE CITY OF MISSION, JOHNSON COUNTY, KANSAS, MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT A POINT ON THE NORTH LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 8 AND 1,155 FEET EAST OF THE NORTHWEST CORNER THEREOF, AS MEASURED ALONG SAID NORTH LINE, SAID POINT ALSO BEING ON THE CENTERLINE OF BARKLEY, AS NOW ESTABLISHED, THENCE SOUTHERLY, ALONG A LINE PERPENDICULAR TO THE NORTH LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 8, AND ALONG THE CENTERLINE OF SAID BARKLEY, A DISTANCE OF 380 FEET; THENCE EASTERLY, ALONG A LINE PARALLEL TO THE NORTH LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 8, A DISTANCE OF 25 FEET, TO A POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF SAID BARKLEY, SAID POINT ALSO BEING THE TRUE POINT OF BEGINNING OF SUBJECT TRACT; THENCE CONTINUING EASTERLY ALONG A LINE PARALLEL TO THE NORTH LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 8, A DISTANCE OF 150 FEET; THENCE SOUTHERLY, ALONG A LINE PERPENDICULAR TO THE LAST DESCRIBED COURSE, A DISTANCE OF 208 FEET, TO A POINT ON THE NORTHERLY RIGHT-OF-WAY LINE OF MARTWAY (60TH STREET), AS NOW ESTABLISHED, THENCE WESTERLY, ALONG A LINE PARALLEL TO THE NORTH LINE OF THE SOUTHWEST 1/4 OF SAID SECTION 8, AND ALONG THE NORTHERLY RIGHT-OF-WAY LINE OF SAID MARTWAY, A DISTANCE OF 150 FEET, TO A POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF SAID BARKLEY; THENCE NORTHERLY, ALONG A LINE PERPENDICULAR TO THE LAST DESCRIBED COURSE, AND ALONG THE EASTERLY RIGHT-OF-WAY LINE OF SAID BARKLEY, A DISTANCE OF 208 FEET, TO THE TRUE POINT OF BEGINNING OF SUBJECT TRACT.

THE ABOVE DESCRIPTION HAS BEEN TAKEN FROM AN ALTA COMMITMENT FOR TITLE INSURANCE, ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY BY ITS AGENT, THOMAS-AFFINITY TITLE, LLC, FILE NO. 233788, DATED JANUARY 25, 2023.

PLAT DEDICATION:
THE UNDERSIGNED OWNER(S) OF THE PROPERTY DESCRIBED HEREIN HAS/HAVE CAUSED THE SAME TO BE SUBDIVIDED IN THE MANNER SHOWN ON THIS PLAT AND THE PROPERTY SHALL HEREAFTER BE KNOWN AS:
"FINAL PLAT OF SWIG - 5959 BARKLEY"

DEDICATION:
ALL EASEMENTS, STREETS, ALLEYS, ACCESS CONTROL AND ALL OTHER PUBLIC AREAS NOT PREVIOUSLY DEDICATED SHALL BE MADE AND UNDERSIGNED BY THE OWNERS.

EASEMENT INFORMATION:
ALL EASEMENT INFORMATION SHOWN HEREON HAS BEEN TAKEN FROM AN ALTA COMMITMENT FOR TITLE INSURANCE, ISSUED BY FIRST AMERICAN TITLE INSURANCE COMPANY BY ITS AGENT, THOMAS-AFFINITY TITLE, LLC, FILE NO. 233788, DATED JANUARY 25, 2023.

FLOOD INFORMATION:
ACCORDING TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY'S FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NO. 20091C0023G, DATED AUGUST 3, 2009, THIS PROPERTY LIES WITHIN ZONE "X", AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN THEREON.

ZONING INFORMATION:
THE SUBJECT PROPERTY IS ZONED C-2A: PEDESTRIAN ORIENTED BUSINESS.

NOTES:
1. THE SUBJECT PROPERTY ADDRESS IS 5959 BARKLEY STREET, MISSION, KANSAS.
2. THE SUBJECT PROPERTY CONTAINS 31,205 SQUARE FEET OR 0.716 ACRES, MORE OR LESS.

APPROVALS:
APPROVED BY THE PLANNING COMMISSION OF THE CITY OF MISSION, JOHNSON COUNTY, KANSAS THIS ____ DAY OF _____, 2023.

MIKE LEE, PLANNING COMMISSION CHAIR
KIMBERLY STEFFENS, PLANNING COMMISSION SECRETARY

APPROVED BY THE GOVERNING BODY OF THE CITY OF MISSION, JOHNSON COUNTY, KANSAS THIS ____ DAY OF _____, 2023.

SOLANA FLORA, MAYOR
ROBYN FULKES, CITY CLERK

IN WITNESS WHEREOF:
"OWNER NAME(S)", "RELATIONSHIP", HAS CAUSED THESE PRESENTS TO BE SIGNED THIS ____ DAY OF _____, 2023.

MARLENE J. LEINMILLER
NOTARY CERTIFICATION:
STATE OF _____)
COUNTY OF _____) S.S.
ON THIS ____ DAY OF _____, 2023, BEFORE ME, THE UNDERSIGNED NOTARY PUBLIC, PERSONALLY APPEARED MARLENE J. LEINMILLER, OWNER, TO ME PERSONALLY KNOWN, WHO, BEING BY ME DULY SWORN DID SAY THAT SHE IS THE OWNER OF THE PREMISES HEREON, AND THAT SAID MARLENE J. LEINMILLER ACKNOWLEDGED SAID INSTRUMENT TO BE HER FREE ACT AND DEED.
I HAVE HEREUNTO SET MY HAND AND AFFIXED MY NOTARIAL SEAL IN MY OFFICE THE DAY AND YEAR LAST WRITTEN ABOVE.
MY COMMISSION EXPIRES: _____
NOTARY PUBLIC
PRINTED NAME

DATE PREPARED: JULY 12, 2023

FINAL PLAT

ANDERSON
SURVEY COMPANY
1270 N.E. DELTA SCHOOL ROAD
LEE'S SUMMIT, MISSOURI 64064
(816) 246-5050

KANSAS STATE CERTIFICATE OF AUTHORITY, LS-3

ROBERT J. ANDERSON
LICENSED PROFESSIONAL SURVEYOR
KANSAS
1648
971972223
ROBERT J. ANDERSON, PLS #1648

AT A GLANCE

Applicant:
Kevin Klassen

Case Number:
#23-12

Location:
Riggs Road between 52nd and 53rd Street

Project Name:
Morrison Ridge Final Plat

Property ID:
KP425000000357; KP425000000351

Project Summary:
The applicant is requesting approval of the final plat for two properties that are currently vacant. The proposed plat splits the two current lots into four lots in preparation for construction of four new single-family homes.

Current Zoning:
R-1

Proposed Zoning:
N/A

Current Land Use:
Vacant

Staff Contact:
Karie Kneller, City Planner

Proposed Land Use:
Single-Family

N/A Public Hearing Required

Legal Notice:
N/A



BACKGROUND AND PROPERTY INFORMATION

The subject property is located at approximately Riggs Street, half a block north of 53rd Street and west of properties on the west side of Riggs Street. Each of the properties are .47 acres. The lots are zoned R-1 “Single-Family Residential” and are surrounded by R-4/RP-4 “Garden Apartment District” zoning on the west and R-1 zoning on the east with multi-family and single-family uses.

The original 1913 plat of Morrison Ridge Park includes the lots on the north (labeled 357-362) and lots on the south (351- 356), and provides a 40-foot right-of-way for a public street, “Florence Street,” that was not constructed. These lots are under ownership by the applicant. The original plat does not provide for public utility easements.

There is underground private storm water infrastructure that runs between two single-family homes at 5230 Riggs Street and 5234 Riggs Street, which currently empties into a storm water inlet at the back of the properties and into a drainage ditch to the west. Additionally, sanitary sewer infrastructure is located in the public right-of-way (Florence Street) and west of the subject properties.

PROJECT PROPOSAL

This re-plat will consolidate lots 357-362 and lots 351-356 and split the consolidated lots north to south to create four lots. Lot 1 and Lot 2 are north of the public right-of-way, and Lot 3 and Lot 4 are south of the public right-of-way. The original 40-foot right-of-way will remain public, but a private driveway is proposed to be constructed that will be privately maintained. Public utility rights-of-way are also proposed.

PLAN REVIEW AND ANALYSIS

Municipal Code

Mission municipal code at §440.260 states that Preliminary Plats shall be approved by the Planning Commission if it determines that:

A. Prior to consideration of any final plat, City staff shall determine whether dedication of right-of-way will be required.

Analysis: No additional right-of-way is required behind what was on the original plat.

B. Final plats shall be approved by the Planning Commission if it determines that:

1. The final plat substantially conforms to the approved preliminary plat and rule exceptions granted thereto.
2. The plat conforms to all applicable requirements of this Code, subject only to approved rule exceptions.

3. All submission requirements have been satisfied.
4. Approval of a final plat shall require the affirmative vote of a majority of the membership of the Planning Commission.

Analysis: The final plat conforms to the approved preliminary plat and conforms to requirements of the municipal code. Easement agreements between property owners at 5230 Riggs Street and 5234 Riggs Street were submitted as part of the final plat.

C. Following approval of the final plat by the Planning Commission, the final plat shall be submitted to the City Council for review of land proposed to be dedicated for public purposes. The City Council shall approve or disapprove the dedication of land for public purposes within thirty (30) days after the first (1st) meeting of the City Council following the date of the submission of the plat to the City Clerk. The City Council may defer action for an additional thirty (30) days for the purpose of allowing for modifications to comply with the requirements established by the City Council. No additional filing fees shall be assessed during that period. If the City Council defers or disapproves any such dedication, it shall advise the Planning Commission of the reasons therefor. No plat shall be filed with the Register of Deeds unless such plat bears the endorsement that the land dedicated to public purposes has been approved by the City Council

Analysis: The final plat, upon Planning Commission's recommended approval, will be considered by the City Council to approve the dedicated easements for stormwater utilities.

D. Final plats shall be recorded with the Register of Deeds within eighteen (18) months following City Council approval of land dedicated to public purposes. Final plats which are not recorded within said time period shall be deemed null and void.

Analysis: Staff will ensure the plat is recorded within the stipulated timeframe.

RECOMMENDATION

For the Preliminary Plat, the Planning Commission approved Case # 23-03, the Preliminary Plat for Morrison with the following conditions:

- A. Prior to submittal of the final plat, the applicant is required to obtain a permanent drainage easement from the existing property to the east of Lot 4 in order to tie into existing storm water infrastructure.
- B. Prior to submittal of the final plat, the applicant is required to obtain a permanent drainage easement for the existing property to the west of Lot 3 in order to install rip-rap.
- C. Prior to submittal of the final plat, the applicant is required to submit a storm water management study that documents existing versus proposed storm water flow and analysis that shows that the additional storm water flow does not have an adverse impact on downstream properties.

D. The final plat shall note that the private drive shall be maintained in perpetuity by the property owner(s) of Lots 1-4.

E. A maintenance agreement shall be recorded with the Johnson County Register of Deeds.

Analysis: The above conditions A-E have been satisfied with the submittal of this final plat.

Staff recommends that the Planning Commission recommend approval of Case #23-12, the Final Plat of Morrison Ridge to the City Council with the following conditions:

1. The property owner will attain an easement agreement with adjacent property owner to the west (5285 Foxridge Drive) to gain access and to install rip rap for drainage on the adjacent property; construction permitting is contingent upon this easement agreement and feasibility of construction details.
2. Permitting drawings for building construction shall be submitted to the City of Mission for review and approval prior to construction.
3. Permitting drawings for stormwater utility construction shall be submitted to the City of Mission for review and approval prior to construction.

PLANNING COMMISSION ACTION

The Planning Commission will hear Case #23-12, the Final Plat of Morrison Ridge, at its September 25, 2023 meeting.

CITY COUNCIL ACTION

The City Council will hear Case #23-12, the Final Plat of Morrison Ridge, at its October 18, 2023.

PERMANENT DRAINAGE EASEMENT

THIS AGREEMENT, made and entered into this 25TH day of AUGUST, 2023, by and between TOWER PROPERTIES COMPANY, a _____, hereinafter called Grantor, and the **CITY OF MISSION, KANSAS**, a Municipal Corporation, located in the County of Johnson, State of Kansas, hereinafter called Grantee.

NOW, THEREFORE, for the consideration hereinafter described, the parties hereto agree as follows:

SECTION ONE
GRANT OF PERMANENT EASEMENT

In consideration of One and No/100 Dollar (\$1.00), in hand paid and other valuable consideration, including just compensation paid for all property damage resulting from the public improvement and from those factors set forth in K.S.A. 26-513 and other factors arising from the public improvement to be made, including but not limited to increased water run-off or drainage; loss of trees and landscaping; and erosion, receipt of which is hereby acknowledged, Grantor hereby grants and conveys to Grantee, its successors and assigns, a permanent drainage easement with reasonable rights to ingress and egress in, on, over, under and through the following described real estate, for the purpose of laying, constructing, operating, inspecting, altering, repairing, replacing, substituting, relocating, adding to, removing and maintaining a culvert, storm sewer, drainage ditch, drainage channel/water course or other drainage facility tributary connections and appurtenant work in any part of said easement, including the right to clean, repair, replace and care for said drainage facilities, together with the right of access in, on, over, under and through said easement for said purposes, said easement more particularly described as follows:

SEE EXHIBIT "A" ATTACHED HERETO
AND BY REFERENCE MADE A PART HEREOF.

SECTION TWO

06/28/2023

1

TERM

The rights granted herein shall be possessed and enjoyed by Grantee, its successors and assigns, so long as the drainage line and appurtenances constructed pursuant hereto shall be maintained and operated by Grantee, its successors and assigns.

SECTION THREE ADDITIONAL RIGHTS OF GRANTEE

Grantee shall have the right to change the drainage channel/water course and to install an additional sewer and/or drainage line or replace said line with a larger line in the above-described easement at some future date and under the same conditions as the original drainage line was installed, except no additional payment shall be made for the purchase of said right.

Grantee and its employees and agents shall have free access to the drainage easement, using such reasonable route as Grantor may designate or approve.

SECTION FOUR CONDITIONS

Grantor shall not construct or permit to be constructed any permanent house or structure, landscaping, trees, or obstruction on or over or interfering with the construction, maintenance, or operation of any sewer and/or drainage facility or appurtenance constructed pursuant to this instrument. Grantor further agrees that it will not change the grade of the area within the Permanent Easement without approval of the City.

SECTION FIVE WARRANTY OF TITLE

Grantor covenants that it is the owner of the premises and has the right, title and capacity to grant the easement granted herein.

SECTION SIX RESTORATION OF SURFACE

Grantee, its agents, successors and assigns shall, as soon as practicable, after construction of said drainage facility and all subsequent alterations and repairs thereto, restore the property to as near as possible to the conditions set out in the project plans and specifications.

SECTION SEVEN EFFECT OF AGREEMENT

This Agreement shall be binding upon the heirs, legal representatives, successors and assigns of the parties hereto.

IN WITNESS WHEREOF, the parties hereto have executed this agreement the day and year first above written.

06/28/2023

3

41


GRANTEE:

CITY OF MISSION, KANSAS,
A Municipal Corporation

By: 
Solana Flora, Mayor

ATTEST:

(SEAL)


Robyn Fulks, City Clerk

ACKNOWLEDGMENT

STATE OF KANSAS)
) ss:
COUNTY OF JOHNSON)

BE IT REMEMBERED, that on this 5 day of September, 2023, before me, the undersigned, a Notary Public in and for the County and State aforesaid, came Solana Flora, Mayor of the City of Mission, Kansas, and Robyn Fulks, City Clerk of said City, who are personally known to me to be the same persons who executed, as such officers, the within instrument on behalf of said City, and such persons duly acknowledged the execution of the same to be the act and deed of said City.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my seal the day and year first above written.




Notary Public

Printed Name: Nilo Fanska

My Appointment Expires:
12-12-2024

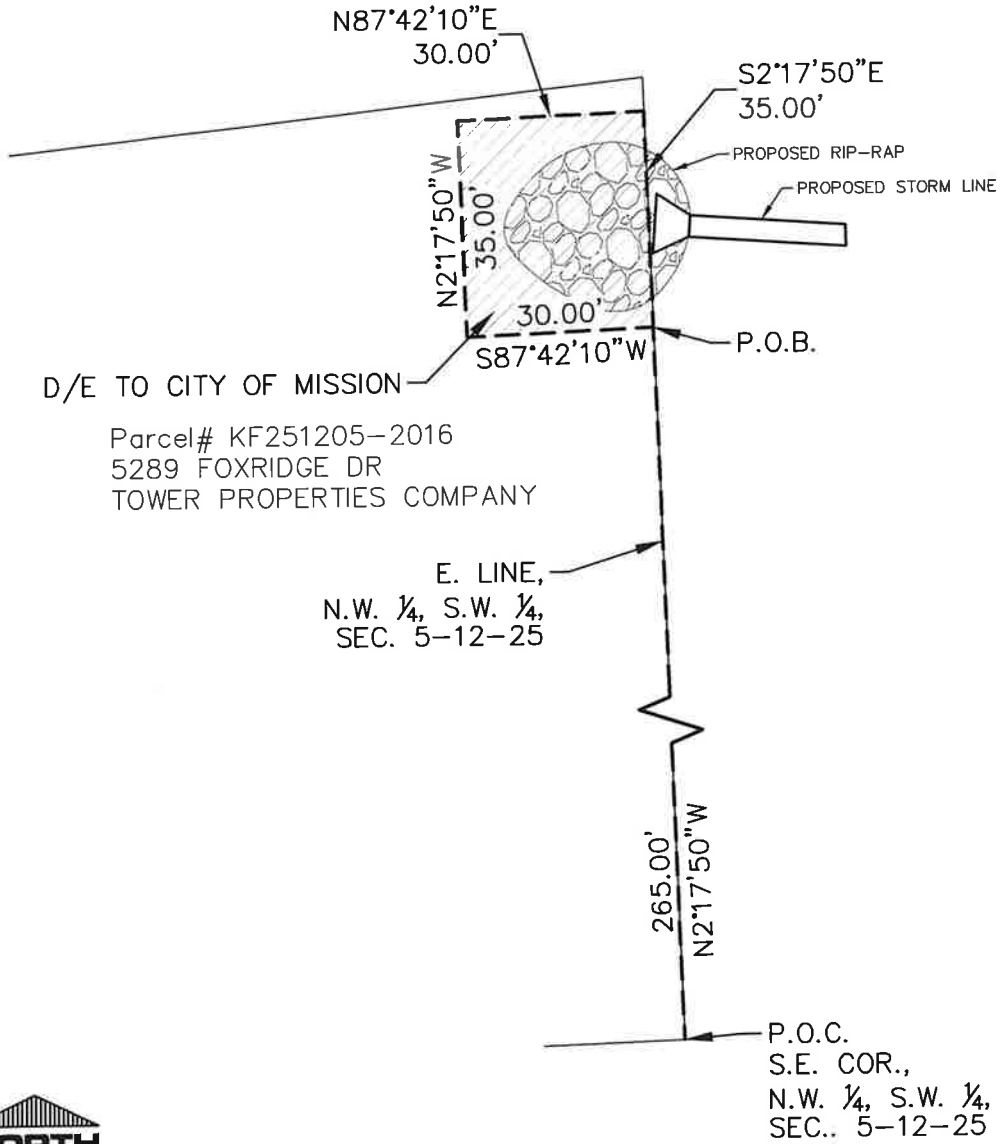
EXHIBIT A
Sheet 1 of 2

LEGAL DESCRIPTION

All that part of the Northwest Quarter of the Southwest Quarter of Section 5, Township 12 South, Range 25 East, in the City of Mission, Johnson County, Kansas, described as follows:

Commencing at the Southeast corner of the Northwest Quarter of the Southwest Quarter of said Section 5; thence North 2 degrees 17 minutes 50 seconds West, along the East line of the Northwest Quarter of the Southwest Quarter of said Section 5, a distance of 265.00 feet to the point of beginning; thence South 87 degrees 42 minutes 10 seconds West, a distance of 30.00 feet; thence North 2 degrees 17 minutes 50 seconds West, a distance of 35.00 feet; thence North 87 degrees 42 minutes 10 seconds East, a distance of 30.00 feet to a point on the East line of the Northwest Quarter of the Southwest Quarter of said Section 5; thence South 2 degrees 17 minutes 50 seconds East, a distance of 35.00 feet to the point of beginning.

EXHIBIT A
Sheet 2 of 2



SCALE: 1"=30'



CIVIL ENGINEERS
LAND SURVEYORS - LAND PLANNERS

122 N. WATER STREET
OLATHE, KANSAS 66061
PHONE: (913) 764-1076
FAX: (913) 764-8635

14 W. PEORIA
PAOLA, KANSAS 66071
PHONE: (913) 557-1076
FAX: (913) 557-6904



MEMORANDUM

To: Chairman Lee and Members of the Planning Commission

From: Brian Scott, Deputy City Administrator – Planning and Development Services

Date: September 20, 2023

Regarding: [“tomorrow together” Comprehensive Plan](#)

Comprehensive plans have long been used as a tool for planning the future growth of cities. The traditional methodology for a comprehensive plan has been to examine current trends in population growth, business development, transportation systems, land use, and community facilities and then to develop a vision for what the city may look like at some point in the future. Based on that vision, recommendations are then developed for a systematic approach to the future growth of the city that may include future annexation of territory (if necessary), specific land uses and zoning, extensions or upgrades of roads and infrastructure, and location of community facilities such as parks and fire stations.

Mission’s first comprehensive plan was adopted in 1968. Some of the key recommendations from this plan included:

- *Of the remaining land available for development, some areas will be subject to market pressures for both residential and non-residential development. In view of the most recent trends of housing patterns in northeast Johnson County, the City should seriously consider providing additional areas for multi-family apartments and townhouses.*
- *Mission has a better than average potential for industrial development. The City is strategically located within the Metropolitan complex, and is well served by major highways. Mission can use these assets to encourage development of high-quality industrial, research, and distribution type enterprises.*
- *Provision of additional community facilities (schools, parks, police and fire protection, libraries, etc.) should be programmed so that the construction is in advance of ultimate need, but in consonance with anticipated tax revenues.*
- *Traffic movement, a major headache on some streets in the City, can be solved if the City adopts the changes to the traffic pattern suggested in this report, and improves and upgrades selected key sections of the local distribution network. This would include widening, repaving, and installing curb and gutter along main, local collectors and arterials.*
- *Provision of the sanitary sewer, and storm water sewer systems must be planned and programmed well in advance of construction. The City must work with the sewer districts management in establishing the necessary physical system needed to accommodate the projected development of the City.*

Subsequent comprehensive plans were adopted in 1995, 1999, 2007 and 2011.

The City embarked on the most recent update to its comprehensive plan in 2019. A request for proposals (RFP) was developed by staff with input from the Planning Commission and sent to prospective planning firms as well as advertised on the American Planning Association's website.

Proposals were evaluated by a selection committee and the top five firms were invited to the city for interviews. Confluence was ultimately selected based on their planning experience, team make-up, knowledge of our community, and the fact that they had recently completed similar studies for two neighboring communities (Roeland Park and Merriam). The City Council approved a contract with Confluence at the end of 2019 and the study began in March of 2020.

The project kick-off was a joint work session with the City Council and the Planning Commission to understand the purpose and components of a comprehensive plan. The joint work session was held on the eve of the COVID-19 pandemic.

Because of the rapidly evolving turn of events with the pandemic and stay-at-home orders issued by the Governor the project was temporarily paused. The project resumed in the late summer of 2020 with the appointment of a steering committee made up of the following individuals:

Cathy Boyer-Sheshol	Sustainability Commission/CFAA
Cherron Williams	Resident
Debbie Kring	City Council, Ward III
Jacque Gameson	Parks, Recreation and Tree Commission
Josh Thede	Sustainability Commission
Lolly Cerda	Resident
Pete Christiansen	Planning Commission
Robin Dukelow	Planning Commission
Robynn Haydock	Multi-family properties/Not-for Profit
Sollie Flora	City Council, Ward IV
Steve Corwine	Downtown property owner
Stuart Braden	Planning Commission
TJ Roberts	Downtown business owner/resident

A formal community kick-off meeting was held in October when the project website was unveiled. Community engagement was severely limited due to the social distancing requirements of the ongoing coronavirus pandemic. The project web page was able to fill that void by offering several on-line engagement tools including an interactive pin-map, visual preference survey, and budgeting tool.

Due to social distancing requirements, the steering committee held many of its meetings virtually for the first year. The committee learned about the current demographic and economic make-up of the city, reviewed responses from the on-line engagement tools, discussed ideas about current development patterns and what they would like to see, and considered other topics around sustainability, transportation and mobility, and housing. The steering committee began to meet in

person during the summer of 2021 to formulate a vision statement and develop recommendations in each of the key areas of the plan.

The vision statement and recommendations were presented to the public in an open house held in November of 2021. A final draft of the plan – known as “tomorrow together – 2040 Mission comprehensive plan” - was completed and presented to the City in the winter of 2022.

Due to the departure of the City Planner and the hiring of a new one, and then an onslaught of development applications in 2022, review of the draft plan took longer than anticipated. There have been several iterations of the draft plan that have resulted in the one that is presented this evening to the Planning Commission.

Because the City is nearly completely developed and landlocked, the “tomorrow together” comprehensive plan takes different view toward the future of Mission than the first plan the City adopted in 1968. Key themes from this plan include:

- Preservation of the natural environment through better storm water management practices, conservation and expansion of green space, and greater emphasis on sustainability measures that will reduce the community’s overall carbon footprint
- Enhance mobility throughout the community for pedestrians, bicyclists, transit users, and others by creating stronger connections, slowing traffic, and providing greater safety
- Support of a variety of housing options in the community by preserving existing housing stock while allowing for development of new housing stock that is appropriate for neighborhoods or commercial areas based on density and design
- Encourage continued economic prosperity for the community by supporting existing businesses in the community and development of new businesses that align with the long-term vision for the community

Each of these key themes are more fully explored in the six chapters of the “tomorrow together” plan:

- Natural Features and Environment
- Parks and Recreation
- Transportation and Mobility
- Economic Revitalization
- Housing and Neighborhoods
- Infrastructure Maintenance and Enhancements

Chapter 10 of the plan is the Implementation Plan where the recommendations are summarized and given a priority ranking. In doing a final review of the Implementation Plan, it was noted that changes to some of the priority rankings and time frames was not updated in the plan that was posted and shared with the Planning Commission a few weeks ago. A corrected version is in the packet and a matrix has been developed to indicate where changes were made. For most part changes reflect an increase in the ranking where “low” became “medium” and “medium” became “high.”

There are two appendices to the plan. Appendix A provides an analysis of the existing conditions of the community including population trends, demographics, housing, and economy. Appendix B provides a summary of community input received from the on-line engagement tool as well as the open house that was held in November of 2021.

It is important to note that while the “tomorrow together” plan is a snapshot in time, it is not a static document. Rather, comprehensive plans, including this one, are meant to be living documents that will evolve over time as the community does. Ideas and concepts presented in the plan are meant to provide context for the recommendations that were based on extensive public or stakeholder input and professional experiential knowledge to stimulate further discussion and analysis.

Recommendations presented in the plan will require further review, analysis, and discussion based on changes in demographics, updated studies, continued public input, and on-going implementation. Implementation of the plan will ultimately be achieved through some action of the City such as a new service or program, a policy directive, or code change. Though we are completing the plan, we are just beginning the journey.

Attachments

Tomorrow Together Comprehensive Plan

Appendix A – Existing Conditions Report

Appendix B – Public Input Summary

Matrix Indicating Updates to Priority Rankings In the Implementation Plan

Appendix B

PUBLIC INPUT SUMMARY

Table of Contents

1.0 Overview	242
2.0 Public Input Summary.....	244

1.1 Schedule and Logo

Public Participation Overview

The Tomorrow Together Plan includes a variety of public input from a wide swath of the community. The Public Participation Plan includes the following activities and meetings:

- Use of a comprehensive plan steering committee
- Key stakeholder interviews
- Public workshop
- Interactive engagement website (6 activities)
- Social media polling

This report summarizes the public engagement methods utilized as well as the feedback received during the engagement process completed for the Tomorrow Together Comprehensive Plan. Most of the planning process took place during the COVID-19 pandemic, which required socially distant engagement. The City was able to utilize robust online engagement strategy to help replace in-person workshops and meetings.

Public Engagement Schedule

Below is a graphic showing the planned public input schedule across the four plan phases. The meetings on the top show Comprehensive Plan Steering Committee Meetings as well as Joint Workshops. The bottom half show public meetings, surveys and public hearings.

Project Brand/Logo

To help create a recognizable vision for the Tomorrow Together planning process, a logo and color scheme were created in Phase 1. The Consultant Team came up with several possible

options that were then presented to the Comprehensive Plan Advisory Committee and City staff. Around the same time as the planning process began, the City of Mission was finalizing the details of a city-wide rebranding process including a new logo. To help promote this change, a decision was made to incorporate the color scheme and imagery of the new logo into the plan logo. The project logo and color scheme are shown below.

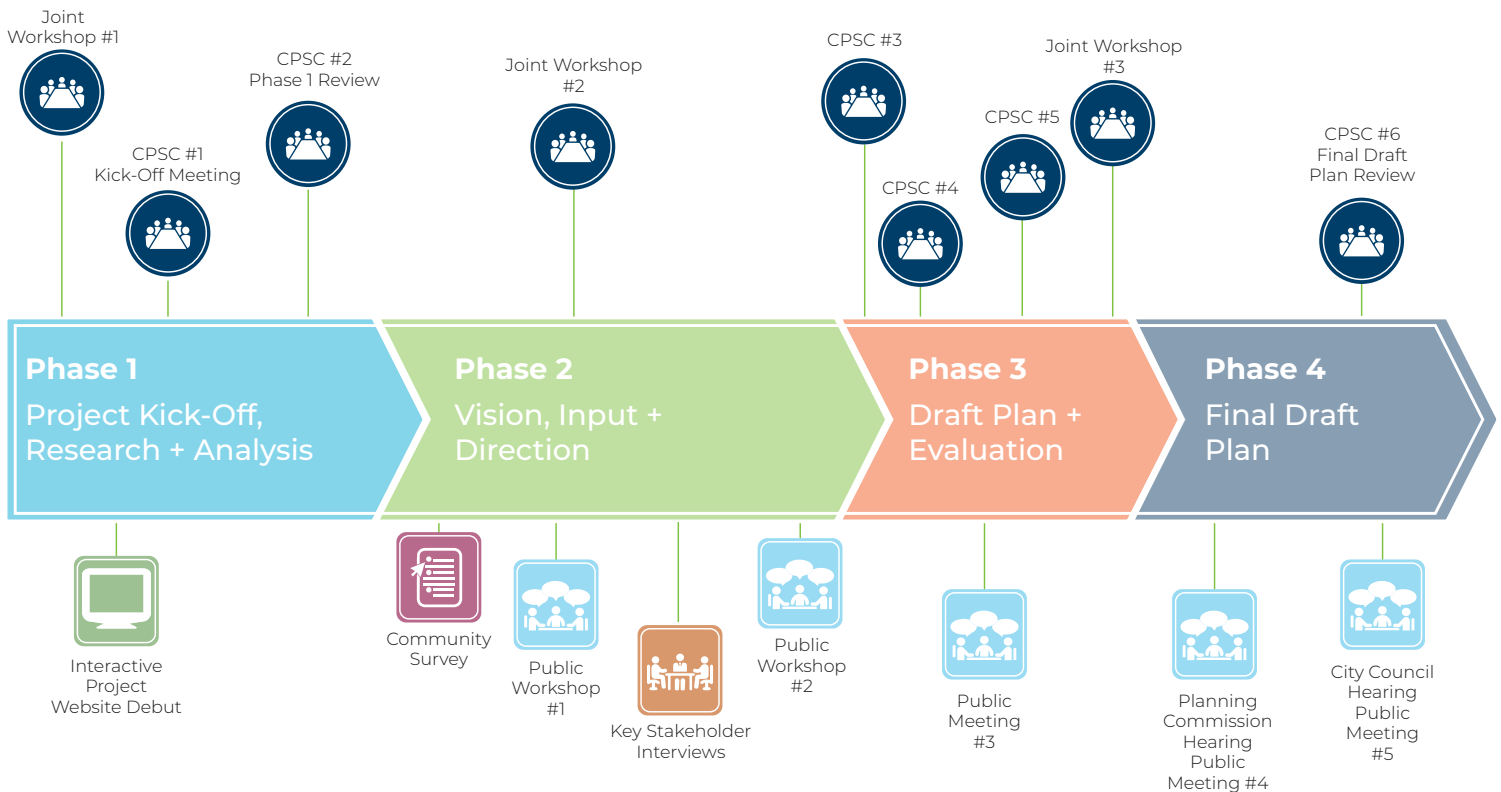


Figure 1 - Public Engagement Schedule

1.2 Comprehensive Plan Steering Committee

Roles and Responsibilities

The Tomorrow Together Comprehensive Plan Steering Committee (CPSC) was comprised of community stakeholders and leaders. This Committee's role was to serve as an advisor to City Staff and the consultant team by providing initial feedback on the plan's direction and priorities, identify areas of concern and opportunities, and evaluate and consider public input. The Committee was also asked to review the various drafts of the plan components and to deliver final recommendations for consideration by the Planning Commission and City Council as part of their review and adoption of the Comprehensive Plan Update. The Committee further serves the important role as a community ambassador for the plan update - promoting awareness of public input events, encouraging participation, and ultimately, advocating for the plan's goals and priorities.

Schedule

The Steering Committee is anticipated to meet approximately 6 times over the next 12 months. Most of these meetings will be in the evening and will last approximately 2 hours. The consultant team will generally conduct the meetings, make presentations, and provide handouts. The meetings will be interactive with feedback requested from all committee members. The committee members may be asked to review materials and plan drafts prior to the meeting.

Steering Committee Members

The Tomorrow Together Comprehensive Plan Steering Committee was composed of the following individuals:

- Cathy Boyer-Shershol, Sustainability Commission/CFAA
- Cherron Williams, Resident
- Debbie Kring, City Council Ward III
- Jacque Gameson, Parks, Recreation and Tree Commission
- Josh Thede, Sustainability Commission
- Lolly Cerda, Resident
- Pete Christiansen, Planning Commission
- Robin Dukelow, Planning Commission
- Robynn Haydock, Multi-family Properties/Not-for-Profit
- Sollie Flora, City Council Ward IV
- Steve Corwine, Downtown property owner
- Stuart Braden, Planning Commission
- TJ Roberts, Downtown business owner/resident

2.1 CPSC Meeting Summary



CPSC KICK-OFF MEETING - AUGUST 6TH 2020

Kick-Off Meeting Vision Exercises Results

A kick-off meeting for the Tomorrow Together Comprehensive Plan was held on August 6, 2020. Due to social distancing guidelines put in place for COVID-19, the meeting was held virtually. The meeting provided an overview of the planning process/schedule and initial demographic data analysis. At the end of the presentation, a series of engagement and visioning exercises were completed to gain initial feedback from the steering committee. The results of the exercises are summarized below.

Postcard Exercise

Steering Committee members were asked to fill-out a postcard exercise with the following prompt:

“DEAR FRIEND/FAMILY, IT IS THE YEAR 2040 AND YOU SHOULD VISIT ME HERE IN MISSION BECAUSE...”

The excerpts to the right and on the following page were responses given by the CPSC.

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

Walkable, bikable, transit accessible, eclectic local unique family owned business along johnson drive, mom and pop stores, lots of character, vibrancy

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

Bike share or the bird scooter idea

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

You will be amazed to enjoy some much so close to our home... parks, museums, great dining, unique shopping. A place for all ages to enjoy.

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

we have wonderful old trees that fill our yards and neighborhoods. The city is incredibly walkable - we can walk downtown - spend the entire day beginning at one end - shopping, eating, and recreation. You'll love the family owned businesses with the home-town feeling. Lots of fun - food - and scenery.

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

We have plenty of dining and recreational amenities all with walking distance as well as downtown stores. Can stay at one of our nice and affordable hotels. Also very friendly residents and a good home base for visiting other local attractions

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

we can spend the whole day on Johnson Drive; shopping, dining, day drinking, visiting friends and then walk home

2.1 CPSC Meeting Summary



MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

You will be amazed to enjoy some much so close to our home... parks, museums, great dining, unique shopping. A place for all ages to enjoy.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

we have wonderful old trees that fill our yards and neighborhoods. The city is incredibly walkable - we can walk downtown - spend the entire day beginning at one end - shopping, eating, and recreation. You'll love the family owned businesses with the home-town feeling. Lots of fun - food - and scenery.

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we can spend the whole day on Johnson Drive; shopping, dining, day drinking, visiting friends and then walk home

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

We have increased diversity and created a more inclusive community with space bikes and cultural activities

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

A vibrant, diverse community with a thriving downtown that offers a lot of vitality and neighborhoods that offer a mix of families and housing options.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

There's a flavor of Mission that's unlike any other. From small locally owned business to amazing places to eat from! There's always something to see and do in Mission.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

A vibrant, diverse community with a thriving downtown that offers a lot of vitality and neighborhoods that offer a mix of families and housing options.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

We're a leader in sustainability and you can come see cutting edge environmental solutions.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

Each local business has unique items you can only find in Mission and the shop owners are so friendly. Our favorites are coffee, dessert, eco-friendly houseware, and a tool library. You won't go hungry or thirsty while you're here with the great restaurants, fresh produce, micro-breweries and wine bars. Car-less Thursdays street parties in the downtown district are always so much fun with the farmer's market, live music, local businesses staying open late, and neighbors spending time together building community. It's especially fun to see community members of all ages: young professionals, families, and retirees enjoying time together. We walk from our house every Thursday that we are free. The downtown area is full of life almost every night of the week in part due to all of the new workforce housing. Now all of the local business employees can live and play in the same place they work. We love it here and you will too!

PLACE STAMP HERE

Key Themes and Takeaways

Some key themes and takeaways from the steering committee's postcard exercise results included:

- Walkable and bikeable
- Vibrant local businesses with character
- Johnson Drive
- Amenities close to home
- Tree filled streets
- Fun, food, and scenery
- Dining
- Recreation
- Family-friendly
- Diverse community
- Neighborhoods
- Sustainability

2.1 CPSC Meeting Summary



VISIONING EXERCISES

After the postcard exercise, there were four visioning exercise questions.

If this new plan could only accomplish one thing, what would it be?

- Keep Mission’s infrastructure strongly intact and up to date.
- Affordable housing/density.
- Climate action and resilience.
- Walkability.
- Come up with an actionable plan to increase diversity.
- When you enter Mission, you see vibrant, well-maintained buildings with great curb appeal and little vacancy.
- Create a truly walkable/bikeable city.
- Walkability (sidewalks and trails) throughout the city.
- Set of guiding principles for the future of Mission.
- Encourage more diversity in not only ethnicity but age groups as well as other social groups.
- City infrastructure redevelopment overhaul that’s progressive and clean.
- Promote new development that fits within the identity.

What do you think the biggest challenge in creating a plan in Mission will be?

- Landlocked/lack of opportunities for new development, how to shape existing landscape to better fit modern needs and wants.
- Slow down traffic.
- In light of COVID - funding.
- Overcoming the perception that Mission is only for starter homes/families or older people.
- Addressing the “affordability” and what that term means to the community.
- Infrastructure of odd streets, old sewers and power lines and tough to maneuver thru town.
- Agreeing which topics are most important to everyone.
- To think ahead of time as we weren’t prepared for Covid, what else aren’t we preparing for?
- Keeping the goals to something that is attainable.
- Getting East Gateway completed, occupied, and income producing.
- NIMBYs and “I don’t want to pay for that.”
- Greenfield development in outer suburbs (Olathe, Lee’s Summit, OP, etc) acts as a disincentive for density in Mission and inner-ring suburbs and NIMBYs.

2.1 CPSC Meeting Summary



CPSC KICK-OFF MEETING - AUGUST 6TH 2020

What is your favorite thing about living in Mission?

- Johnson Drive business corridor
- Convenience of location within the metro and having a variety of services nearby in Mission
- Proximity to everything - Mission and regionally
- The community.
- Small town feel with big city amenities.
- Small town feel with easy access to airport, Plaza and Downtown KCMO.
- How walkable shops and restaurants are on Johnson Drive.
- Walkability to Johnson Drive/community character/affordability.
- The walkability and functionality of the business and where they are. The community wide engagement is great.
- Diversity of businesses, housing options, and residents.
- Being a member of a friendly, supportive residential community.
- Evenings relaxing with community such as Mission Market nights. It's great because it's walk-able and near green park space. Great to enjoy the outdoors and the local shops.

Do you think there are negative perceptions about Mission this plan should seek to address?

- Even though we know Mission is special, the view from the outside that it's just another suburb, blends into the next
- I think there are a variety of ideas about development; some want to remain single-family and some think we should become more densely populated.
- Old and not hip.
- Currently the Gateway project.
- "The Gateway" is not a typical development.
- Tough to get thru town so they just stay on SMPkwy, not good places to dine or be entertained, so keep going to Plaza or Crossroads.
- I'm not sure a lot of people know how entertaining the city is.
- I'd say more diverse businesses and retailers. Business involvement with the community of Mission.
- Mission is not dead. Don't overlook us.
- Mission lags behind other Johnson County cities in development and future planning.
- There's still fallout and distrust around driveway tax, gateway. City could go bankrupt from infrastructure cost, a strong towns approach may be good.
- Lack of diversity.

2.2 Key Stakeholder Interviews



KEY STAKEHOLDER INTERVIEWS - OCTOBER 2020

An important engagement tool used in the development of this comprehensive plan was the completion of key stakeholder interviews. Members of the planning met virtually with several different members of the Mission community representing a broad swath of residents, business owners, school officials, and city leaders. The small group settings these conversations took place in, helped provide a comfortable and open environment for participants to speak freely about any strengths, weaknesses, opportunities, and challenges facing Mission both right now, and in the years to come. The feedback gained through these conversations were critical in the consultant team’s understanding and comprehension of the City of Mission.

Groups represented in the key stakeholder interviews include:

- Rushton Elementary
- Long-time residents
- Former City officials
- Local developers
- Transportation advocates
- Business community

Major themes to come from the stakeholder interviews include:



GROWTH AND DEVELOPMENT

STRENGTHS

- Keep encouraging increase in density, mixed-use developments, and walkability.

GROWTH AND DEVELOPMENT

CHALLENGES

- Poor quality of development along Johnson Drive - fast food.
- Long term viability of retail along Johnson Drive in the age of COVID and online retailers.
- Not enough parking.
- Infill projects are challenging.
- Effect of empty storefronts on surrounding businesses.
- West Gateway project.
- Form Based Code can be problematic - but important to have clear vision and guidelines for development.
- Vertical mixed-use can be challenging to make the numbers work.
- “Red carpet” is not displayed for developers.
- Finding the right balance for development.

GROWTH AND DEVELOPMENT

OPPORTUNITIES

- Keep focusing on Downtown.
- Businesses benefit from pedestrian activity.
- Develop business improvement plan - maintenance of businesses.
- City should actively reach out to developers interested in Mission.
- Reinstate facade improvement program.
- Encourage experiential retail.
- Look at flexible ways to regulate mixed-use projects.
- Retail needs to be innovative.
 - Convenient curb-side pick-up
 - Expand parking in the rear of buildings

2.2 Key Stakeholder Interviews



KEY STAKEHOLDER INTERVIEWS - OCTOBER 2020

TRANSPORTATION AND INFRASTRUCTURE

STRENGTHS

- Walkability along Johnson Drive.
- Infrastructural/aesthetic upgrades to Johnson Drive made a big difference.
- Overall, infrastructure in Mission is good.
- Sidewalks and cross walks are in good shape.
- Johnson Drive is the centerpiece of Mission - provides image and reputation to citizens, travelers, and visitors.
- Bike lanes on Lamar are nice - need more of this.

CHALLENGES

- Traffic volumes are a concern - removing lanes on Johnson Drive could be challenging?
- Crossing Johnson Drive as a pedestrian is not as safe as it could be.
- Aging residents - need to provide services they can afford - Easy Ride.
- Mission has a great core of a Downtown - but Johnson Drive still feels like a suburban trafficway - how to improve?

OPPORTUNITIES

- Look at infill opportunities - family co-ops, accessory buildings, granny flats.
- Important to keep sliding-scale assistance programs.
- Find ways to keep older housing competitive.
- Retain people who live here so they trade-up in the market.
- Renting by choice is big right now.
- Embrace empty nesters who moved away and now are coming back.
- Offer classes/tool-sharing to help homeowners.

HOUSING

STRENGTHS

- Housing
- Smart to focus on senior living and multi-family.
- Diversifying housing is important.
- Existing homes rehabilitations and updates are good - encourage this.
- Eclectic housing stock is an asset.

CHALLENGES

- Need age-in-place housing options.
- Perception that Mission isn't friendly towards renters/multi-family.
- Not a lot of homeowner organizations.
- Code enforcement is always an issue.

OPPORTUNITIES

- Bring Metcalf Avenue down to grade at Johnson Drive - enhance the West Gateway area.
- Need more improvements along Johnson Drive (landscaping, small parks).
- Transit Center is not used enough - encourage multi-modal transportation services.
- Explore reducing travel lanes where possible.
- Mission needs to continue to invest in infrastructure and quality of life.

2.2 Key Stakeholder Interviews



KEY STAKEHOLDER INTERVIEWS - OCTOBER 2020

REPUTATION AND CHARACTER

STRENGTHS

- “All roads lead to Mission.”
- Residents tend to live here a long time.
- Mission has a cowboy attitude - do what they want.
- Diversity of personalities.
- Mission’s location within the metro is a major asset.
- Retail space has always been affordable.
- Amenities of a larger city.
- Good parks and recreation, public works.
- Downtown is an asset for the community - provides identity.
- Mission has a history of setting good goals and getting them done.
- Neighborhoods are improving and are more diverse.
- Strong quality of life.
- Hometown feel.
- Character and charm of Johnson Drive - “face” of the community.
- Diverse restaurants.
- School district has a good relationship with the city.

CHALLENGES

- Don't “sanitize” too much - needs to stay authentic.
- Retaining Mission’s family feel.
- Home prices are too high.
- Would like to see more kids around.
- Developing in Mission is difficult - not responsive - no formality.
- Stay ahead of trends.
- Don't lose post office.

CITY ADMINISTRATION AND SERVICES

STRENGTHS

- City is focusing on what they can do like public works, parks, etc.
- The market, community center and programs/events are all good.

CHALLENGES

- Don't try to control everything.
- This plan shouldn't start from the beginning - don't reinvent the wheel.
- Public meetings tend to bring out dominant voices - make sure we listen to everyone.

OPPORTUNITIES

- Build on momentum of past administrations.
- City should use tools like CID/TIF to encourage responsible development.
- Council of NE Johnson County Mayors can be helpful.
- Some consolidations of services might help (6 police departments in NE Johnson County).
- Don't do anything that will keep Mission from reaching its full potential.

2.3 Virtual Public Workshop



VIRTUAL PUBLIC WORKSHOP - OCTOBER 1, 2020

Public Workshop Overview

A public workshop for the Tomorrow Together Comprehensive Plan was held on October 1, 2020. Due to social distancing put in place for COVID-19, the event was held virtually, recorded and uploaded to the project website. The results of the public workshop input was summarized on the following pages.

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

It is a diverse and community centered city.

mission

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

Great neighborhood. We walk to all of our favorite places. Coffee shops, restaurants, fun activities. Great neighbors and community feel.

mission

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

It's the place to visit in Kansas City

mission

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

We have great restaurants and shops

mission

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

The City has finally developed its East Gateway project and the commercial district includes mix use, beautiful apartments, restaurants and parks.

mission

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

We're a diverse, bustling cultural hub in the metropolitan area with beautiful trails, public transport, breweries, shops, and green space that beckon for all to visit for a day or a lifetime.

mission

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

You can get around the whole city by bicycle!

mission

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

It's a wonderful small town feel within minutes of downtown Kansas City, football and baseball teams, wonderful shopping. We also have great restaurants and interesting shops. Lots of trails, wonderful big tall trees. Walkable communities and friendly residents.

mission

PLACE STAMP HERE

2.3 Virtual Public Workshop



VIRTUAL PUBLIC WORKSHOP - OCTOBER 1, 2020

VISIONING EXERCISES

At the end of the public workshop, there were four visioning exercise questions.

If this new plan could only accomplish one thing, what would it be?

- Improve residential streets and increase convenient residents and commerce.
- Connect residents of all ages and abilities to important services and each other.
- Help Mission prioritize projects.
- Community for all ages to live in and enjoy doing their favorite things in.
- Safe and welcoming for ALL people.
- Draw from other areas of Johnson County to spend their money in Mission.
- Safe streets and living environment.
- Align Mission with the sustainable environmental recommendations in accords like the Paris agreement.
- More diversity in socio-economic status and race.

What do you think the biggest challenge of creating a plan in Mission will be?

- Getting buy-in from legacy residents.
- Economics of improving city/life goals.
- Prioritization of limited resources.
- Implementing the plan.
- Completing projects that are underway/Get developers to complete their projects.
- Leveraging diverse perspectives and peoples.
- Land locked.
- Funding major projects.

Do you think there are some negative perceptions about Mission this plan should seek to address?

- The Gateway seems to be a media joke.
- Combat cynicism amongst residents by touting real progress and tangible outcomes.
- Too many fast food places and hair salons.
- Perceptions of Johnson county as full of snobby, rich white people.
- Can't complete projects. Not as hip as some of the other JoCo suburbs.
- Incomplete projects and bad streets.
- People are wary of mixed income areas. But they should NOT be so.
- Gateway progress

What is your favorite thing about living in Mission?

- Doesn't feel like other Johnson County suburbs - has character and unique features.
- Many of the people and walkability!
- Liveable community in proximity to everything.
- Small town feel, but close to Downtown.
- Knowing neighbors and business owners and community members.
- Small town feel with everything we need.
- Old trees and close to everything.
- There's something for everyone.
- Retail on Johnson Drive, closeness of post office, other government/state offices.
- The people and how much they care about their neighbors and the businesses.
- Socioeconomic mix - refreshing in JoCo!

What else should be a main goal of this updated Tomorrow Together comprehensive plan?

- Energy conservation and sustainability.
- Social infrastructure and people: strengthening community connections.
- Affordable housing.
- Community for all ages influence in each category or as one goal itself.
- Public transit ridership data.
- Would promote healthy lifestyles and healthy residents fall within Parks and Recreation.

2.4 Interactive Website



INTERACTIVE WEBSITE

Due to COVID-19, social distancing guidelines were in place during the public participation process. An interactive project website was created to help assist in engagement and replace some of the input that would normally be obtained during a public workshop.

Website URL: <https://confluence.mysocialpinpoint.com/mission-comprehensive-plan/mission-home/>

WEBSITE STATISTICS

TOTAL VISITS: 3,422

UNIQUE USERS: 770

UNIQUE STAKEHOLDERS: 226

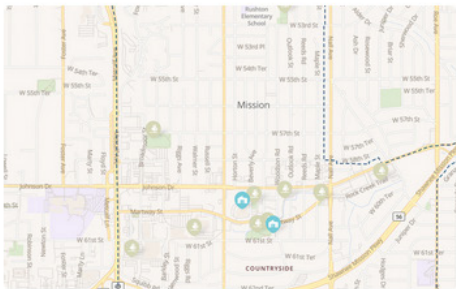
MAP COMMENTS: 242

VISUAL PREFERENCE VOTES: 1,848

SURVEY RESPONSES: 205

BUDGET RESPONSES: 73

Below are the six engagement modules included on the site. The results of each will be summarized on the following pages.



Where are the opportunities in Mission?

Add comments directly onto a map of Mission. Tell us what you like, don't like and new ideas!

See Project Map



Why should your friends visit Mission?

Tell us about your vision for Mission in 2040 by writing a postcard to your friend/family.

Write your Postcard



What should Mission look like?

Vote for images depicting the style, type and density of development you want in Mission.

Vote on Images



What are your views toward Mission?

Please complete this short survey so we can better understand your views towards Mission.

Take The Survey



What amenities do you want in Mission?

Tell us what's missing in Mission today including amenities, retail, services, housing and more.

Take The Survey



What should Mission fund?

Please help us understand priorities for Mission by submitting a pretend budget

Cast Your Vote

2.4 Interactive Website



INTERACTIVE WEBSITE - MAPPING TOOL

Mapping Comment Types

Visitors to the public engagement website were able to leave comments regarding the existing conditions of the City of Mission. The comment types included:

- Idea
- Favorite Place
- More of This
- Less of This
- Trail Idea
- Sidewalk Idea
- Bike Facility
- Sustainability Idea
- Transportation Idea

Comment Type	Count	Share
Idea	116	47.9%
More of this	25	10.3%
Transportation Idea	25	10.3%
Sustainability Idea	24	9.9%
Favorite Place	15	6.2%
Sidewalk Idea	13	5.4%
Less of this	11	4.5%
Trail Idea	9	3.7%
Blke Facility	4	1.7%
TOTAL	242	100.0%

Table 1.1 - Total Comments by Comment Type

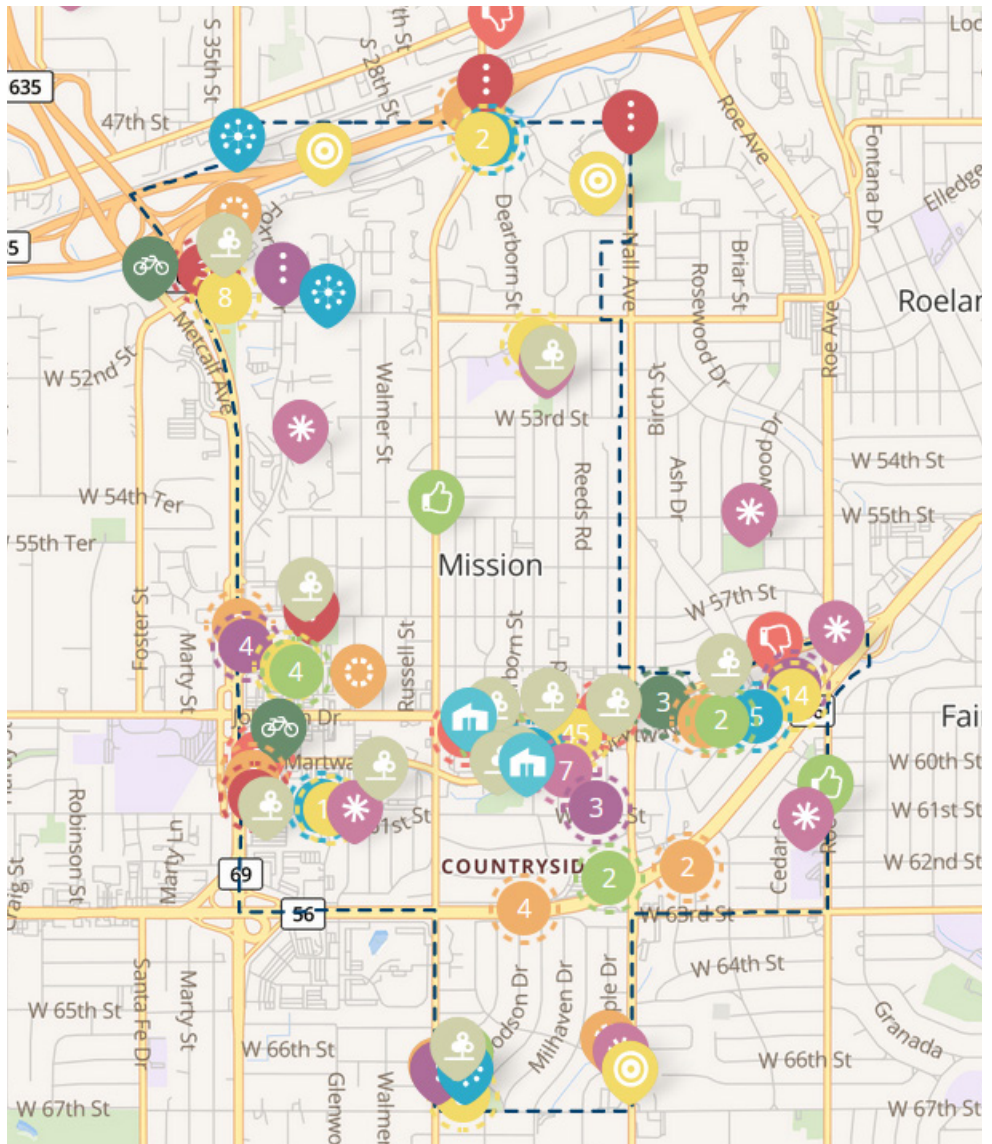


Figure 1.1 - Interactive Mapping Tool Interface

2.4 Interactive Website



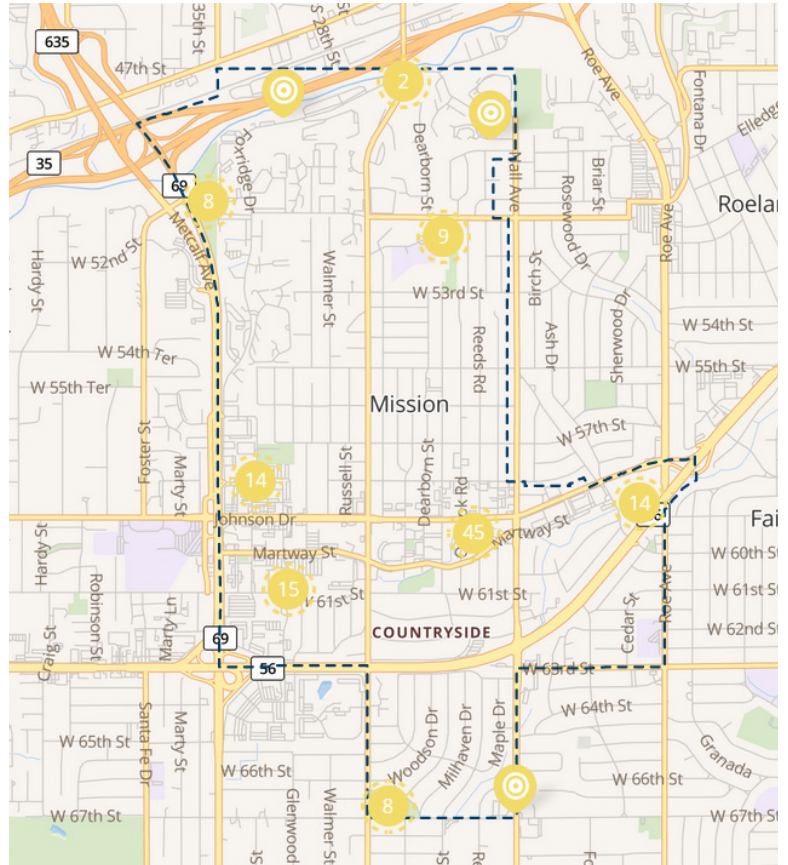
INTERACTIVE WEBSITE - MAPPING COMMENTS



Idea (47.9% of comments)

The dominant comment type was 'Idea' with 116 comments.

Figure 1.2 - Locations of 'Idea' Comments



Mohawk Park - "This park has HUGE potential to be a jewel for the neighborhood. It's mostly unimproved since tearing down the grade school years ago"
Want to see: bathroom facility, splash pad, dog park, more trees, more amenities, and more shade.

Streamway Park - well maintained trail, park/vegetation needs some TLC; want to see playground, lighting (safety), access to Turkey Creek, bridge over 69 Highway, signage, and dog park.

Waterworks Park - needs enhancements to activate space/draw people to park, standing water near school.

Broadmoor Park - walking path needs re-paving, ramp for trail access, and dog park option.

Anderson X Park - add mural to racquetball courts, keep tennis courts but update them, and dog park option.

General Park Ideas - Signage to show distance between parks, dog park highly requested, and add pocket park Downtown at Old Backyard Burgers Gazebo.

Dog Park - multiple locations proposed including north of Panera Bread, along Martway St, and Andersen Park.

Johnson Drive - more retail, replace lost tenants, and work with some property owners to resurface parking.

Streetscape on Johnson Drive - consistent street design from Metcalf to Roe (bike/ped friendly) with lighting, building, and facade improvements.

Sidewalk under 69 Highway is creepy - graffiti and overgrown vegetation.

Consider dropping Metcalf to "at grade" with Johnson Drive.

Martway Street - any redevelopment needs to be ped friendly along the trail.

Martway North of Aquatic Center - eyesores, need to be torn down or new tenants, dog park location, and keep the trees here if redeveloped.

Farmer's market parking difficult, ped crossing scary

61st street commonly used as cut thru for Target or SMP - speed bump or two would be good.

Gateway Site - multiple complaints, request for intervention, ideas for park/gathering space.

Target - needs more than once exit.

Rock Creek Trail - needs more shade trees, benches, bike racks, pet waste stations. Add solar light benches, and public art along trail, build an overhead shelter for the market space, and crime prevention via environmental design.

Chik-Fil-A/Culvers/Panera - stacking doesn't work, could impede emergency vehicles.

Lamar and W 61st Street - high foot traffic crosswalk.

Building east of Security Bank - several comments about need to redevelop this building, possible dog park or outdoor food truck facility

Street car down Johnson Drive - long-term idea.

Former Pride Cleaners Location - calls for it to be demolished, complaints about code violations.

Multiple comments about signage at entrances

2.4 Interactive Website

INTERACTIVE WEBSITE - MAPPING COMMENTS

*** Favorite Place (6.2% of comments)**
There were a total of 15 'Favorite Place' comments.

Favorite places include:

- Waterworks Park - great for walking.
- Highlands Elementary is a fabulous school with a wonderful staff.
- Osage Orange trees in median along W 61st Terrace please preserve this median.
- Mission Market - would love to see it get bigger and more permanent.
- Great coffee shop - Urban Prairie Coffee.
- Victor X Andersen Park - love the outdoor racquetball, pool and tennis courts.
- R-Park in Roeland Park.
- Matney Park in Kansas City.

Figure 1.3 - Locations of 'Favorite Place' Comments

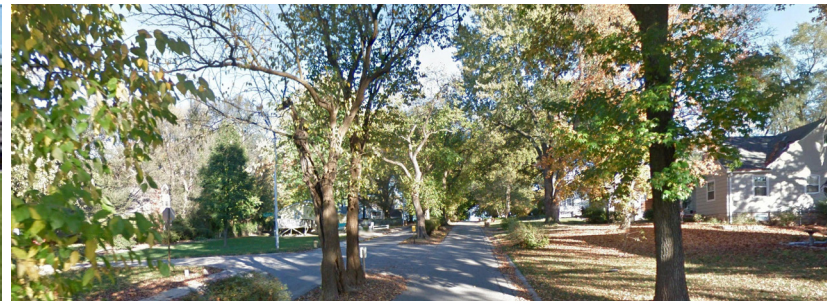
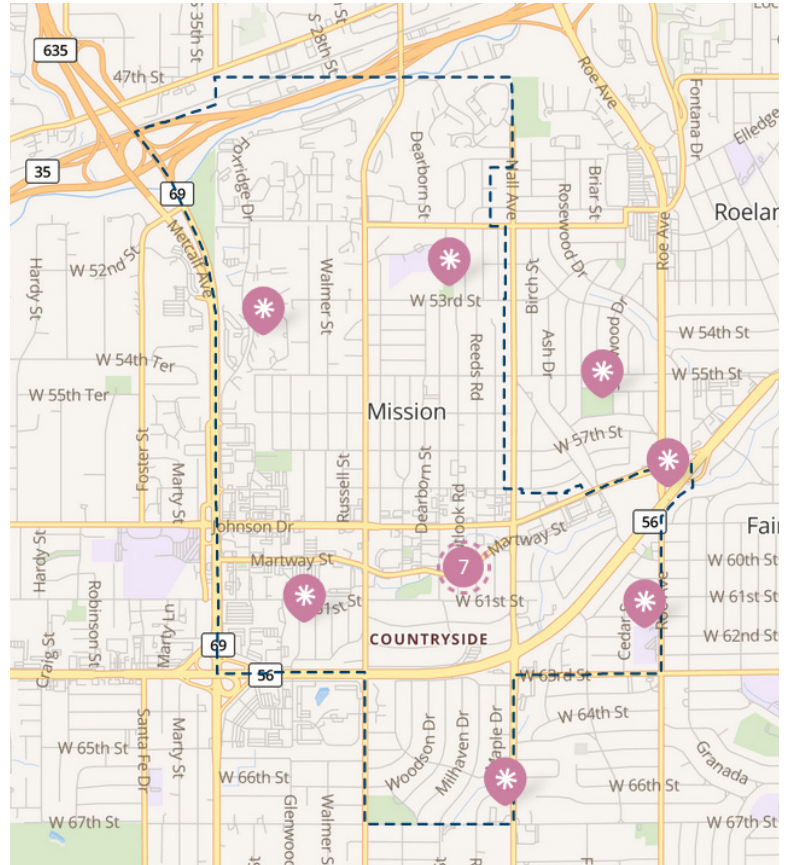


Figure 1.4 - Images of Identified 'Favorite Places'

2.4 Interactive Website



INTERACTIVE WEBSITE - MAPPING COMMENTS



More of This (10.3% of comments)

There were a total of 25 'More of This' comments.

Amenities - Rock Creek Trail is a great amenity!

Lights on the tennis courts at Victor X Anderson Park are great!

Mission has a great community center!

Continue to make Mohawk Park better and add additional parks like this in Mission.

Services - Thank you for the new food bank. There is definitely people struggling in the community. Consider more food bank locations throughout Mission.

So glad to have ripple glass at the Hy-Vee parking lot.

Infrastructure - The resurfaced Lamar Avenue with curbs, sidewalks and bike lane looks great!

Such a great trail/sidewalk/crosswalk along Nall and across Shawnee Mission Parkway.

Development - The building at Nall and Martway just added a rooftop patio. Its great!

More development similar to townhomes on W 60th Terrace. The city should encourage mixed-use buildings with a diversity of incomes and intensity to encourage community and walkability.

Some great businesses along Johnson Drive - block with Urban Prairie, Sandhills, Brian's Bakery, etc. is the best of Downtown Mission.

Supporting small businesses should be a key priority. Small retail fronts encourage pedestrian activity.

Opportunity to spruce up alley spaces in Downtown.

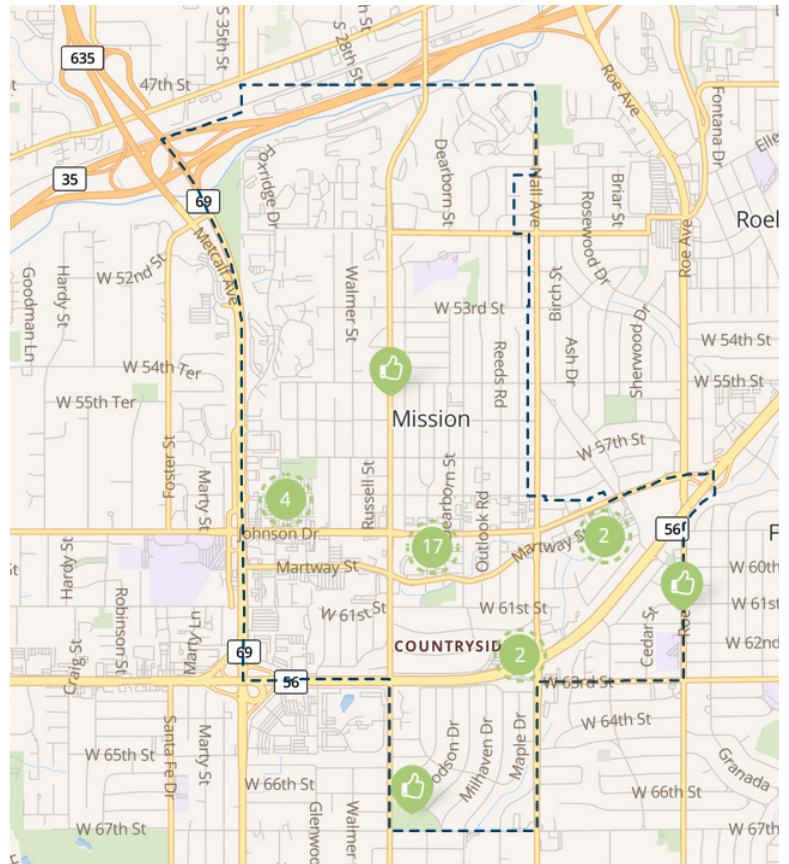
Great local bars/breweries in Downtown.

Love the new apartments - tons of new residents and still fits in with Mission.

Excited for new drive through car wash!

The properties developed under the form-based code look great. The required streetscape really enhances the area and makes it feel more pedestrian friendly.

Figure 1.5 - Locations of 'More of This' Comments



Other Comments - Love all the beautification along Broadmoor. Great job!

Improvements made to the Mission Towers satellite parking lot is great!

Keep Broadmoor Park for families and kids to enjoy - no dog park here.

Script Pro takes good care of their property and even has an outdoor basketball hoop. More of this is needed throughout Mission.

2.4 Interactive Website



INTERACTIVE WEBSITE - MAPPING COMMENTS

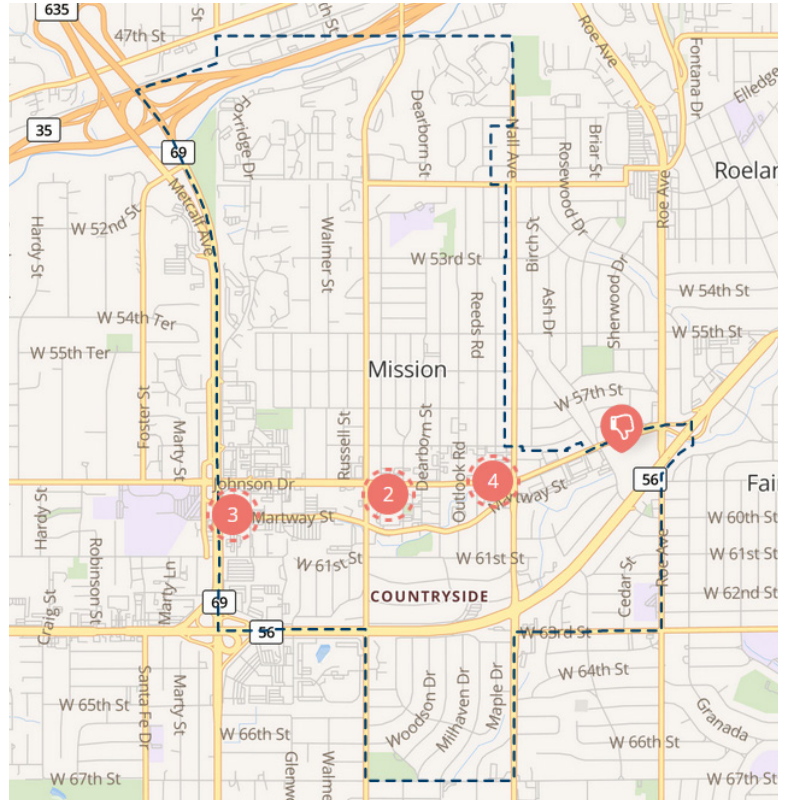


Less of This (4.5% of comments)

There were a total of 11 'Less of This' comments.

- Beautification needed along the north side of Johnson Drive between Metcalf Avenue and Broadmoor Street.
- Commercial property on the SE corner of Johnson Drive and Outlook Road needs a refresh. Possible second story added for residential use? Opportunity to restaurant patio in rear along Rock Creek Trail.
- Crossing Johnson Drive by foot is still scary.
- Enhance BP gas station at Johnson/Nall. Corner is unsightly and needs to be better maintained.
- Failed Gateway project needs to be addressed - site is an eyesore.
- Locale development on Johnson Drive does not fit within the character of Mission. Parking garage is a behemoth and casts large shadows.
- Parking lots in western gateway area poorly designed/confusing.

Figure 1.6 - Locations of 'Less of This' Comments



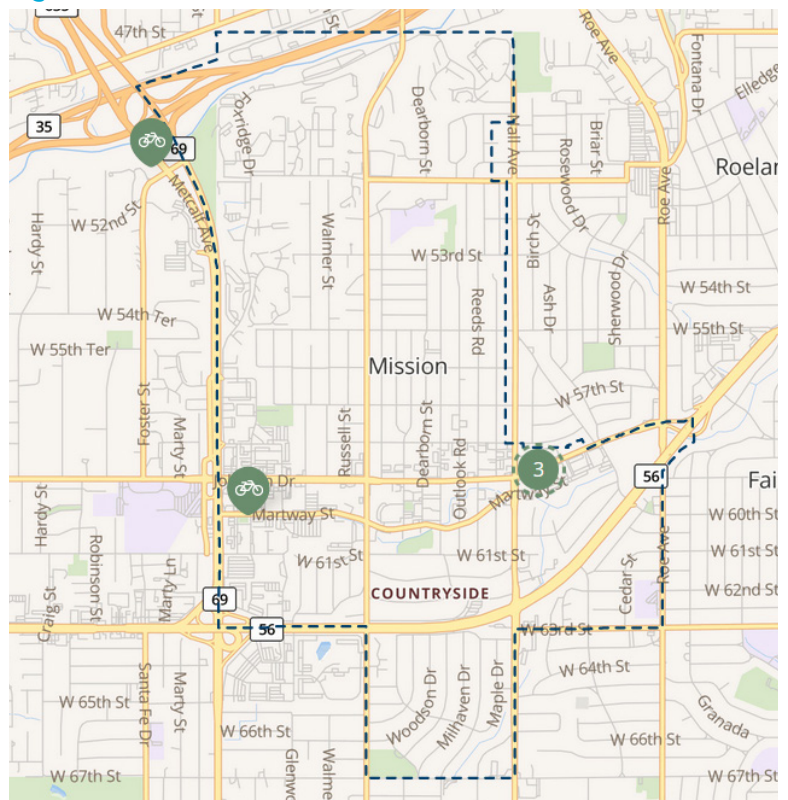
Bike Facilities (1.7% of comments)

There were a total of 4 'Bike Facilities' comments.

Bike Facility ideas include:

- Add a bike share station in the western area of the city. This would allow people to use transit into/out of Mission and utilize bikes within it.
- Add more bike racks and facilities like the bike fix-it station along the Rock Creek Trail.
- Add bike/scooter rental station at the Mission Transit Center.
- Finish the Turkey Creek Trail so it connects to other cities in Johnson County and to routes going to Downtown.

Figure 1.7 - Locations of 'Bike Facilities' Comments



2.4 Interactive Website



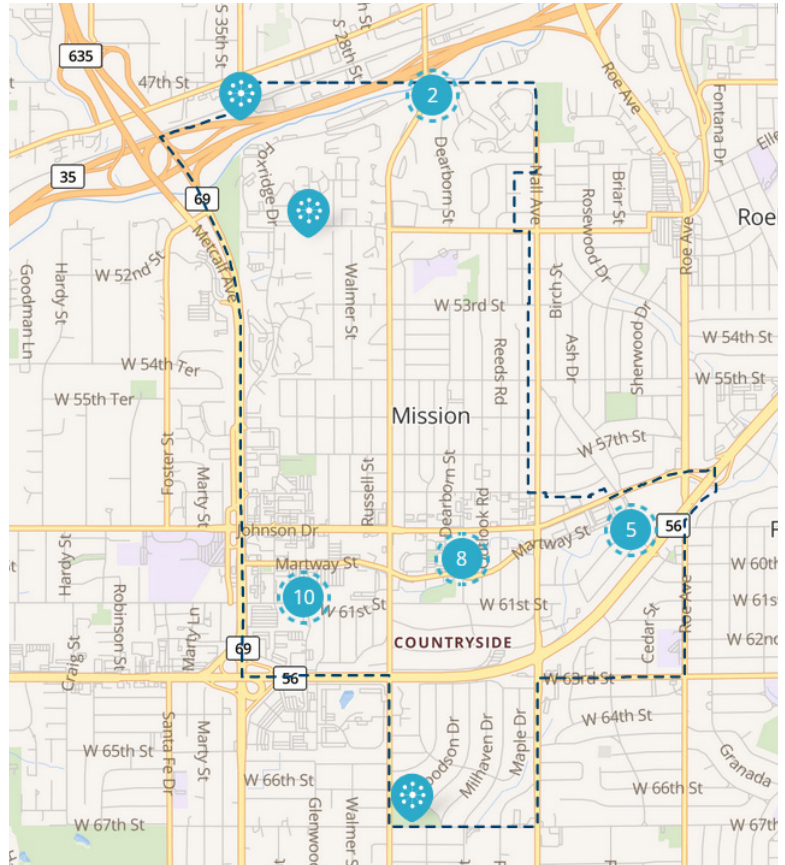
INTERACTIVE WEBSITE - MAPPING COMMENTS



Sustainability Idea (6.2% of comments)

There were a total of 24 'Sustainability Idea' comments.

Figure 1.8 - Locations of 'Sustainability Idea' Comments



Ideas included:

Community Gardens - Expand community garden opportunities, including those on Lamar Avenue.

Opportunity for community garden near Target along Rock Creek Trail?

How do we encourage brownfield clean-up projects?

Services/Amenities - Add more electric charging stations throughout Mission.

Mission should promote carpool, EV stations, bike and pedestrian infrastructure, and transit options to reduce transportation pollution.

City facilities should be built and certified with LEED standards.

Appreciate recycling opportunities throughout the community (Target, Ripple Glass). Expand this throughout Mission.

Consider adding curbside leaf pickup as a city service. Would reduce the number of bags going to the landfill.

Maybe the Gateway site is better positioned as a large green space with an outdoor events center. Would be a place people from Fairway, Mission and Roeland Park would go.

Green Infrastructure - Diminish impervious surfaces along Rock Creek Trail. This will alleviate intensity of flooding and enhance the trail experience.

Invest in green infrastructure along Rock Creek and design facilities for 500-year (or more) floods. Consider more detailed floodplain/watershed plans.

Convert medians and edges of Shawnee Mission Parkway into bioswales.

Would like to see more green infrastructure elements in the Mission Market area. It is a great spot for rain gardens, educational signage to describe BMP's.

Other Comments - Would like to see native/pollinator gardens in Mission park facilities. It would also be greater if there were pesticide/herbicide free zones.

Preserve natural features north of W 51st St. between Riggs and Foxridge.

Light pollution is a serious threat to many environmental entities from sleep deprivation to animal migration. Some signs along Johnson Drive are too bright and has a negative impact on nearby neighbors. Regulate things like this.

2.4 Interactive Website



INTERACTIVE WEBSITE - MAPPING COMMENTS



Sidewalk Ideas (5.4% of comments)

Visitors to the Mission Tomorrow Together interactive website were able to articulate their vision for sidewalk enhancements and installations. Mission has an extensive sidewalk network but, in some locations, there are gaps in the coverage or undesirable walking conditions. These comments should help Mission prioritize future improvements to the existing sidewalk network.

“SIDEWALK CONNECTION HERE WOULD MAKE IT EASIER FOR STUDENTS TO WALK TO HIGH SCHOOL.”

A summary and location of the comments and routes provided is shown below.

Figure 1.9 - Locations of 'Sidewalk Ideas' Comments

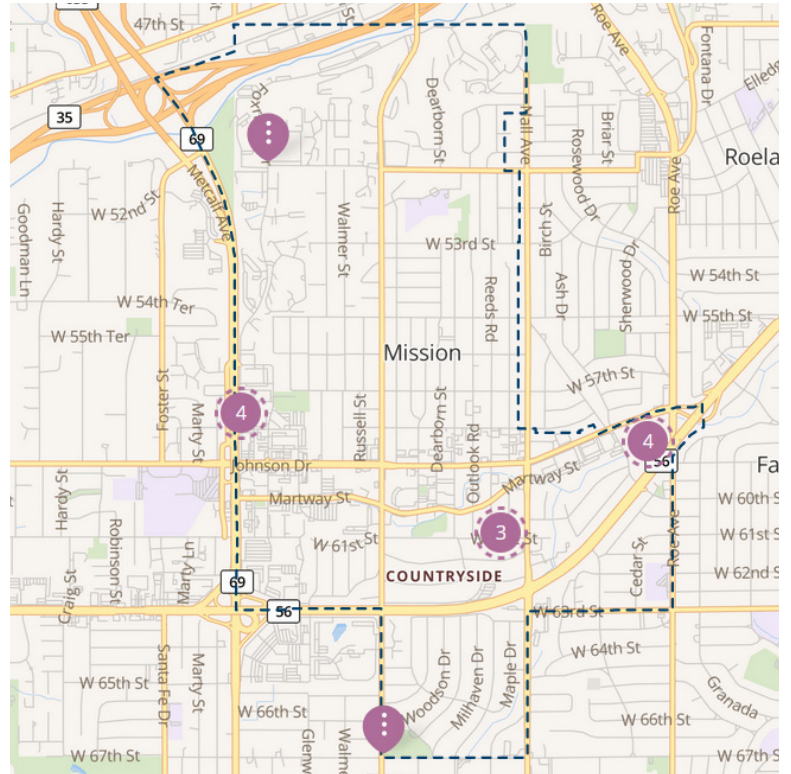
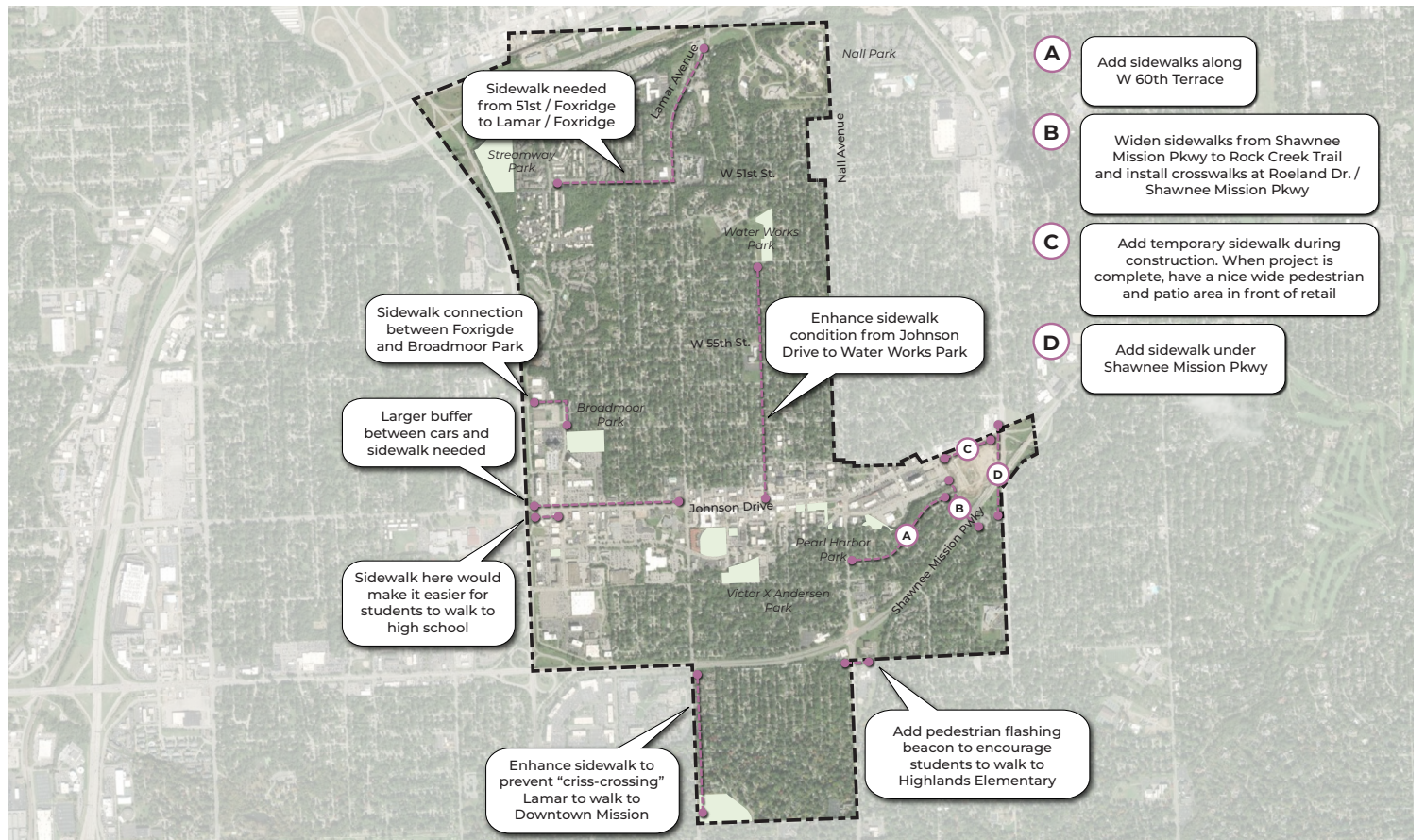


Figure 1.10 - 'Sidewalk Ideas' Routes and Comments



2.4 Interactive Website



INTERACTIVE WEBSITE - MAPPING COMMENTS



Trail Ideas (3.7% of comments)

Visitors to the Mission Tomorrow Together interactive website were able to articulate their vision for future trail connections and enhancements. Many of the comments relate to existing trail facilities that reside in neighboring communities such as Merriam and Roeland Park. The City of Mission should work with these communities to establish a cohesive network of trails throughout northeast Johnson County.

A connection between the Turkey Creek Trail and the Rock Creek Trail should be explored. These trails are highly used and connection between them would add to the overall quality of life and opportunities for recreation in Mission.

A summary and location of the comments and routes provided is shown below.

Figure 1.11 - Locations of 'Sidewalk Ideas' Comments

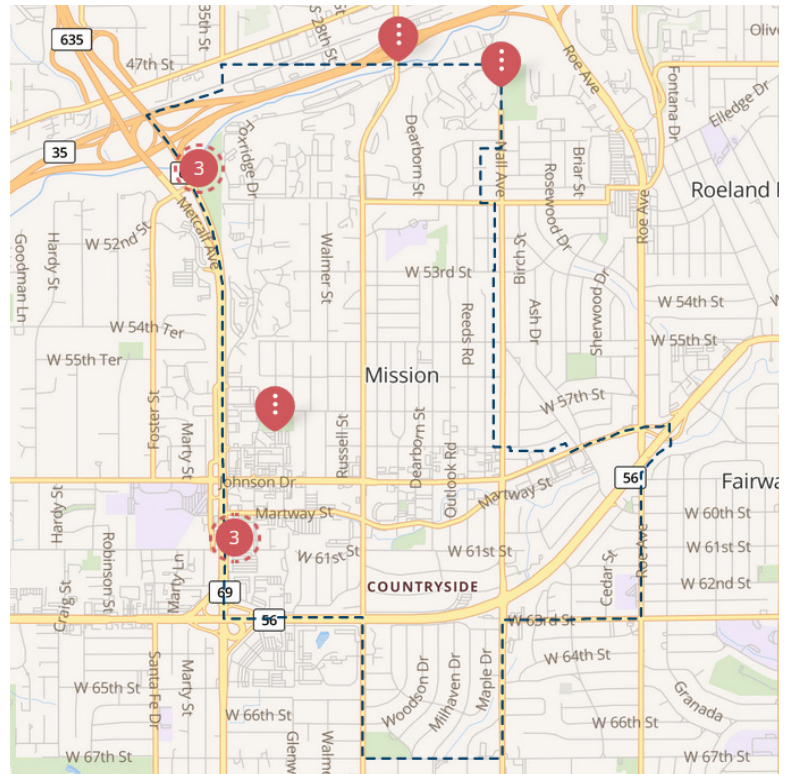
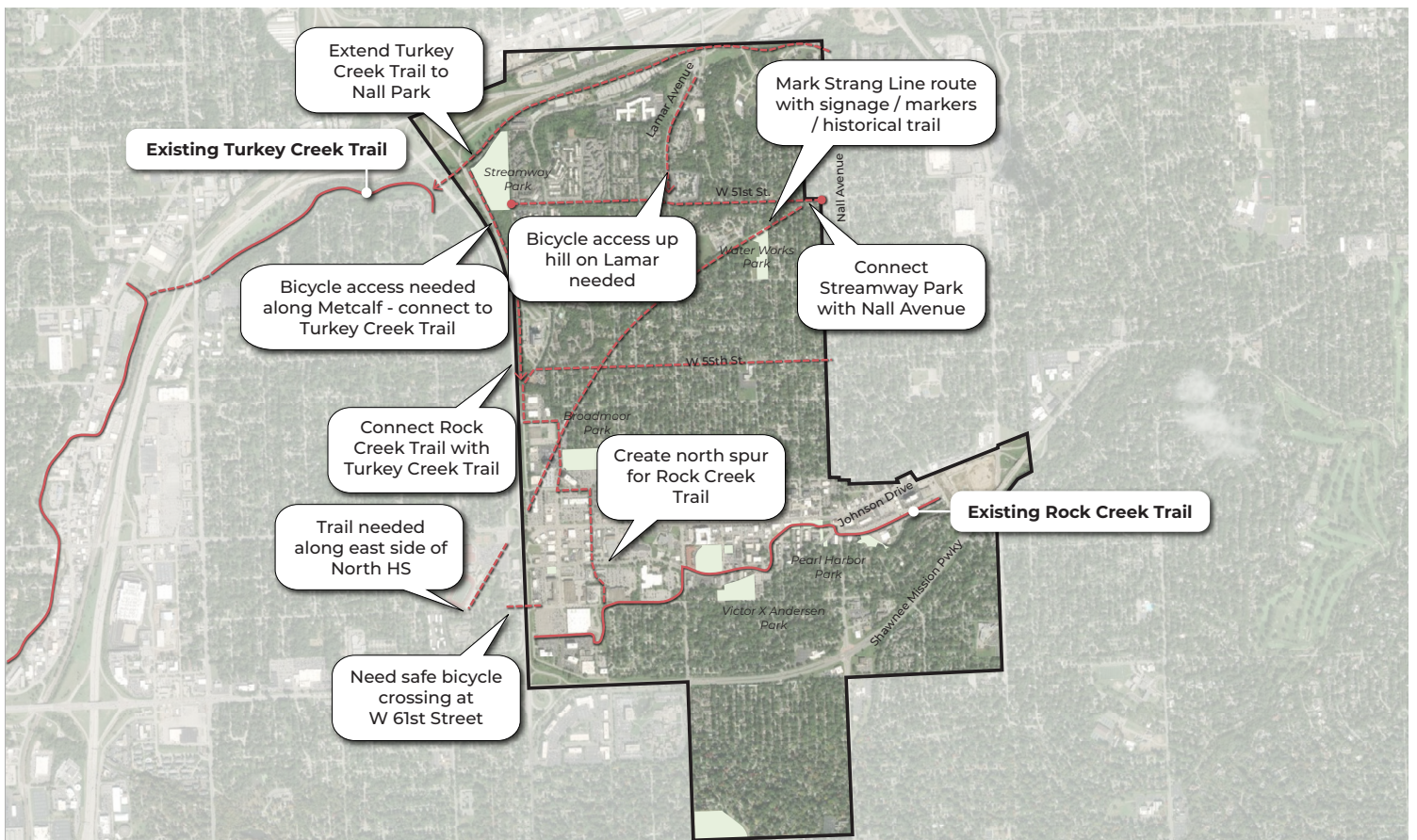


Figure 1.12 - 'Trail Ideas' Routes and Comments



2.4 Interactive Website



INTERACTIVE WEBSITE - MAPPING COMMENTS



Transportation Idea (10.3% of comments)

There were a total of 25 'Transportation Idea' comments.

Ideas included:

Metcalf Avenue - Metcalf/W 56th Street intersection is an issue. Either make it a true intersection or extend the merge lanes for safe merging.

Metcalf and W 58th Street is dangerous. Extend merge lane onto Metcalf.

Metcalf and Johnson Drive should be a gateway into Mission. Bring Metcalf down to grade and install welcome signage.

Metcalf and Johnson Drive extremely dangerous for pedestrians and cyclists. Needs improvements.

Need safer access from Martway Street to Metcalf Avenue - northbound.

Work with Overland Park to bring Metcalf/Shawnee Mission Parkway down to grade. Easier for drivers, pedestrians and provide land for development.

W 61st Street, near the post office, is in poor condition. A safe bike and walking route would be nice to connect to the Rock Creek Trail and Metcalf.

Shawnee Mission Parkway - Add left turn lane at Outlook Road.

People often cross on foot over Shawnee Mission Parkway at Outlook Road - dangerous. Would love to see a tunnel or bridge at this location.

Add 'no parking' signs within 30 feet of stop sign on both sides of the road for 62nd Terrace, north of Shawnee Mission Parkway. Issues with visibility.

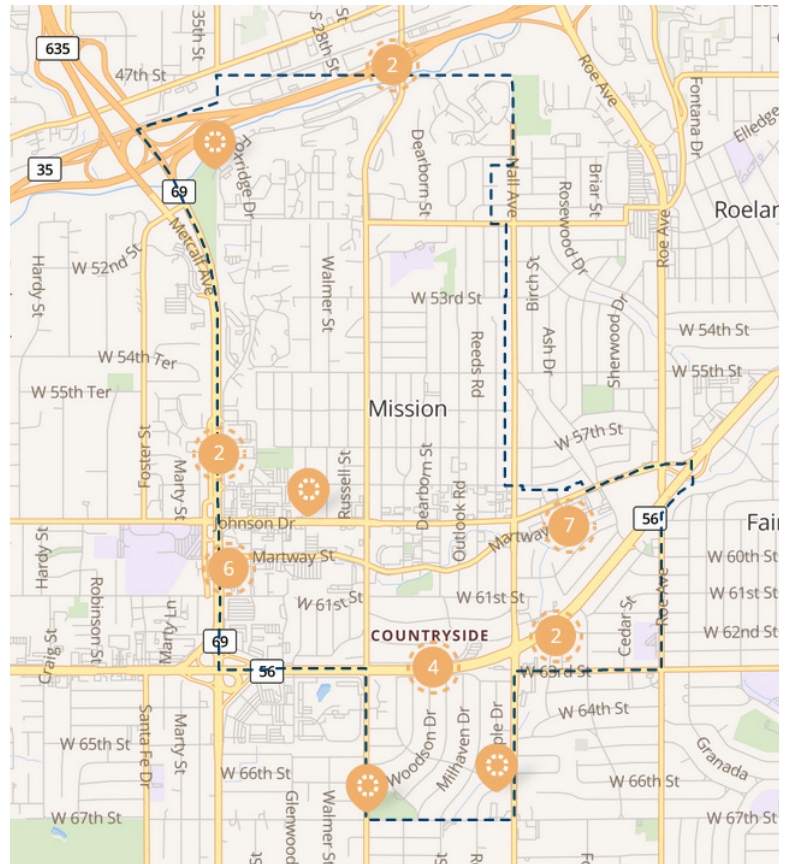
Shawnee Mission Parkway and Nall intersection is dangerous. Consider complete strategies to improve safety/slow down traffic.

Transit - Add fans/heaters to the transit center to make more comfortable during extreme weather.

Add a bike share rack at the transit center.

Support frequent and weekend service to Downtown KC!

Figure 1.13 - Locations of 'Sidewalk Ideas' Comments



Other Comments - Make the Mission Village neighborhood a "car-free" or "car-lite" area where the streets would be open to bikes and pedestrians and safer for children to play.

Extend Johnson Drive improvements west of Lamar. Area is a miss-match of things, has a lot of driveways, narrow sidewalks and a variety of lighting.

Crosswalk needed at W 67th St and Lamar Avenue to safely connect the neighborhoods with Mowhawk Park.

Speed bumps on Nall Drive could prevent non-residents from using this as a shortcut between Nall Avenue and W 67th St.

Enhancements need at Roeland Drive. Need more accessible and visible parking at Streamway Park.

I-35/Lamar Avenue interchange is very dangerous and difficult to navigate. Left turns onto Lamar are problematic. Off-ramp needs to be paved.

2.4 Interactive Website



INTERACTIVE WEBSITE - POSTCARD EXERCISE

Why should your friends visit Mission?

Participants were asked to “Tell us about your vision for Mission in 2040 by writing a postcard to your friend/family.”

The responses are shown to the right.

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

Bring your bike because we have a 10 mile trail that stems from Johnson Drive and Nall. Afterward, you will love the shops and dining in downtown Mission! Once we walk the strip and grab a drink and bite, we can take the bus downtown for a show! It is a super quick trip and 100% free.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

...this community always shows up and comes together. We have unique local events, our small businesses are thriving and our various community led committees have done so many wonderful things! And there are so many great locally own restaurants to choose from. Bring comfy shoes because we'll be doing so much walking!

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

The beautiful downtown area has many new shops, ethnic restaurants and apartments within convenient walking distance.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

Mission is like a small town. So many unique independent businesses.

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

Please visit the gateway to not only Johnson County but the heart of Kansas City and the Plaza!

PLACE STAMP HERE

MISSION COMPREHENSIVE PLAN
Mission, Kansas

Dear Friend/Family,
It is the year 2040 and you should visit me here in Mission because...

There are fun activities and trails to walk.

PLACE STAMP HERE

2.4 Interactive Website

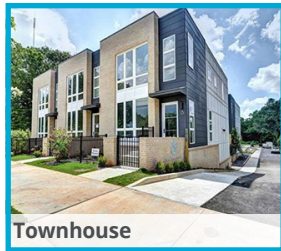


INTERACTIVE WEBSITE - VISUAL PREFERENCE EXERCISE

Visual Preference Exercise

Visual preference exercises were completed for six categories: Multi-Family Residential, Signage, Infill Residential, Park Amenities, Streetscape Enhancements, and Placemaking Elements. The results are summarized below.

MULTI-FAMILY RESIDENTIAL



Most Preferred → Neutral



Neutral ← Less Preferred



Least Preferred

Table 1.2 - Multi-Family Image Votes

Comment Type	Votes
High-Rise Apartment	50
Townhouse	50
Mid-Rise Apartment	44
Rowhouse	32
Duplex	25
Fourplex	23
Townhouse 2	21
Mid-Rise Apartment 2	17
Rowhouse 2	15
Duplex 2	12
Fourplex 2	5
High-Rise Apartment 2	4
TOTAL	298

Multi-Family Residential

The most preferred Multi-Family Residential images were High-Rise Apartment, Townhouse, Mid-Rise Apartment and Rowhouses. The High-Rise Apartment that received the most votes was in the 3-4 story range with an activated street front and mixed uses on the first story. The least preferred option were the second options for Fourplex and High-Rise Apartment. The second fourplex option was more generic in its exterior and the high-rise apartment was 6+ stories.

Overall, the preference seemed to be for human-scale mid-intensity density such as 3 to 4 story mixed-use apartment buildings, townhomes, and rowhouses.

2.4 Interactive Website

INTERACTIVE WEBSITE - VISUAL PREFERENCE EXERCISE

SIGNAGE



Most Preferred ← ————— → Prefer



Prefer ← ————— → Neutral



Neutral ← ————— → Neutral



Less Preferred ← ————— → Least Preferred

Table 1.3 - Signage Image Votes

Comment Type	Votes	Comment Type	Votes	Comment Type	Votes
Gateway Option A	29	Gateway Option J	15	Gateway Option N	4
Gateway Option E	24	Gateway Option Q	13	Gateway Option S	4
Gateway Option T	22	Gateway Option K	12	Gateway Option L	2
Gateway Option P	20	Gateway Option M	12	Gateway Option C	1
Gateway Option B	18	Gateway Option D	11	Gateway Option I	1
Gateway Option O	18	Gateway Option H	9	Gateway Option R	1
Gateway Option G	15	Gateway Option F	5	TOTAL	236

2.4 Interactive Website



INTERACTIVE WEBSITE - VISUAL PREFERENCE EXERCISE

INFILL RESIDENTIAL



Remodeled Home*



Detached ADU **



New Medium Home*



New Medium Home*



Garage ADU**

Most Preferred ←

→ Neutral



New Narrow Home*



Modern Home*



Micro Home*



New Medium Home*



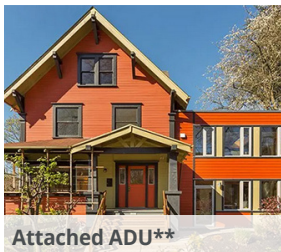
New Large Home***

Neutral ←

→ Preferred Less



Basement ADU**



Attached ADU**

* Single-Family Detached Home
 ** ADU = Accessory Dwelling Unit
 *** Consolidated Lot

← Least Preferred

Infill Residential

The most preferred Infill Residential option was the Remodeled Single-Family Home, which received a total of 48 votes. The next two options with a higher number of votes were the Detached Accessory Dwelling Unit or ADU (35 votes) and the New Medium Sized Single-Family Home with a Setback Garage (31 votes). Infill residential options with between 20 and 25 votes each included New Medium Single-Family Homes with Detached Garage, Garage Accessory Dwelling Unit, the New Narrow Home and the Modern and Micro Home options. The least preferred options were the New Large Home on a consolidated lot and the basement and attached accessory dwelling units.

These results indicate that small to medium-sized homes are most desired in Mission. There also appears to be support for certain types of accessory dwelling units (detached and over a detached garage).

- Preference seems to be for remodeling/rehabilitating existing family-homes.
- Accessory dwelling units as detached structures or units above detached garages.
- People did not appear to support large homes being built on consolidated lots.

Table 1.4 - Infill Residential Image Votes

Comment Type	Votes
Remodeled Home*	48
Detached ADU**	35
New Medium Home*	31
New Medium Home* 2	25
Garage ADU**	25
New Narrow Home*	23
Modern Home*	20
Micro Home*	20
New Medium Home*	17
New Large Home***	15
Basement ADU**	15
Attached ADU**	13
TOTAL	287

* Single-Family Detached Home
 ** ADU = Accessory Dwelling Unit
 *** Consolidated Lot

2.4 Interactive Website



INTERACTIVE WEBSITE - VISUAL PREFERENCE EXERCISE

PARK AMENITIES



Park Bathrooms



Green Infrastructure



Additional Tree Canopy



Dog Park



Park Shelter

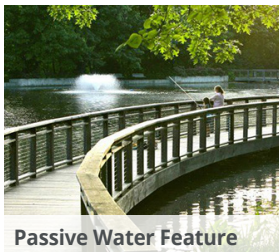
Most Preferred ← ————— → Prefer



Interactive Water Feature



Inclusive Playground



Passive Water Feature



Playground Equipment



Benches/Seating

Prefer ← ————— → Neutral



Placemaking Elements*



Formal Garden



Interpretive Signage



Park Signage/Branding

* Art, Signage, etc.

Preferred Less ← ————— → Least Preferred

Park Amenities

Park Bathrooms (51 votes) were the most commonly selected park amenity followed closely by Green Infrastructure (49 votes) and Additional Tree Canopy (46 votes). Dog parks were also popular choices with 46 and 40 votes, respectively. Park shelters, interactive water feature, inclusive playground and passive water feature all had between 32 and 37 votes. The least popular park amenity choices were formal garden (14 votes), interpretive signage (12 votes) and park signage/branding (10 votes).

Table 1.5 - Park Amenity Image Votes

Comment Type	Votes
Park Bathrooms	51
Green Infrastructure	49
Additional Tree Canopy	46
Dog park	40
Park Shelter	37
Interactive Water Feature	34
Inclusive Playground	32
Passive Water Feature	32

Comment Type	Votes
Playground Equipment	29
Benches/Seating	24
Placemaking Elements	24
Formal Garden	14
Interpretive Signage	12
Park Signage/Branding	10
TOTAL	434

2.4 Interactive Website



INTERACTIVE WEBSITE - VISUAL PREFERENCE EXERCISE

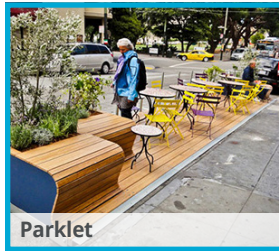
STREETSCAPE ENHANCEMENTS



Green Infrastructure



Multi-Purpose Path



Parklet



Additional Street Trees



Linear Park

Most Preferred ●—————→ Prefer



Food Truck Area



Pedestrian Refuge Island



Curb Bump Out

Neutral ←—————● Least Preferred

Streetscape Enhancements

The two most preferred streetscape enhancements were Green Infrastructure (57 votes) and Multi-Purpose Path (55 votes). Close behind these two was a Parklet with 41 votes. Additional Street Trees, Linear Park, and Food Truck Area all received between 30 and 34 votes each. Pedestrian Refuge Island had approximately 19 votes. The least selected streetscape enhancement was a Curb Bump Out, which only receives 7 votes.

Generally, the enhancements selected were related to street beautification and sustainability. Direct pedestrian safety improvements such as a refuge island or curb bump out were the least selected options. These two factors collectively point towards an improved pedestrian experience being more a priority than pedestrian safety alone. However, separating paths for pedestrian users itself helps to enhance safety as well.

Table 1.6 - Streetscape Enhancements Image Votes

Comment Type	Votes
Green Infrastructure	57
Multi-Purpose path	55
Parklet	41
Additional Street Trees	34
Linear Park	32
Food Truck Area	30
Pedestrian Refuge Island	19
Curb Bump Out	7
TOTAL	275

2.4 Interactive Website

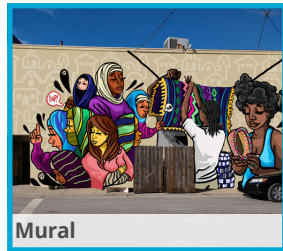


INTERACTIVE WEBSITE - VISUAL PREFERENCE EXERCISE

PLACEMAKING ELEMENTS



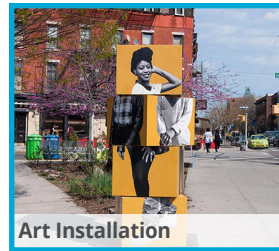
Gathering Space



Mural



Enhanced Alley



Art Installation



Plaza Space

Most Preferred ●

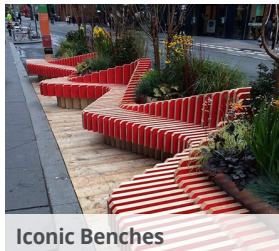
→ Prefer



Oversized Planters



Crosswalk Art



Iconic Benches



Public Piano



Sculptural Bus Stop

Neutral ←

● Preferred Less

Placemaking Elements

The most voted on placemaking element, by 14 votes, was Gathering Space, which received 61 votes. The next two most popular placemaking elements were Murals (47 votes) and Enhanced Alley (44 votes). Art installation and Plaza Space each received 33 and 32 votes, respectively. The least popular elements were Public Piano, which received 13 votes, and Sculptural Bus Stop, which received only 5 votes.

Generally, the top three choices were about creating actual places for people to spend time, such as gathering spaces or enhanced alleys, or features that draw in visitors such as a unique mural or art installations. Specific public art examples included within the list were less popular. It is difficult to know if this is because of the specific example shown in the image or if public art in this form overall is not preferred.

Table 1.7 - Placemaking Elements Image Votes

Comment Type	Votes
Gathering Space	61
Mural	47
Enhanced Alley	44
Art Installation	33
Plaza Space	32
Oversized Planters	29
Crosswalk Art	27
Iconic Benches	27
Public Piano	13
Sculptural Bus Stop	5
TOTAL	318

2.4 Interactive Website



INTERACTIVE WEBSITE - MAIN COMPREHENSIVE PLAN SURVEY

There were two surveys included on the Interactive Engagement Website: the Main Comprehensive Plan Survey and the Desired Amenities Survey.

The Main Comprehensive Plan Survey was composed of ten questions. The responses received are summarized on the following pages.

Question 1 - What is your relationship to Mission?

Question 1 asked about participant relationship to Mission. Participants could select more than one option. In total, there were 111 surveys completed. Nearly 90% live in Mission and almost half indicated they shop or dine out in Mission. Nearly 4% of respondents said they do not live in Mission and approximately 16% said they work in Mission.

Question 2 - What one word would you use to describe Mission?

Respondents were asked to write-in one word to describe Mission. Answers were reviewed and made into a word cloud to identify the most common words or phrases. The larger the word, the more frequently it was used. Quaint, small, friendly, community, home, cozy and small-town feel were all common words used in the response.

Question 3 - How would you rate the current quality of life in Mission?

Question 3 asked respondents to rate the current quality of life in Mission. Generally, most respondents indicated High (59.6%) or Very High (18.4%), representing a collective 78% of respondents. Nearly 20% rated the quality of life somewhere between high and low. Less than 2% said Low and 0% rated quality of life as Very Low. These responses indicate a strong quality of life in Mission with some areas of improvement. Other questions can help provide context to these responses.

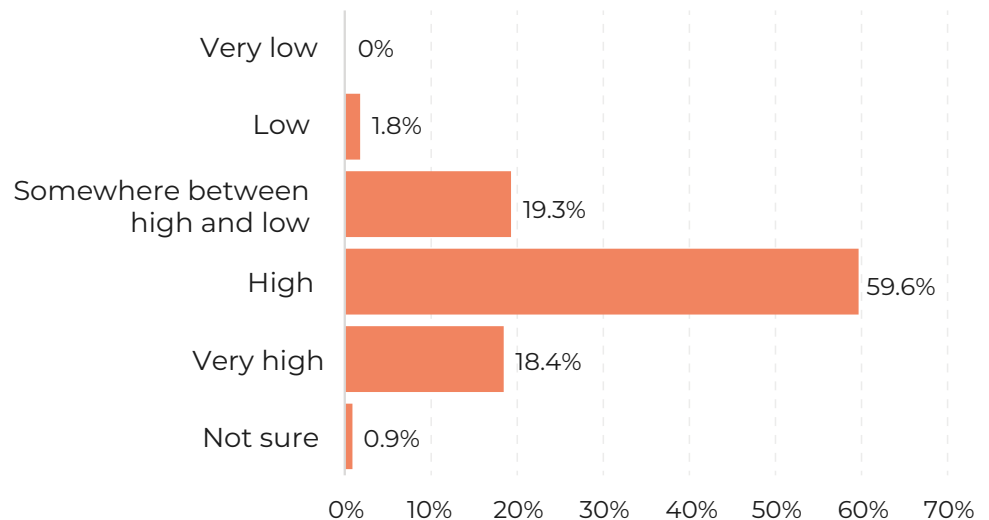
Table 1.8 - Question 1 Responses

Selected Answer	Count	Share
I live in Mission	98	88.3%
I work in Mission	18	16.2%
I do not live in Mission	4	3.6%
I shop/dine out in Mission	55	49.5%
TOTAL	111	100.0%

Figure 1.14 - Question 2 Responses



Figure 1.15 - Question 3 Responses



2.4 Interactive Website

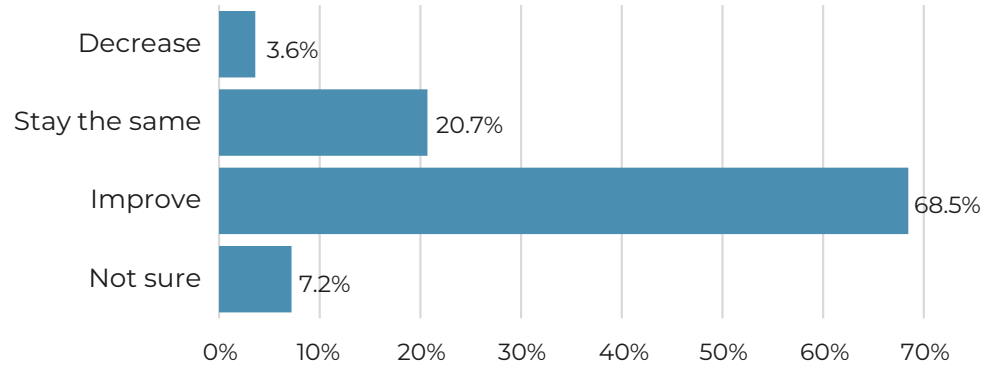


INTERACTIVE WEBSITE - MAIN COMPREHENSIVE PLAN SURVEY

Question 4 - Overtime, do you expect the quality of life in Mission to...

As a follow-up to question's three quality of life question, question 4 asked about the anticipated future quality of life expected by Mission residents. Nearly 70% said they expect the quality of life to increase over time in Mission. Another 20% expect the quality of life to stay the same. Of those that selected Stay the Same 16/23 had already rated the existing quality of life as High or Very High. Overall, these results can be interpreted as a significant amount of optimism for the future in Mission. Only 7% said they were not sure and less than 4% selected Decrease. Optimism about the future can be difficult to create so it is a positive sign that it already exists within the community.

Figure 1.16 - Question 4 Responses



Question 5 - Please describe why you currently live/conduct business in Mission.

Question 5 was another write-in question that asked respondents why they currently live or conduct business in Mission. The word cloud below shows the common themes and phrases used in the responses. The most common reasons were location, access, small-town feel, affordable, walkable, and safe.

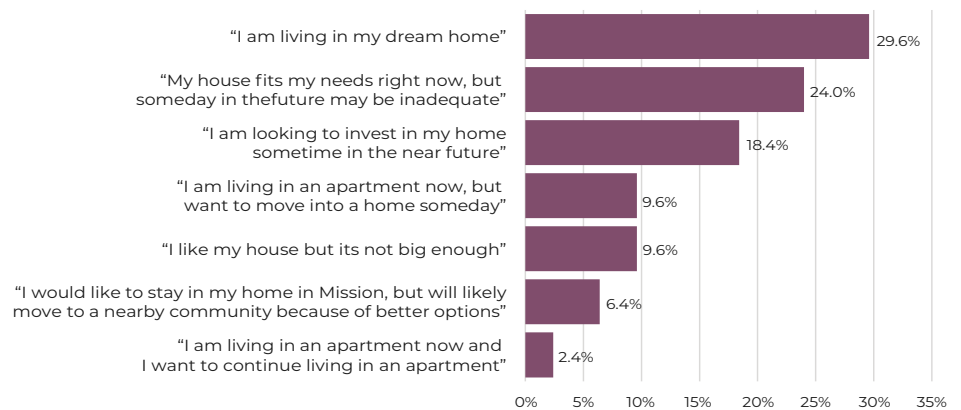
Figure 1.17 - Question 5 Responses



Question 6 - Which of the following statements regarding housing choice and diversity in Mission do you agree with most?

Question 6 asked respondents to select from a series of statements regarding housing choice and diversity. Nearly 30% said they were currently living in their dream home. Another quarter of respondents said the house fits their needs right now but, in the future, may be inadequate. Nearly 20% said they were looking to invest in their home in the near future. The least commonly selected option was "I am living in an apartment now and I want to continue living in an apartment" with 2.4% of the responses.

Figure 1.18 - Question 6 Responses



2.4 Interactive Website

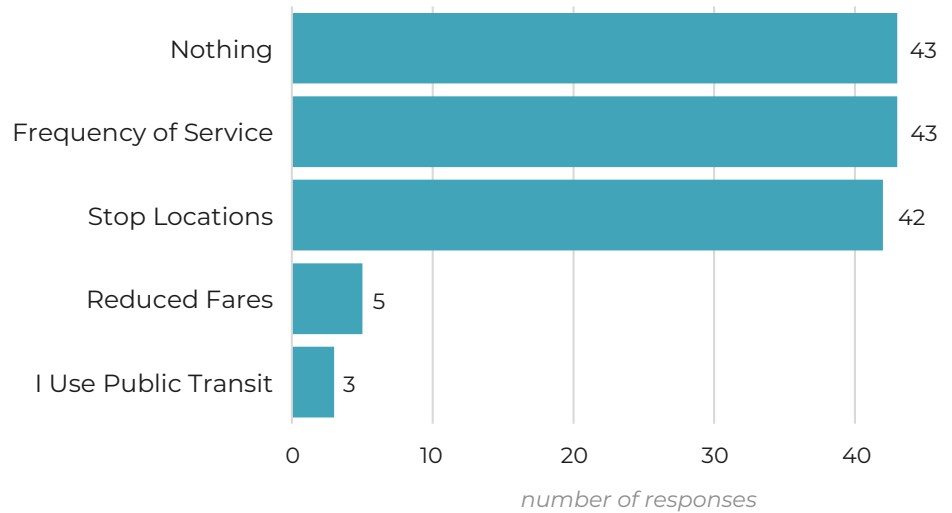


INTERACTIVE WEBSITE - MAIN COMPREHENSIVE PLAN SURVEY

Question 7 - If you don't use public transit today, what would be needed to make you start using it?

Question 7 asked what, if anything, would make the respondent use public transit. Roughly one-third of respondents said nothing would entice them to use public transit. For those willing to consider public transit, the frequency of service and stop locations were the two factors that each received roughly one-third of the responses. Reduced fares did not emerge as a barrier. Very few respondents currently use public transit (3 respondents).

Figure 1.19 - Question 7 Responses



Question 8 - If you were Mayor for one day, what would be the first thing you would address to improve the city of Mission?

Results varied considerably for this unique write-in question. Some common themes included:

- Gateway Project (mentioned multiple times).
- Dog Park.
- Improving Downtown with more businesses/development.
- Trail, sidewalk and connectivity improvements.
- Improve roads throughout Mission.
- Promoting diversity.
- Stop adding fast food chains, incorporate more small-business restaurants.
- Prevent tearing down historic homes and enforce laws to limit the footprint of new construction.
- Encourage sustainable garden/lawn care practices.
- Attract charming businesses to Downtown.
- Upgrade parks throughout Mission.
- Streetscape improvements along Johnson Drive.
- Plant more trees.
- Consider height limits for new construction in Mission.
- Focus on being Mission, not trying to be Lenexa etc.
- Deal with abandoned buildings at Roeland Drive and Johnson Drive.
- Some did not want to see more apartments in Mission.
- Safety.
- Plant more trees throughout the community.
- Sidewalk expansion (every street, at least one side).
- Prioritize small-town identity and feel.
- Home improvement incentives (energy efficiency, home equity credits, etc.).
- Make West Johnson Drive look more like East Johnson Drive.
- Climate action.

2.4 Interactive Website



INTERACTIVE WEBSITE - MAIN COMPREHENSIVE PLAN SURVEY

Question 9 - Which best describes your race/ethnicity? (Optional)

Question 9 was specifically listed as optional and asked respondents to describe their race/ethnicity. The vast majority (94.3%) of respondents listed White/Caucasian. Nearly 3% of respondents said they were Multiracial or Biracial. Roughly 2% said they were Hispanic or Latino. Around 1% said they were Black.

Census data from 2019 indicates approximately 76% of Mission residents are White alone - so while they are the majority in Mission, they are over-represented in the survey results. Black/African American alone represent nearly 10% of Mission residents but only 1% of survey respondents. Asian alone account for 4.7% of Mission residents but were not accounted for in the survey results. Similarly, Hispanic or Latino ethnicity represents 5.2% of Mission residents but less than 2% of survey respondents.

Question 10 - What is your age? (Optional)

Question 10 was specifically listed as optional and asked respondents to indicate their age range. There were zero respondents that selected Under 18 years. There was roughly equal representation between those 18 to 34 years and those 35 to 49 years, each with around 30% of the responses. Similarly, there was equal representation between those 50 to 64 years and those 65 years and over, each with around 20% of the responses.

This age breakdown in Mission and Johnson County is shown in Figure 16. Based on the age profile, it appears as if the 18 to 34 years old and 50 to 64 years old presences are representative. Those age 35 to 49 years appears slightly over represented as does those age 65 and older.

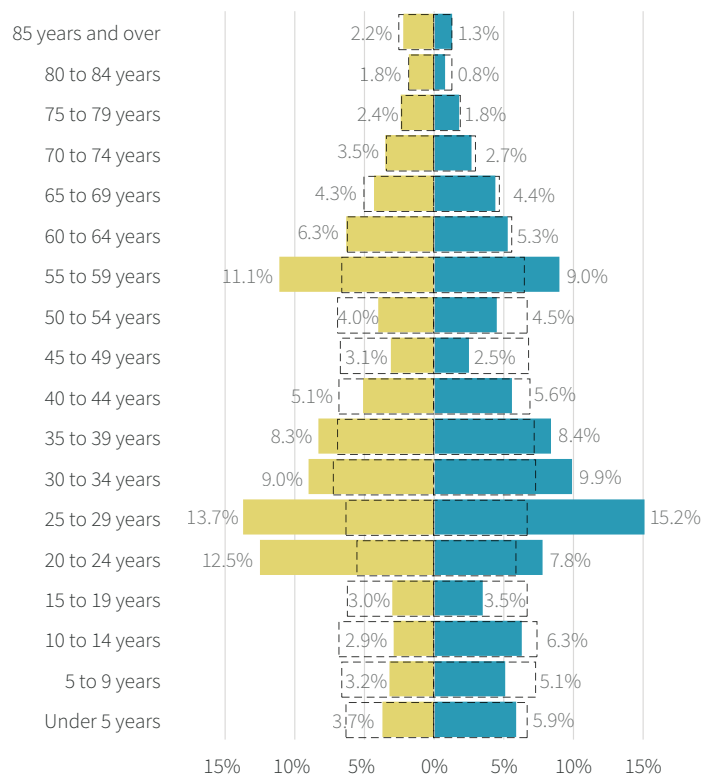
Table 1.9 - Question 9 Responses

Selected Answer	Count	Share
White/Caucasian	99	94.3%
Black	1	1.0%
Hispanic/Latino	2	1.9%
American Indian or Alaska Native	0	0.0%
Asian or Pacific Islander	0	0.0%
Multiracial or Biracial	3	2.9%
A race/ethnicity not listed here	0	0.0%
TOTAL	105	100.0%

Table 1.10 - Question 10 Responses

Selected Answer	Count	Share
Under 18 years	0	0.0%
18 to 34 years	32	31.4%
35 to 49 years	30	29.4%
50 to 64 years	20	19.6%
65 years and over	20	19.6%
TOTAL	102	100.0%

Figure 1.20 - Age Profile (2019) Mission and Johnson County for Reference



2.4 Interactive Website



INTERACTIVE WEBSITE - DESIRED AMENITIES SURVEY

The Desired Amenities Survey on the interactive website asked respondents to indicate what is missing in Mission today. The responses for this survey are located on the following pages.

Question 1 - Please rank the following items in order of importance for quality of life in Mission

Each option in Question 1 was given an average ranking of importance. The lower the average the more important it was to the most respondents.

Based on the results, the most important quality of life factors are:

- Safety and security
- City services (Public Works, Fire, Police)
- Affordable housing
- Walkability
- Parks

The least important quality of life factors based on the respondent ranking are:

- Recreational facilities (community center, aquatic center)
- Diversity
- Cultural activities (art, music, literature, theater, dance)
- Youth programs
- Parking availability

The least important quality of life factors may not necessarily indicate these things are not important to residents but may indicate they are important but Mission currently meets the needs of residents on these topics. Likewise, the most important factors may also be lacking.

Table 1.11 - Question 1 Responses

Quality of Life Factors, by ranked order of importance	Average Ranking
Safety and Security	4.0
City Services (Public Works, Fire, Police)	4.6
Affordable Housing	4.9
Walkability	5.0
Parks	5.7
Transportation Options (Walking, Driving, Bicycling, Transit)	6.7
Shopping Convenience	6.7
Access to Jobs	7.2
Recreational Facilities (Community Center, Aquatic Center)	7.3
Diversity	7.3
Cultural Activities (Art, Music, Literature, Theater, Dance)	8.0
Youth Programs	9.7
Parking Availability	10.3

2.4 Interactive Website



INTERACTIVE WEBSITE - DESIRED AMENITIES SURVEY

Question 2 - How important are the following items in terms of future development?

Question 2 asked respondents to rank items related to future development in Mission with the goal of identifying future development priorities of residents. Table 12 shows the average ranking for each item or development type. The lower the average ranking the more important the item based on survey responses.

The most desired future development factors include:

- Affordability of housing
- Walkability and bikeability
- Houses/Neighborhoods
- Parks (access to green spaces)
- Sustainability and energy efficiency

The least important future development factors include:

- Building design/aesthetics
- Smart technology (Wi-Fi)
- Parking availability

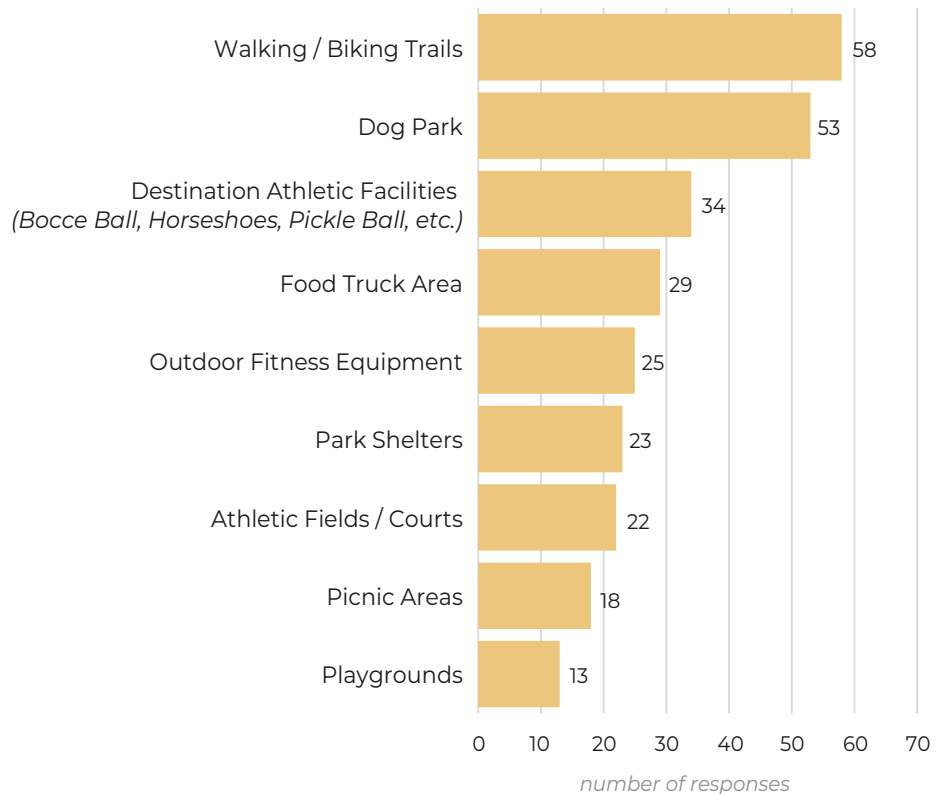
Question 3- Which type of park/open space amenities are currently missing or inadequate in Mission?

The clear two most common answers were walking/biking trails (58 vote) and dog parks (53 votes). There was also considerable support for destination athletic facilities (bocce ball, horseshoes, pickle ball, etc.) and a food truck area with 34 and 29 votes, respectively. The amenities there were less commonly identified as missing or inadequate was picnic areas (18 votes) or playgrounds (13 votes).

Table 1.12 - Question 2 Responses

Future Development Factors, by ranked order of importance	Average Ranking
Affordability of Housing	4.0
Walkability and Bikeability	4.2
Houses/Neighborhoods	4.5
Parks (Access to Green Spaces)	4.5
Sustainability and Energy Efficiency	4.9
Roads	5.0
Variety of Businesses	5.1
Building Design/Aesthetics	6.1
Smart Technology (Wi-Fi)	7.1
Parking Availability	8.4

Figure 1.21 - Question 3 Responses



2.4 Interactive Website



INTERACTIVE WEBSITE - DESIRED AMENITIES SURVEY

Question 4 - What would help you improve your health and well-being?

The most commonly selected options were enhancements to parks (60 votes) and walkability enhancements (trees, benches, etc.) (59 votes). More sidewalks (51 votes) was also a popular selection. The least selected options were improved transit (14 votes), fewer drive thru (20 votes) and more bicycle facilities (27 votes).

Overall, the most common answers related to recreation and walkability. These type of solutions were more common than intervention into the availability of healthy food or fast-food.

Question 5 - Prior to COVID-19, did you attend community events throughout the year in Mission?

The possible selections for Question 5 were “Yes, I went to events all the time”, “I wanted to, but I wasn’t able to attend”, “No, none of the events interested me”, or Not sure/not applicable. “Yes, I went to community events all the time” was the most selected option with 50 votes. Another 28 respondents said they wanted to but were not able to attend. Only 7 said none of the events interested me. Overall, community events appear to be reasonably well attended or at least there is a community desire to attend even if they can’t make many events due to scheduling.

Figure 1.22 - Question 4 Responses

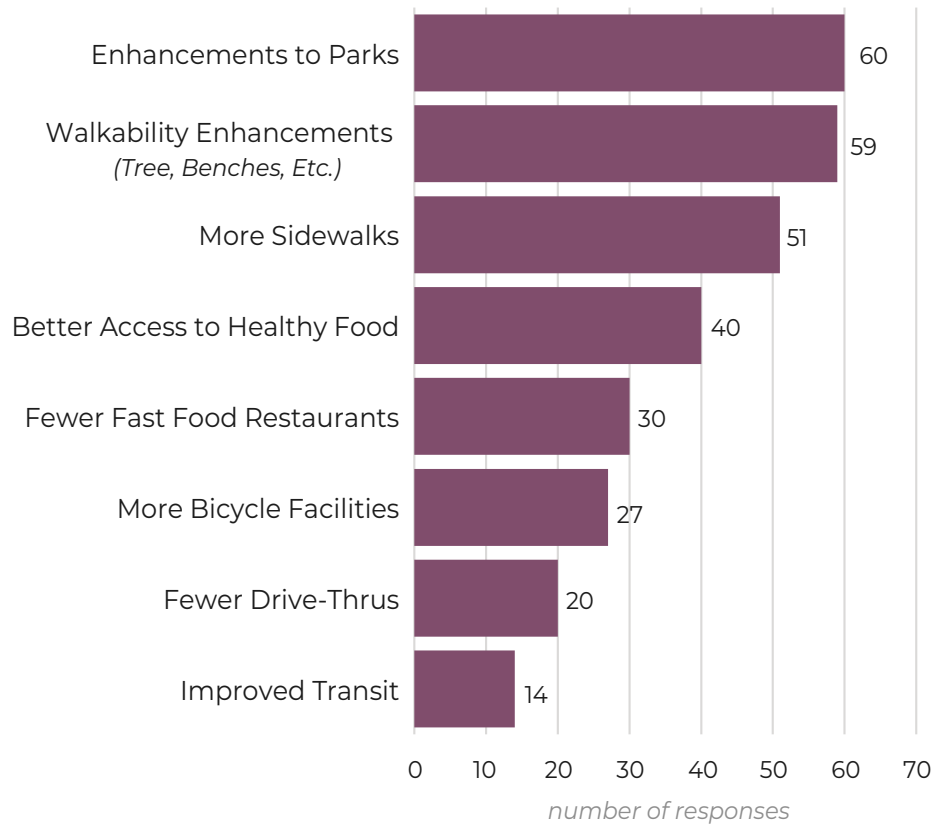
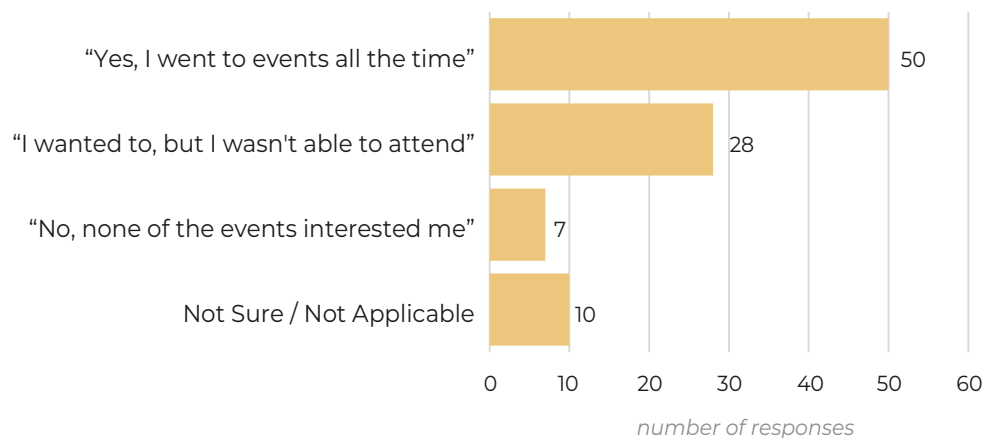


Figure 1.23 - Question 5 Responses



2.4 Interactive Website

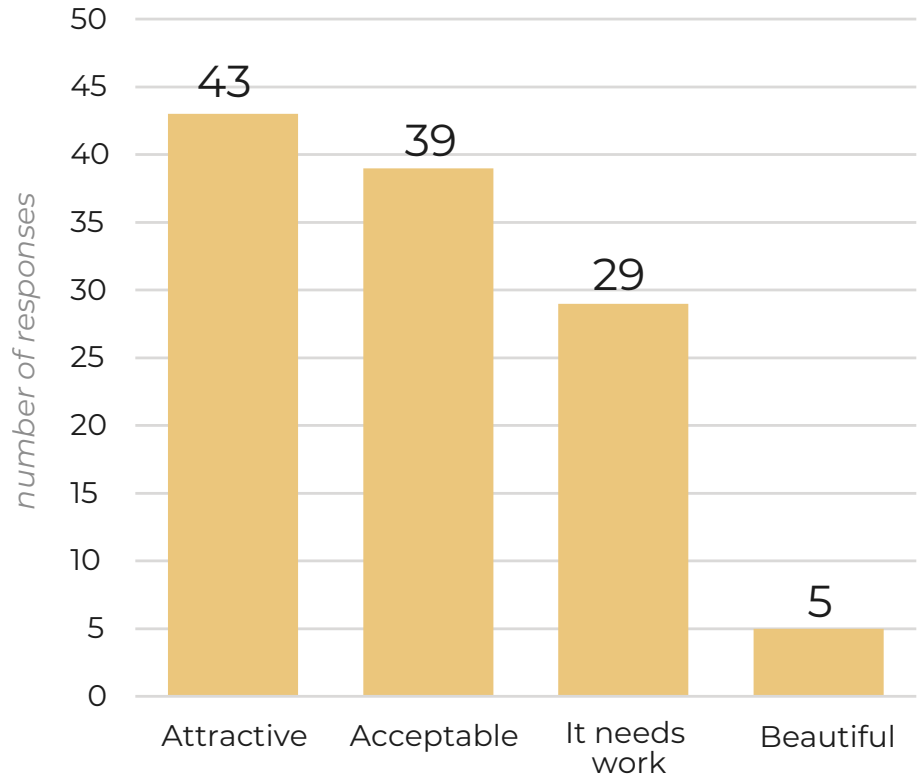


INTERACTIVE WEBSITE - DESIRED AMENITIES SURVEY

Question 8 - Do you think the visual appearance of Mission is...

Question 8 asked respondents about their thoughts on the visual appearance of Mission. They were asked to select from four possible descriptors: Beautiful, Attractive, Acceptable, or It needs work. Attractive was the most commonly selected response with 43 votes. Close behind was Acceptable with 39 votes. Another 29 respondents said Mission needs to work on its visual appearance. Only five respondents said the visual appearance of Mission was beautiful.

Figure 1.25 - Question 8 Responses



2.4 Interactive Website



INTERACTIVE WEBSITE - PRIORITY BUDGET TOOL

What should Mission fund?

Participants were asked to “Tell us about your vision for Mission in 2040 by writing a postcard to your friend/family.”

There were six responses, shown below.

In terms of number of votes, the top four priority items were Residential Streets (58 votes), Park Improvements (53 votes), Community Events (42 votes) and Streetscape Improvements (42 votes).

Each priority was assigned a valuation of estimated cost. While these numbers were somewhat arbitrary (in that they were not based on actual estimated cost of any one improvement) they did act as a limiting factor on votes. The total amount of funding for each priority is summarized in Table 15. The top four most funded priorities were Trail Expansion (\$11,100,000), Park Improvements (\$10,600,000) and Major Streets (\$9,000,000).

Table 1.15 - Priority Items by Total Votes

Priority Item	Total Votes
Residential Streets	58
Park Improvements	53
Community Events	42
Streetscape Improvements	42
Major Streets	36
Neighborhood Preservation	36
Trail Expansion	36
Recreation Programs	35
Sidewalks, Bike Lanes, Trails, and Transit Stops	15

Table 1.16 - Priority Items by Total Allocated Funding

Priority Item	Total Funding
Trail Expansion	\$11,100,000
Park Improvements	\$10,600,000
Major Streets	\$9,000,000
Streetscape Improvements	\$8,400,000
Residential Streets	\$7,250,000
Sidewalks, Bike Lanes, Trails, and Transit Stops	\$7,250,000
Neighborhood Preservation	\$5,400,000
Recreation Programs	\$1,750,000
Community Events	\$1,050,000



What should Mission Fund?

Budget: \$1,000,000

Residential Streets \$125,000
\$ Fund

Major Streets \$250,000
\$ Fund

Sidewalks, Bike Lanes, Trails and Transit Stops \$250,000
\$ Fund

Park Improvements \$200,000
\$ Fund

Neighborhood Preservation \$150,000
\$ Fund

Trail Expansion \$300,000
\$ Fund

Recreation Programs \$50,000
\$ Fund

Community Events \$25,000
\$ Fund

Streetscape Improvements \$300,000
\$ Fund




2.5 Social Media Polling

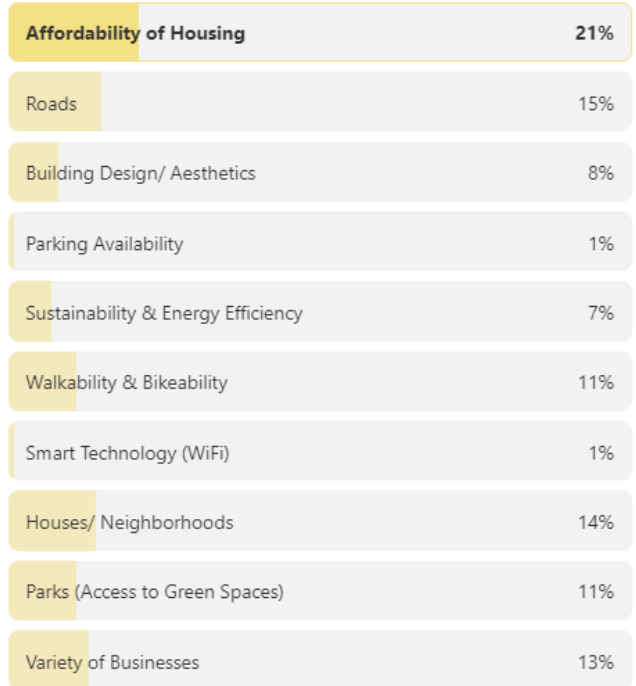
SOCIAL MEDIA POLLING: DECEMBER 2020 - JANUARY 2021

In an effort to extend public input to an even wider audience, the City of Mission staff began posting polling questions to their city social media sites. The results of the polls will be provided below by question.

What is most important for future development in Mission?

Comments:

- 
Jeff S. • East Mission ⋮
 It seems like 'Safety/Security' should be on this list and would have been my vote. Without it/them, none of the other attributes and amenities of our community and neighborhoods... See more
 16 Dec [Like](#) [Reply](#) 👍 1
- 
Virginia L. • Lido Villas ⋮
 Something about meeting the needs of the elderly should also be in here. Walkability is part of it; so is affordable housing, and, for that matter, security. I'd like this city to... See more
 17 Dec [Like](#) [Reply](#) 👍 1
- 
Mike R. • Milhaven ⋮
 Get the eyesore Gateway completed.
 18 Dec [Like](#) [Reply](#) ❤️ 2



103 votes




Posted to **Subscribers of City of Mission**

👍 Like [3 Comments](#)

❤️👤 5 · 860 Impressions

What would improve your health and well-being?

Comments:

- 
Chester S. • Oakwood ⋮
 If "enhancement to parks" means a dog park that one.
 19 Dec [Like](#) [Reply](#)
- 
Cindy S. • Countryside ⋮
 More bathrooms to Parks
 19 Dec [Like](#) [Reply](#)
- 
Aaron S. • Oakwood ⋮
 Where's the option for 'Anything that won't cause construction/closures to Mission streets' ?
 19 Dec [Like](#) [Reply](#)



76 votes

Posted to **Subscribers of City of Mission**

👍 Like [3 Comments](#)


· 789 Impressions


2.5 Social Media Polling


SOCIAL MEDIA POLLING: DECEMBER 2020 - JANUARY 2021


What would you love to see in Mission?


Comments:

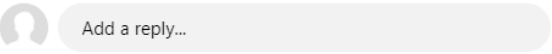

- 


Chalanna N. • Oakwood ...
 Can we get a variety of these options? They're all great additions!
 17 Dec [Like](#) [Reply](#) ❤️ 1
 - 


Meghan C. • East Mission ...
 A Dog park
 17 Dec [Like](#) [Reply](#) ❤️ 3
 - 


Aaron S. • Oakwood ...
 second
 17 Dec [Like](#) [Reply](#)
 - 


Chester S. • Oakwood ...
 This request continues to get ignored. Would get used more than any of these other options.
 17 Dec [Like](#) [Reply](#)
 - 


Marissa M. • East Mission ...
 Yes!!! Would love having a dog park in town
 18 Dec [Like](#) [Reply](#)
- 
- 


Tara M. • Walmer Homestead ...
 Sidewalks on residential streets
 17 Dec [Like](#) [Reply](#) ❤️ 2
 - 

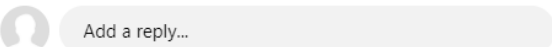
Ruth B. • Milhaven ...
 More upscale housing with features that attract today's families.
 17 Dec [Like](#) [Reply](#) ❤️ 1
 - 

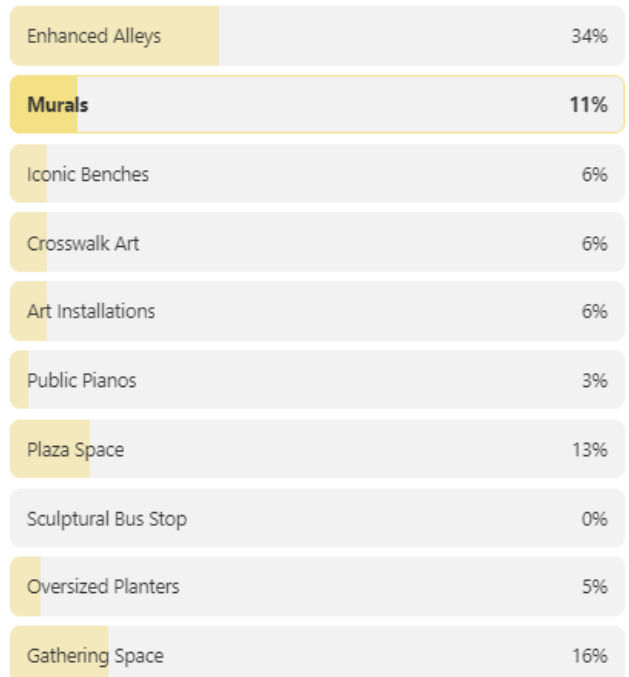
Ryan B. • Alta Vista Heights ...
 None trying to hard. Make all the road nice, let's say 300-500 yards to Lamar then talk, new side walks, curbs, cross walks, lighting, landscaping etc
 17 Dec [Like](#) [Reply](#) ❤️ 1
 - 

Mark N. • East Mission ...
 How about fixing some curbs that has been promised for years. And some street work wouldn't hurt.
 17 Dec [Like](#) [Reply](#) ❤️ 2
 - 

J G V. • Milhaven ...
 None of the items listed are more important than some of the real needs mentioned in the comments: enhanced street lighting; promised curb and neighborhood sidewalk improvements; a... See more
 17 Dec [Like](#) [Reply](#) ❤️ 3
 - 

Nicole R. • Milhaven ...
 By "enhanced street lighting" do you mean brighter? I do like the street lights the way they are now in Milhaven and wouldn't want them any brighter.
 20 Dec [Like](#) [Reply](#)
 - 

J G V. • Milhaven ...
Nicole Routh There are many new forms of street lighting, including types which reduce the light pollution created by older lighting systems and can focus more... See more
 20 Dec [Like](#) [Reply](#) ❤️ 1
- 



116 votes

Posted to **Subscribers of City of Mission**

 Like
  11 Comments
 ❤️ 2 · 959 Impressions

Comments continued:

- 

Dominic A. • Milhaven ...
 I second the other comments, we need more modern and improved lighting, streets, street signs, sidewalks. Let's improve on current infrastructure to mirror our neighbors in PV... See more
 17 Dec [Like](#) [Reply](#) ❤️ 1
- 

Blair B. • Oakwood ...
 The alleys wood be cool!
 22 Dec [Like](#) [Reply](#)
- 

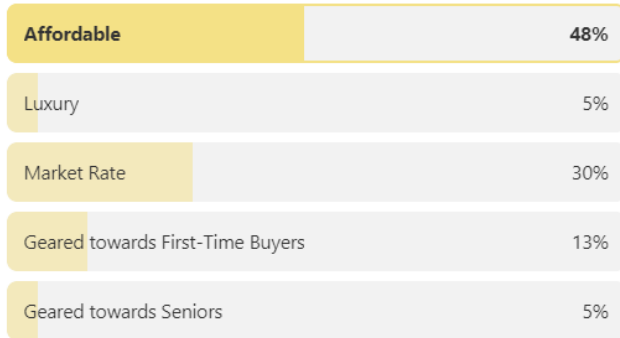
Josh T. • Mission Village ...
 Hi Neighbors, some good thoughts and comments here. I encourage you to also submit them to the Confluence website.
<https://confluence.mysocialpinpoint.com/mission-home>
 The map feature where you can drop a pin and comment is really neat, and the surveys and polls have areas for leaving comments as well.
 22 Dec [Like](#) [Reply](#) ❤️ 1
- 

Sarah W. • East Mission ...
 Street lights and sidewalks before any of the other things listed. It's dangerous for so many streets to be so dark. (edited)
 22 Dec [Like](#) [Reply](#) ❤️ 1

2.5 Social Media Polling

SOCIAL MEDIA POLLING: DECEMBER 2020 - JANUARY 2021

It is most important for housing in Mission to be...



40 votes

Posted to **Subscribers of City of Mission**

Like Comment

330 Impressions

Comments:

(No Comments)

Should accessory dwelling units be allowed in Mission?



143 votes

Posted to **Subscribers of City of Mission**

Like 4 Comments

926 Impressions

Comments:

- J** J G V. • Milhaven ...

You would really have to beef up code enforcement and rental property regulations to keep these from turning into short term internet rentals and to keep the new landlords in... See more

17 Dec Like Reply ❤️ 2
- B** Bryan L. • East Mission ...

Yes please! Allowing for ADUs is a great idea and grants homeowners more property rights.

17 Dec Like Reply
- J** Jan K. • Walmer Homestead ...

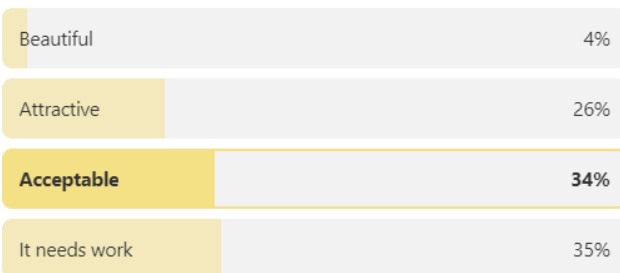
Given some neighbors I've had, I think some rules about proximity to neighbor dwellings would have to be put in place, for noise control and light pollution control.

17 Dec Like Reply -1 1
- C** Casey C. • Countryside ...

It's not that easy. There are deed restrictions that limit accessory buildings for some of us.

17 Dec Like Reply -1 1

Do you think the visual appearance of Mission is...



68 votes

Posted to **Subscribers of City of Mission**

Like 1 Comment

485 Impressions

Comments:

- L** Laura C. • Countryside ...

I walk on the walk/bike trail just north of Johnson Drive between Martway and Nall almost everyday. I notice that the city cut down some volunteer trees but they did not pick up... See more

21 Dec Like Reply

2.5 Social Media Polling

SOCIAL MEDIA POSTS: FALL 2020 - WINTER 2020/2021

The City of Mission posted several times on their Facebook account to promote the Social Pinpoint interactive engagement website as well as receive comments on the posts themselves. Below are the posts and any comments.

Facebook Post #1

mission City of Mission, KS - Government
Published by Cityof Mission · October 14, 2020 ·

<https://confluence.mysocialpinpoint.com/mission-home>

Add your voice. Shape the future of Mission. Share what you like, what you don't like, and things you'd like to see improved.

438 People Reached 56 Engagements [Boost Post](#)

7 1 Share

Comments:

- Jim Carlet**
Joni Mitchell "They paved paradise
And put up a parking lot
With a pink hotel, a boutique
And a swinging hot spot
Don't it always seem to go
That you don't know what you've got
Till it's gone
They paved paradise
And put up a parking lot"
Not what I would like to see.
Like · Reply · Message · 7w
- Kandace G Khoury**
Well... I tried this and the website would not let me submit.
Like · Reply · Message · 7w
- Author**
City of Mission, KS - Government
Hi Kandace, We would love to help you with this! If you would like to try again, please feel free to call us at 913-676-8360 and we can walk through it together. Thanks!
Like · Reply · Commented on by Cityof Mission · 7w

Facebook Post #2

mission City of Mission, KS - Government
Published by Cityof Mission · October 21, 2020 ·

<https://confluence.mysocialpinpoint.com/mission-home>

Add your voice. Shape the future of Mission. Share what you like, what you don't like, and things you'd like to see improved.

1,029 People Reached 188 Engagements [Boost Post](#)

14 15 Comments 3 Shares

Comments:

- Lisa DiMaio Arnett**
Leaf clean-up, like Roeland Park does.
Like · Reply · Message · 16w
- Nikki Shippee**
I would love to see 55th between Lamar and Nall fixed, the street and curbs are atrocious and make it so challenging for cars to pass each other.
Like · Reply · Message · 16w
- Irvinlee Leonard**
btw 58th and jo dr on riggs...you know where all the slow down kids at play signs are. you need to post no parking signs on one side of the street. the street is congested with parked cars which makes it extremely dangerous for children. ive been compl... See More
Like · Reply · Message · 16w
- Jennifer Casper Barton**
Obviously, the mall, and SM parkway between Lamar and Metcalf going west
Like · Reply · Message · 15w
- Sheila Coakley**
I like when you have the art fair and I like when the farmers market is here.
Like · Reply · Message · 16w
- Lynn Bain**
It would be nice to have a bike rack on each block in the business area along Johnson drive.
Like · Reply · Message · 15w
- Lisa DiMaio Arnett**
Also, could you trim low hanging trees that are over the streets?
Like · Reply · Message · 16w

2.5 Social Media Polling

SOCIAL MEDIA POSTS: FALL 2020 - WINTER 2020/2021

Facebook Post #3

mission City of Mission, KS - Government
Published by Cityof Mission · October 26, 2020 ·

Grab a blanket and provide feedback from the comfort of your couch on this snowy fall morning.

Add your voice. Shape the future of Mission.
<https://confluence.mysocialpinpoint.com/mission-home>



947
People Reached

36
Engagements

Boost Post

5

4 Shares

Facebook Post #4

mission City of Mission, KS - Government
Published by Cityof Mission · December 17, 2020 ·

How do you envision the future of development, transportation, and housing in Mission?

Add your ideas here:
<https://confluence.mysocialpinpoint.com/mission-home>



811
People Reached

89
Engagements

Boost Post

7

3 Comments 4 Shares

Comments:

Verna Whitworth Crane
That unfinished project at Johnson Drive and Roe is an eye sore. I bet if I graded dirt and partly built a house and then walked the city would have a way to get it away from me. This makes the city look bad to not be able to handle this and get something done or get the site cleaned up again.

Like · Reply · Message · 15w

Sheila Coakley
What about finishing where the mall use to be. What is going on with it.

Like · Reply · Message · 16w

Cindy Wood Bowen
How about a shopping center at Johnson Drive and Roe with a drug store, grocery store, dime store, cafeteria, a Macy's, a couple of shoe stores, dress shops, and a place to take music lessons.

Like · Reply · Message · 15w

Marilyn Siebrasse
Cindy Wood Bowen I don't know where that intersection is but what you described sounds just like all the things that were up and down Johnson Drive when we were kids. Especially the dime store - TG&Y I think it was.

Like · Reply · Message · 15w

Reply as City of Mission, KS - Govern...

Nicole Vela
Bathrooms, water fountains, and updated Parks. Mission Parks have not been updated in so many years.

Like · Reply · Message · 10w

Christy Milford
I love that trash service is paid for with our property taxes - would love it if leaf removal services were also included!

Like · Reply · Message · 10w

Comments:

Top Fan
Robyn Dozier Fulks
Bathrooms at Waterworks Park would be #1 on my family's wishlist!

Like · Reply · Message · 10w

Janet Gaines Hartley
For right now and immediate future the city of Mission should be recommending residents to stay home as much as possible and to wear a mask if they must go out.

Like · Reply · Message · 11w

2.5 Social Media Polling

NEXT DOOR POLLS: FALL 2020 - WINTER 2020/2021

Area Polls #1-#3

Below are the results from a series of Next Door polling completed by the City of Mission using survey questions.

Table 1.17 - It is Most Important for Housing in Mission to be...

Options	Votes
Affordable	17
Luxury	2
Market Rate	12
Geared towards First-Time Home Buyers	5
Geared towards Seniors	2
TOTAL	38

Table 1.18 - Do you Think the Visual Appearance of Mission is...

Options	Votes
Beautiful	3
Attractive	18
Acceptable	21
It needs work	24
TOTAL	66

Table 1.19 - What would help you improve your health and well-being?

Options	Votes
More Sidewalks	20
More Bicycle Facilities	5
Enhancements to Parks	10
Walkability Enhancements (Trees, Benches, Etc.)	17
Improved Transit	3
Better Access to Healthy Food	6
Fewer Drive-Thrus	3
Fewer Fast Food Restaurants	10
TOTAL	74

2.5 Social Media Polling

NEXT DOOR POLLS: FALL 2020 - WINTER 2020/2021

Next Door Polls #4-#6

Below are the results from a series of Next Door polling completed by the City of Mission using survey questions.

Table 1.20 - What Would you Love to see in Mission?

Options	Votes
Enhanced Alleys	39
Murals	12
Iconic Benches	7
Crosswalk Art	7
Art Installations	7
Public Pianos	3
Plaza Space	15
Sculptural Bus Stop	0
Oversized Planters	6
Gathering Space	18
TOTAL	114

Table 1.21 - Should Accessory Dwelling Units be Allowed in Mission?

Options	Votes
Yes	104
No	25
I'm not sure	12
TOTAL	141

Table 1.22 - What is Most Important for Future Development in Mission?

Options	Votes
Affordability of Housing	20
Roads	15
Building Design/Aesthetics	8
Parking Availability	1
Sustainability and Energy Efficiency	7
Walkability and Bikeability	11
Smart Technology (WiFi)	1
Houses/Neighborhoods	14
Parks (Access to Green Spaces)	11
Variety of Businesses	13
TOTAL	101

2.5 Social Media Polling

NEXT DOOR POLLS: FALL 2020 - WINTER 2020/2021

Neighborhood Results for Polls #1-#2

Below are the results from a series of Next Door polling completed by the City of Mission using survey questions.

Table 1.23 - It is Most Important for Housing in Mission to be...

NEIGHBORHOOD	Affordable	Luxury	Market Rate	First-Time Buyers*	Seniors*	TOTAL VOTES
East Mission	5	1	5	1	0	12
Reeds hill	1	0	0	0	0	1
Oakwood	5	0	1	1	1	8
Wellington	1	0	0	0	0	1
Milhaven	2	0	4	0	0	6
Mission Hills Acre	1	1	0	1	0	3
Alta Vista Heights	1	0	1	2	0	4
Santa Fe Manor	1	0	0	0	0	1
Countryside	0	0	1	0	1	1
TOTAL	17	2	12	5	2	38

* Homes geared toward

Table 1.24 - Do you Think the Visual Appearance of Mission Neighborhoods are...

NEIGHBORHOOD	Beautiful	Attractive	Acceptable	It needs work	TOTAL VOTES
Alta Vista Heights	1	2	2	2	7
Lido Villas	1	0	0	0	1
Mission Village	0	1	0	0	1
Reeds Hill	0	1	0	0	1
Wellington	0	1	0	0	1
Mission Hills Acres	0	3	1	0	4
Oakwood	0	3	3	7	13
East Mission	0	3	3	7	13
Countryside	0	1	1	5	7
Milhaven	0	1	5	3	9
Walmer Homestead	0	0	1	1	2
Santa Fe Manor	0	0	1	0	1
TOTAL	3	18	21	24	66

2.5 Social Media Polling

NEXT DOOR POLLS: FALL 2020 - WINTER 2020/2021

Neighborhood Results for Polls #3

Below are the results from a series of Next Door polling completed by the City of Mission using survey questions.

Table 1.25 - What Would Help you Improve your Health and Well-Being?

NEIGHBORHOOD	More Sidewalks	More Bicycle Facilities	Enhancements to Parks	Walkability Enhancements	Improved Transit
East Mission	6	2	0	3	0
Countryside	3	0	2	3	1
Mission Hills Acres	1	0	1	1	0
Oakwood	5	2	3	2	1
Lido Villas	2	0	0	1	0
Alta Vista Heights	1	1	2	0	0
Mission Village	1	0	0	1	0
Walmer Homstead	1	0	0	1	0
Reeds Hill	0	0	1	1	0
Milhaven	0	0	1	4	1
TOTAL	20	5	10	17	3

NEIGHBORHOOD	Better Access to Healthy Food	Fewer Drive-Thrus	Fewer Fast Food Restaurants	TOTAL VOTES
East Mission	4	0	3	18
Countryside	1	0	3	13
Mission Hills Acres	0	0	1	4
Oakwood	1	1	1	16
Lido Villas	0	1	4	5
Alta Vista Heights	0	1	0	5
Mission Village	1	1	4	1
Walmer Homstead	0	0	0	2
Reeds Hill	0	0	0	2
Milhaven	0	0	0	6
TOTAL	6	3	10	74

2.5 Social Media Polling

NEXT DOOR POLLS: FALL 2020 - WINTER 2020/2021

Neighborhood Results for Polls #4

Below are the results from a series of Next Door polling completed by the City of Mission using survey questions.

Table 1.26 - What Placemaking Elements would you like to see in Mission?

NEIGHBORHOOD	Enhanced Alleys	Murals	Iconic Benches	Crosswalk Art	Art Installations
Oakwood	11	4	1	2	1
Countryside	6	1	2	0	1
Milhaven	3	0	0	1	2
East Mission	6	2	2	2	2
Mission Village	4	0	0	1	0
Lido Villas	3	0	1	0	0
Wellington	1	0	0	0	0
Alta Vista Heights	2	2	1	1	1
Mission Hills Acres	3	0	0	0	0
Reeds Hill	0	1	0	0	0
Walmer Homestead	0	1	0	0	0
Santa Fe Manor	0	1	0	0	0
Mission	0	0	0	0	0
TOTAL	39	12	7	7	7

NEIGHBORHOOD	Public Pianos	Sculptural Bus Stop	Oversized Planters	Gathering Space	TOTAL VOTES
Oakwood	10	0	2	5	37
Countryside	0	0	1	4	15
Milhaven	2	0	0	1	11
East Mission	1	0	0	4	19
Mission Village	0	0	1	1	7
Lido Villas	0	0	0	0	4
Wellington	0	0	0	0	1
Alta Vista Heights	2	0	1	1	11
Mission Hills Acres	0	0	0	2	5
Reeds Hill	0	0	0	1	3
Walmer Homestead	0	0	0	0	1
Santa Fe Manor	0	0	0	0	1
Mission	0	0	1	0	1
TOTAL	15	0	6	18	114

2.5 Social Media Polling

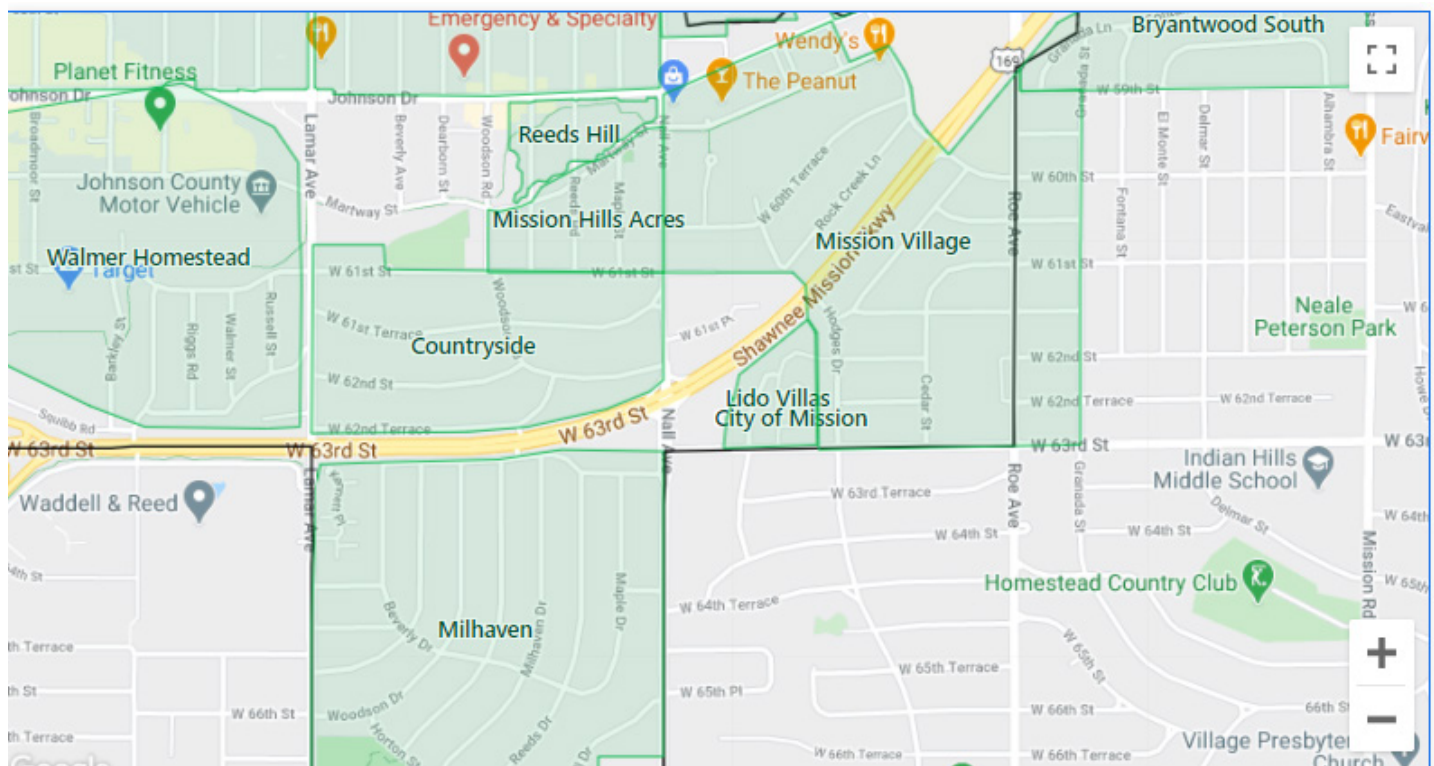
NEXT DOOR POLLS: FALL 2020 - WINTER 2020/2021

Neighborhood Results for Polls #5

Below are the results from a series of Next Door polling completed by the City of Mission using survey questions.

Table 1.27 - Should Accessory Dwelling Units be Allowed in Mission (by neighborhood)?

NEIGHBORHOOD	Yes	No	I'm not sure	TOTAL VOTES
Alta Vista Heights	10	2	0	12
Walmer Homestead	4	0	1	5
Wellington	18	3	1	22
East Mission	18	3	1	22
Santa Fe Manor	2	0	1	3
Oakwood	33	3	4	40
Milhaven	15	5	2	22
Mission Village	7	3	0	10
Mission Hills Acres	3	0	1	4
Countryside	10	7	1	18
Lido Villas	1	1	0	2
Reeds Hill	0	1	1	2
TOTAL	104	25	12	141



2.5 Social Media Polling

NEXT DOOR POLLS: FALL 2020 - WINTER 2020/2021

Neighborhood Results for Polls #6

Below are the results from a series of Next Door polling completed by the City of Mission using survey questions.

Table 1.28 - What is Most Important for Future Development in Mission?

NEIGHBORHOOD	Affordability of Housing	Roads	Building Design /Aesthetics	Parking Availability	Sustainability and Energy Efficiency
Reeds Hill	1	0	0	0	0
Milhaven	4	3	0	0	0
Walmer Homestead	1	0	0	0	0
Wellington	1	0	0	0	0
Countryside	2	6	2	0	2
Oakwood	4	4	2	0	3
East Mission	4	1	1	1	1
Mission Hills Acres	1	0	0	0	0
Lido Villas	1	0	0	0	0
Alta Vista Heights	1	1	0	0	0
Mission Village	0	0	3	0	1
Mission	0	0	0	0	0
Santa Fe Manor	0	0	0	0	0
TOTAL	20	15	8	1	7

NEIGHBORHOOD	Walk and Bikeability	Smart Technology	Houses/ Neighborhoods	Park Access	Variety of Businesses	TOTAL VOTES
Reeds Hill	0	0	0	0	0	1
Milhaven	0	0	2	2	2	13
Walmer Homestead	0	0	0	0	0	1
Wellington	0	0	0	0	0	1
Countryside	2	1	0	1	0	16
Oakwood	2	0	4	1	3	18
East Mission	2	0	4	1	3	18
Mission Hills Acres	1	0	0	1	1	4
Lido Villas	1	0	0	0	0	2
Alta Vista Heights	1	0	0	1	0	6
Mission Village	1	0	0	1	0	6
Mission	0	0	1	0	0	1
Santa Fe Manor	0	0	1	1	0	2
TOTAL	11	1	14	11	13	101

Appendix A

EXISTING CONDITIONS REPORT

Table of Contents

1.0 Community Profile	173
2.0 Physical Profile	186
3.0 Market Analysis	209

1.0 Community Profile

1.1 Regional Context and Fast Facts

Regional Context

The City of Mission is a northeast Johnson County community in the Kansas City metropolitan area. Most of the major activity centers within the Kansas City region fall within a 10-mile buffer of Mission (Figure 1.1). Mission offers easy access to Interstate 35

and Downtown Kansas City, Missouri. Interstate 35 runs near the northern section of the community. Metcalf Avenue (Highway 69) marks most of its western boundary and Shawnee Mission Parkway (Highway 169) makes up a portion of its southern boundary. Mission is a first-ring suburb and is

landlocked on all sides by Overland Park, Roeland Park, Fairway, Prairie Village, and Wyandotte County, Kansas.



Figure 1.1 - Regional Context Map

Mission, Kansas Fast Facts

<p> Population (2020): 9,618</p> <p> Population Change 2010 - 2020: +3.4%</p> <p> Median Age: 33.4</p>	<p> Median Household Income: \$64,703</p> <p> Median Home* Value: \$203,600 <small>*owner occupied</small></p> <p> Median Gross Rent: \$1,016</p>
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Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

1.2 Base Map

Base Map

Mission is a landlocked community that is approximately 2.62 square-miles. It has two main east-west arterials, Johnson Drive and Shawnee Mission Parkway. There are several north-south major roads including Metcalf Avenue, Lamar Avenue, Nall Avenue, and Roe Avenue.

The land along Johnson Drive, Martway Drive, and Metcalf Avenue are the most intensely developed areas. Outside of these areas, the dominant land use is low-density residential.

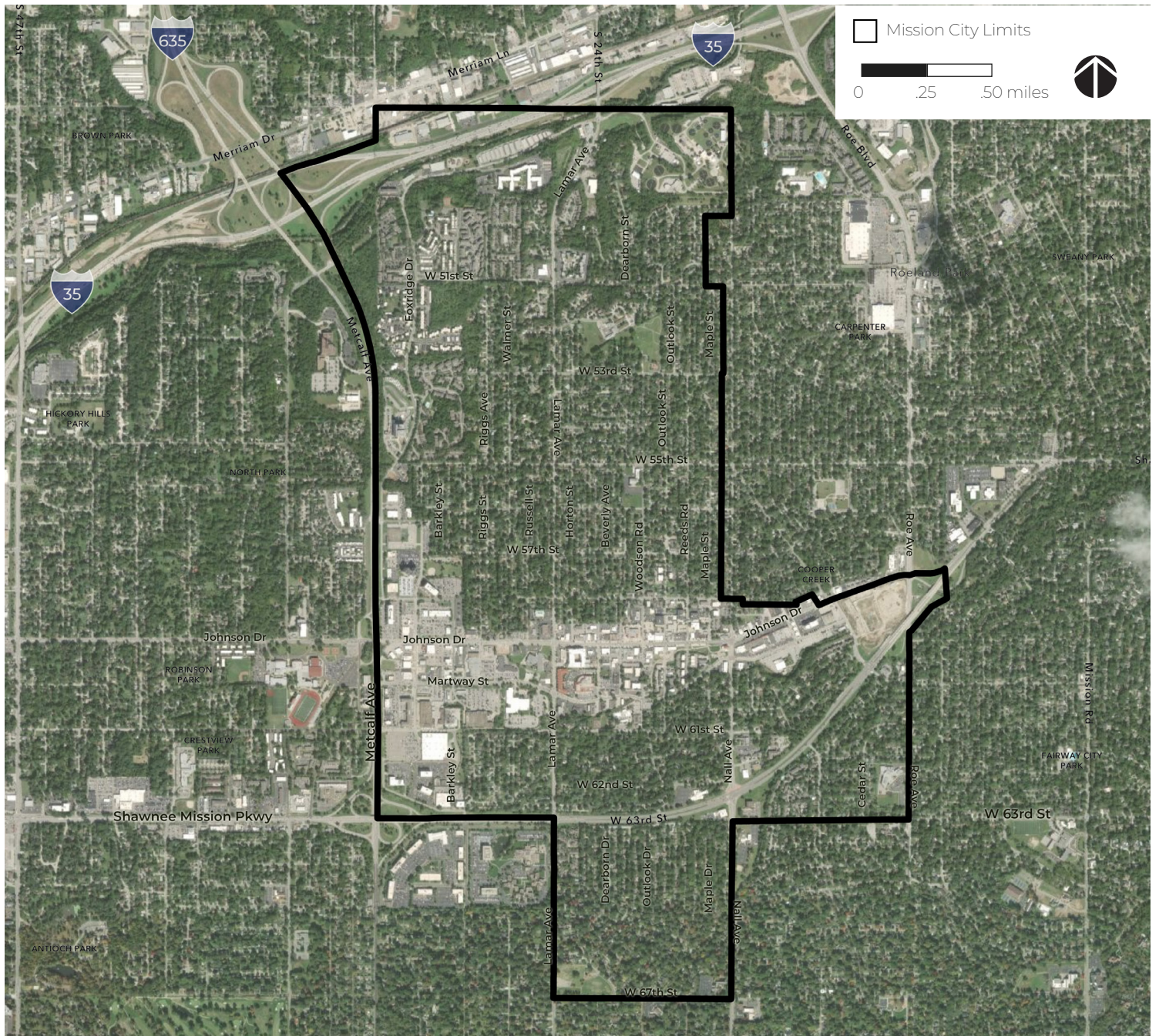


Figure 1.2 - Base Map of Mission, Kansas

1.3 Population and Growth

Short-Term Population Trends

Mission's population has hovered between an estimated 9,400 to 9,500 residents for many years, but has begun to see a slight increase in the past two years of estimates (Figure 1.3). The latest population estimate for 2020 puts the current population at approximately 9,618. Since 2010, this represents an increase of 3.4%.

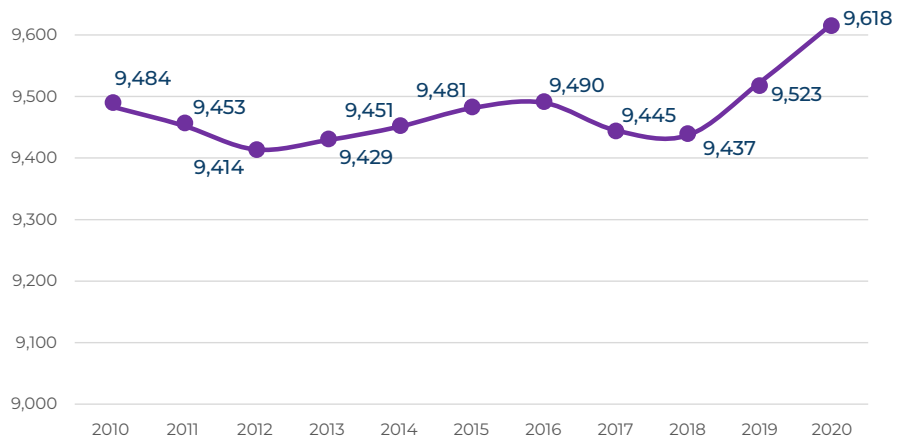
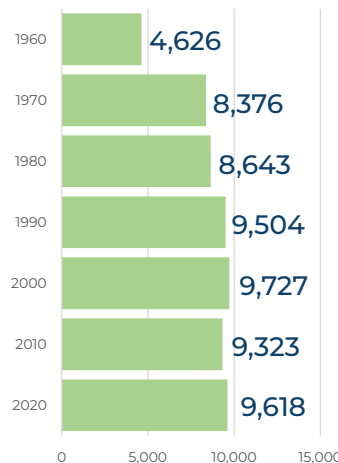


Figure 1.3 - ACS Population Estimates 2010-2020

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

Long-Term Population Trends

In 1960, Mission's population was an estimated 4,626. Over the past sixty years, the community has more than doubled in size. A substantial amount of the population growth occurred between 1960 to 1970. The population had evened out since 1990 to around 9,500 residents. Latest estimates are up closer to 9,700. Additional future growth will depend on several macro and micro-level trends and changes discussed below.



DESPITE BEING A LANDLOCKED COMMUNITY, MISSION'S GROWTH HAS CONTINUED TO SLOWLY INCREASE

Figure 1.4 - Population 1960 - 2020

Source: U.S. Census Bureau

Population Growth

Mission is a landlocked community. Therefore, its population growth will mainly stem from redevelopment, densification, or a significant change in household size. Higher density mixed-use or residential redevelopment could allow Mission's population to grow. The overall average household size in Mission in 2020 was 2.01 persons. Families in Mission have an average household size of 2.68 persons. The 2020 estimates were slightly less - 1.98 persons for total average household size and 2.63 for families.

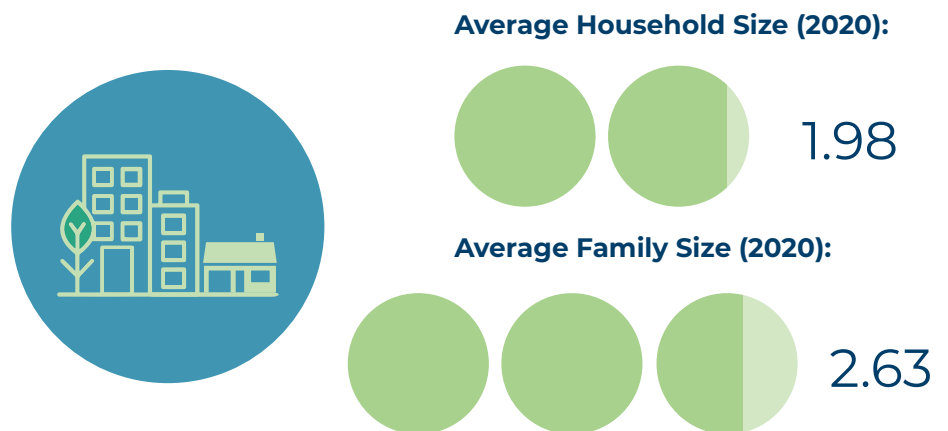


Figure 1.5 - Average Household and Family Size (2020)

1.4 Households and Families

Household Sizes

There are a nearly equal percentage of one and two-person households in Mission as of 2020 (Figure 1.6). Together, these two household sizes account for close to 80% of all households in Mission. Another nearly 15% are four-person and 8% are three-person households.

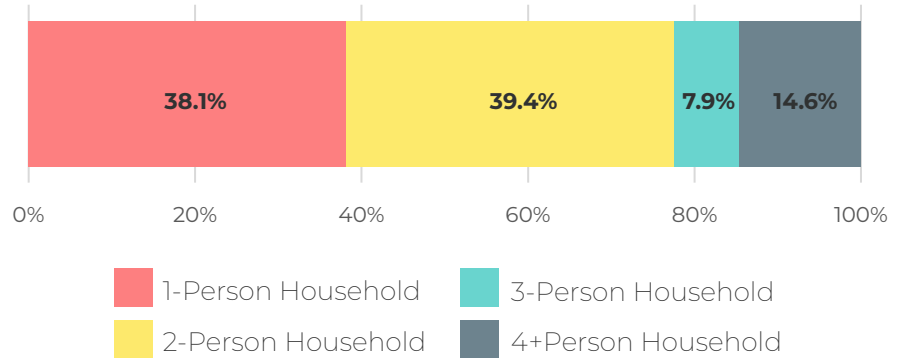


Figure 1.6 - Household Sizes (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

Average Household Sizes

The overall average total household size for Mission in 2020 was 1.98 persons, down from 2.01 persons in 2019.

Average household size is often related to housing tenure. Housing tenure is the financial arrangement and ownership status under which someone has the right to live in a house. Owner-occupied households in Mission have an average household size of 2.22 persons. Renter-occupied households are typically smaller. This remains the case with Mission where average renter households are 1.76 persons, down from 1.82 persons in the year prior.

Average household size often reflects the demographics of a community (mainly age, income, or educational attainment), but can also reflect the housing options available. Smaller homes and multi-family residential units are often better suited for smaller average household sizes. Mission's recent growth, which mostly can be attributed to multi-family units, can help explain this downward trend in household sizes.

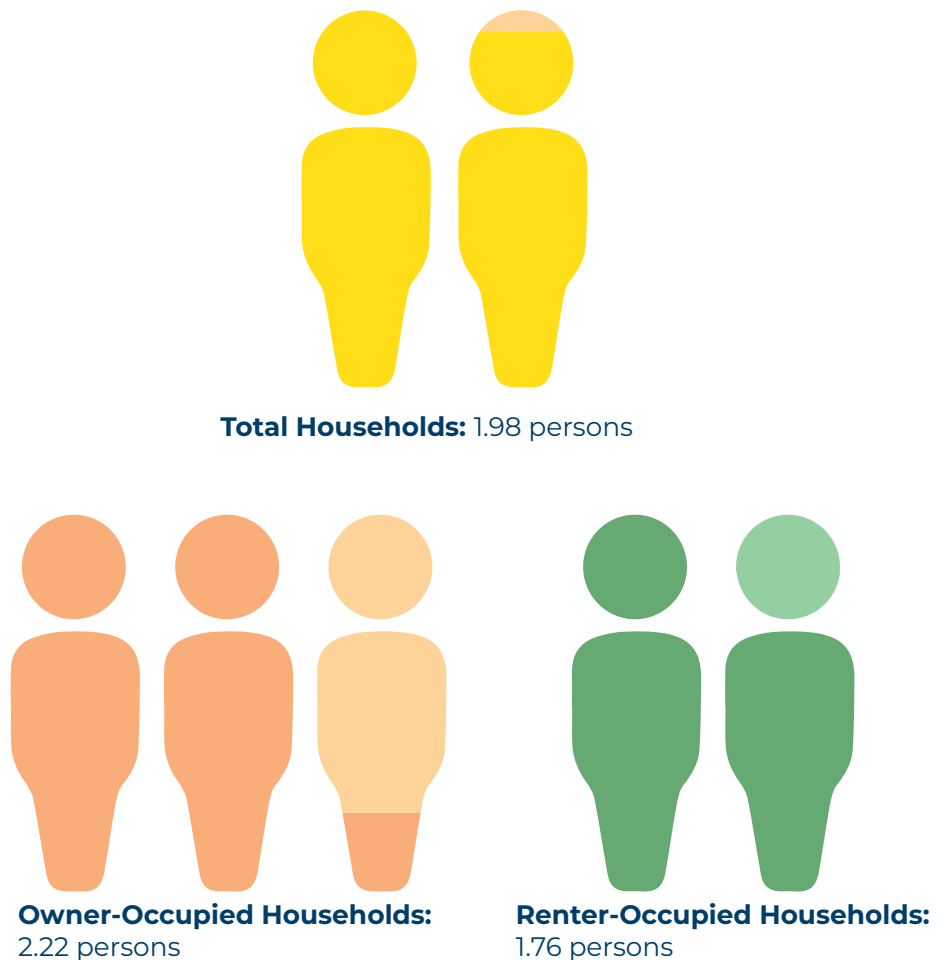


Figure 1.7 - Average Household Size by Tenure (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

1.5 Household Incomes

Median Household Income

The 2020 median household income in Mission was \$64,703, up from \$61,144 in 2019. While this is similar to the nearby community of Merriam, it is lower than other Johnson County communities such as Overland Park, Roeland Park, Shawnee, and Johnson County as a whole. Figure 1.8 summarizes the data.

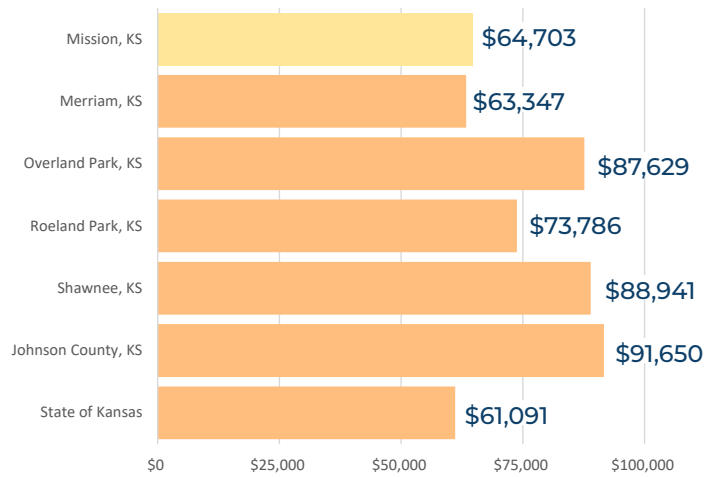


Figure 1.8 - Median Household Income Comparison (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

MISSION'S MEDIAN INCOME IS LOWER THAN THAT OF JOHNSON COUNTY AND MANY OTHER NEARBY COMMUNITIES

Household Income

The most common income bracket is \$75,000 to \$99,999 per year, which accounts for 19.5% of households. Over half of all households earn between \$35,000 to \$99,999. Approximately 24% of households earn over \$100,000 per year and 13.6% of households earn below \$25,000 annually.

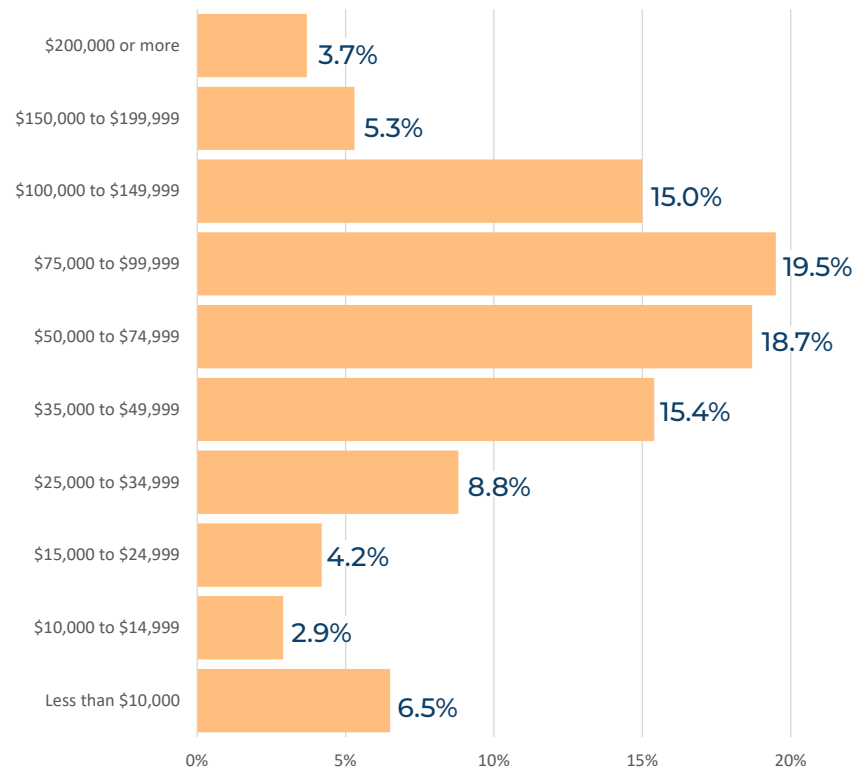


Figure 1.9 - Household Incomes by Bracket (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

OVER HALF OF MISSION HOUSEHOLDS EARN BETWEEN \$35,000 TO \$99,999

1.5 Household Incomes

Household Incomes

Figure 1.10 below shows how the percentage of households by income bracket vary between Mission, the Kansas City metro area, Johnson County, the State of Kansas, and the United States.

Mission exceeds local and national trends in percentage of households in the middle incomes of between \$50k to \$99k. It also has a smaller percentage of households in the highest income brackets, those earning \$100,000 or more per year.

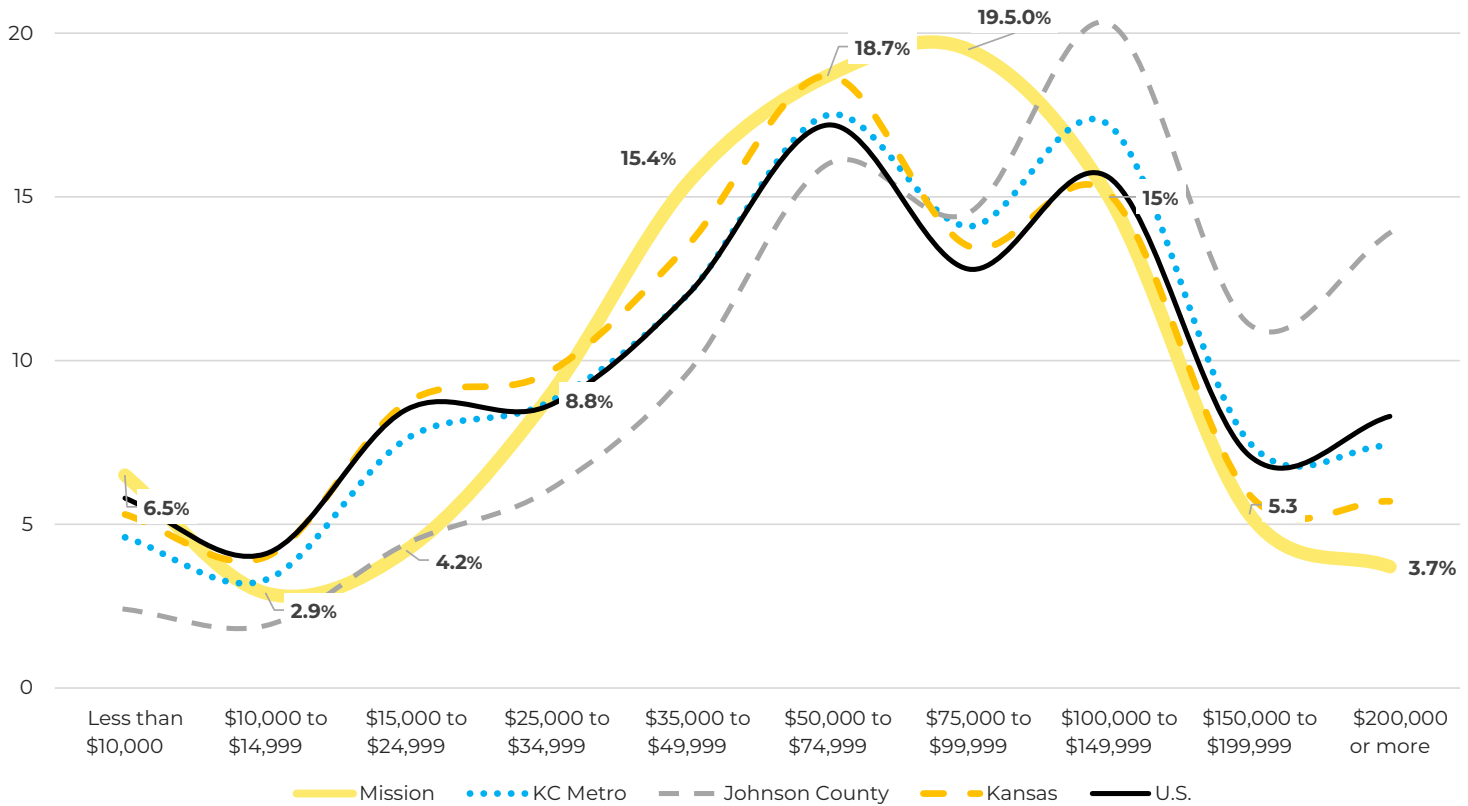


Figure 1.10 - Household Incomes by Bracket Comparison (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

MISSION EXCEEDS THE KC METRO, JOHNSON COUNTY, STATE OF KANSAS AND U.S. IN PERCENTAGE OF HOUSEHOLD EARNING BETWEEN \$35-99K

1.6 Housing

Home Values

In 2020, Mission's median owner-occupied home value was \$203,600. The majority of homes are valued between \$100,000 to \$149,999 (23%), \$150,000 to \$199,999 (28%) or \$200,000 to \$299,999 (29%). There are few homes that are below \$100k or above \$500k in Mission (Figure 1.11).

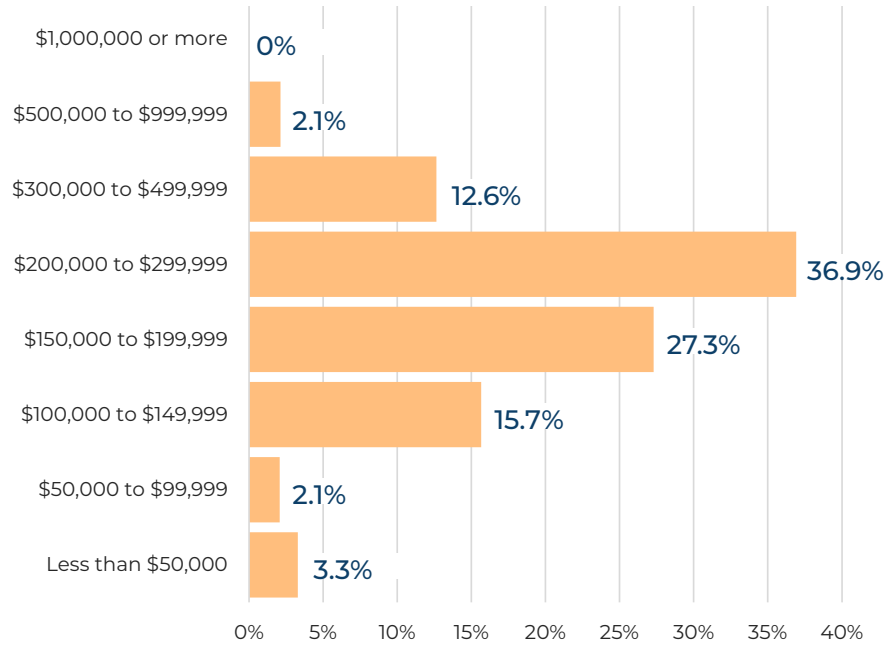


Figure 1.11 - Owner-Occupied Home Values (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

Year Built

The majority of homes in Mission were built in the years following the end of World War II. Fewer than 6% of homes were constructed in 1939 or earlier. Likewise, there are not many homes built since 2010, which is not surprising given that Mission is landlocked and mostly built-out.

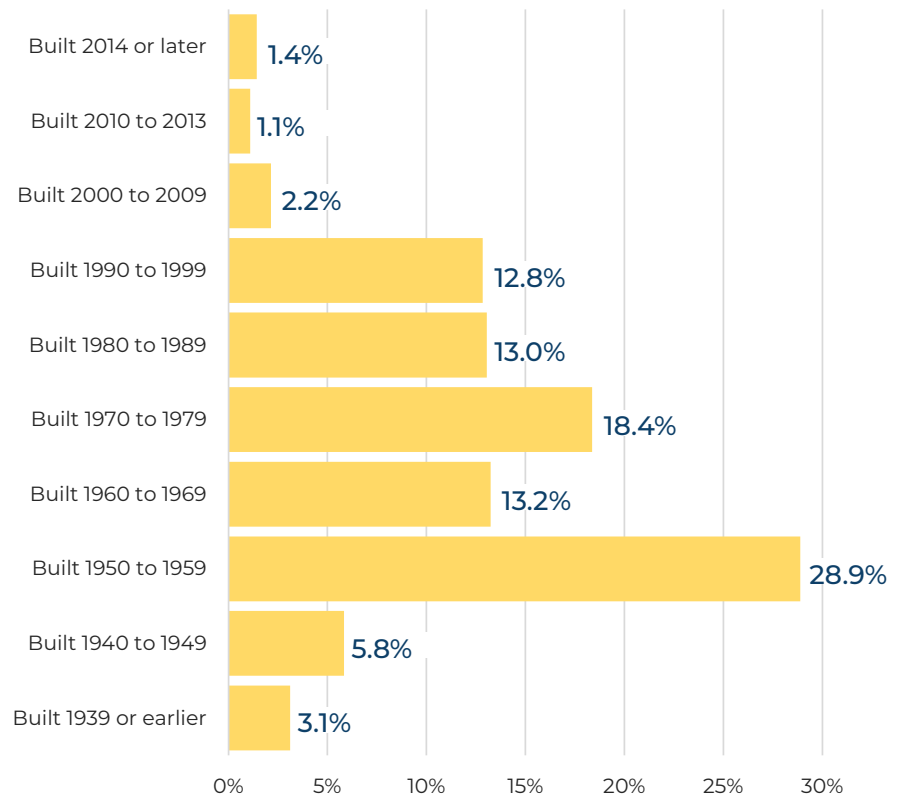


Figure 1.12 - Year Built (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

1.6 Housing

Housing Costs - Homeowners

The 2020 median monthly housing costs for homeowners with a mortgage living in Mission was \$1,391. Figure 1.13 shows the breakdown of housing costs. Just over half of Mission homeowners with a mortgage pay between \$1,000 and \$1,499. Another 27.1% pay between \$1,500 and \$1,999 monthly. Approximately 10.3% pay more than \$2,000 per month.

According to the Department of Housing and Urban Development (HUD), households are considered cost-burdened when monthly housing costs account for 30% or more of household income. Figure 1.14 shows the percentage of household income going towards monthly housing costs of homeowners with a mortgage. Approximately 23% of homeowners with a mortgage are cost burdened by this standard.

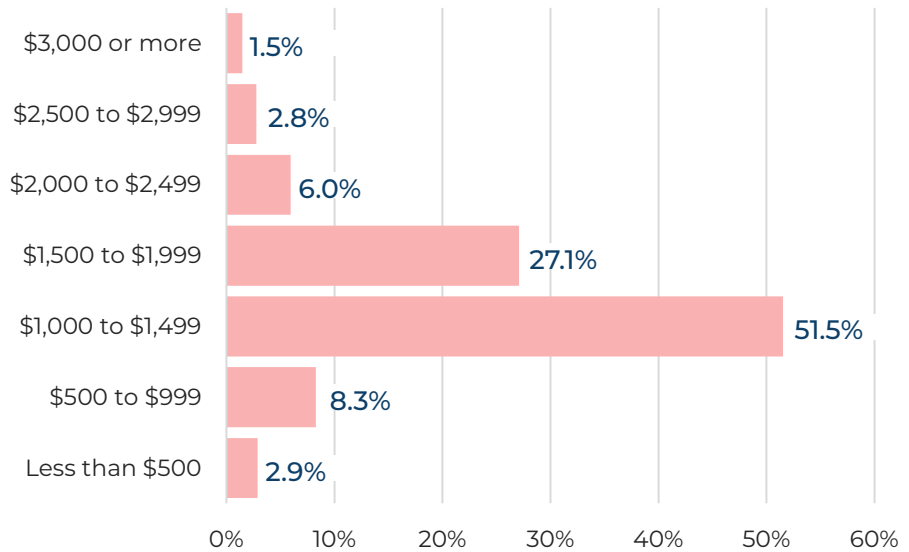


Figure 1.13 - Owner-Occupied Monthly Housing Costs, Units with a Mortgage (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

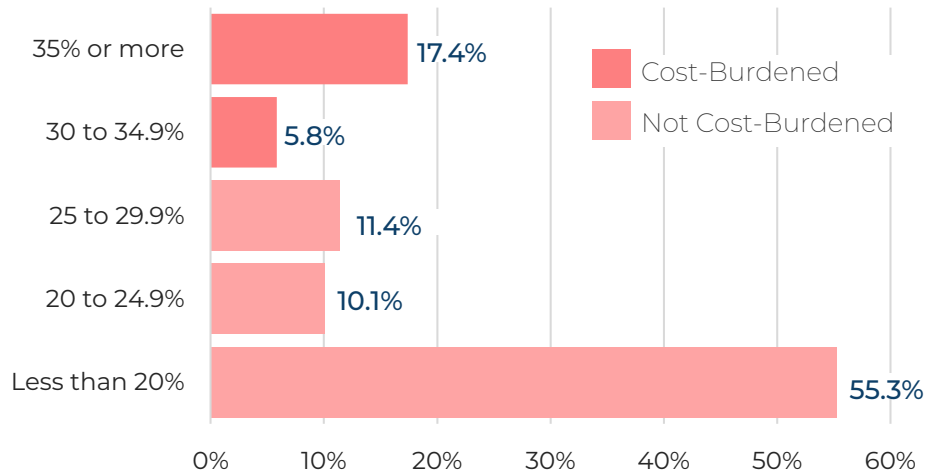


Figure 1.14 - Owner-Occupied Percent of Household Income Spent on Housing (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

AROUND 23% OF MISSION HOMEOWNERS WITH A MORTGAGE ARE CONSIDERED COST-BURDENED

1.6 Housing

Housing Costs - Renters

The median gross rent for Mission renters is \$984 per month. Figure 1.15 shows the number of renting households by how much rent they pay per month. Over half of renters in Mission pay between \$500 to \$999 per month. Another nearly 44% pay between \$1,000 to \$1,999 per month. Less than 2.5% of renters in Mission pay more than \$2,000 per month.

Households are considered cost-burdened when monthly housing costs account for 30% or more of household income. Figure 1.16 shows the percentage of household income going towards monthly rent payments. Approximately 38% of Mission renters are considered cost burdened by this standard.

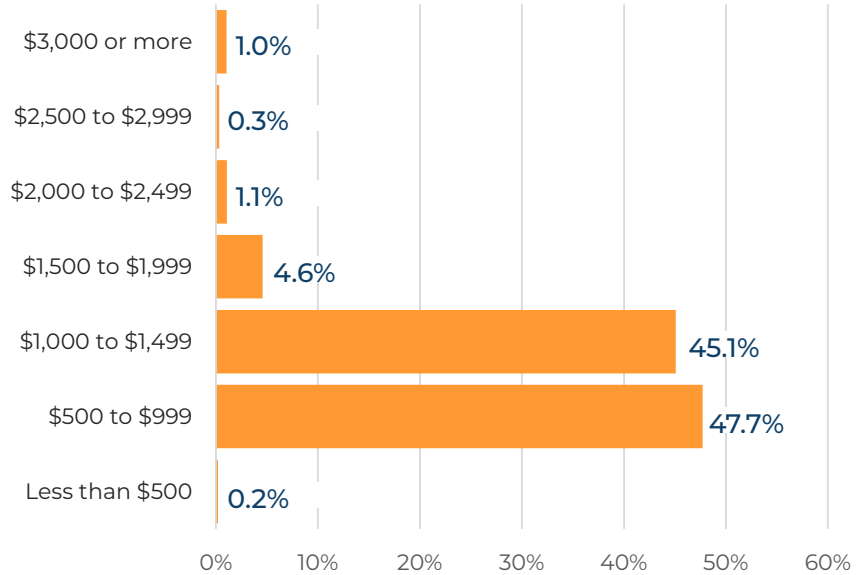


Figure 1.15 - Gross Rent (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

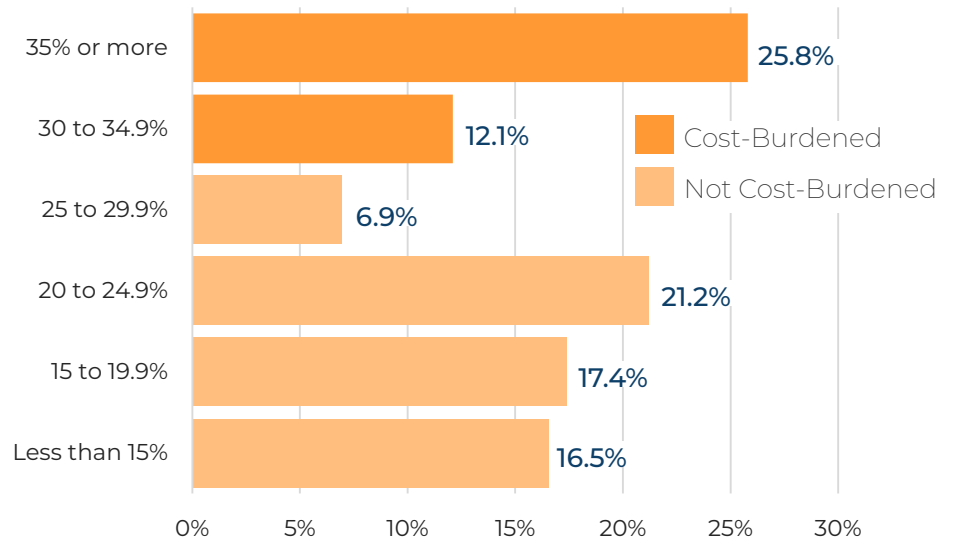


Figure 1.16 - Percentage of Household Income Spent on Rent (2020)

Source: U.S. Census Bureau ACS 5-Year Estimates 2016-2020

NEARLY 38% OF MISSION RENTERS ARE CONSIDERED COST BURDENED

1.6 Housing

Home Sales

Figure 1.17 shows the total number of home sales in Mission between January 2012 and April 2022. A clear cyclical nature of home sales over time is evident from the data, which is in line with national trends (more home sales in summer, fewer home sales in winter).

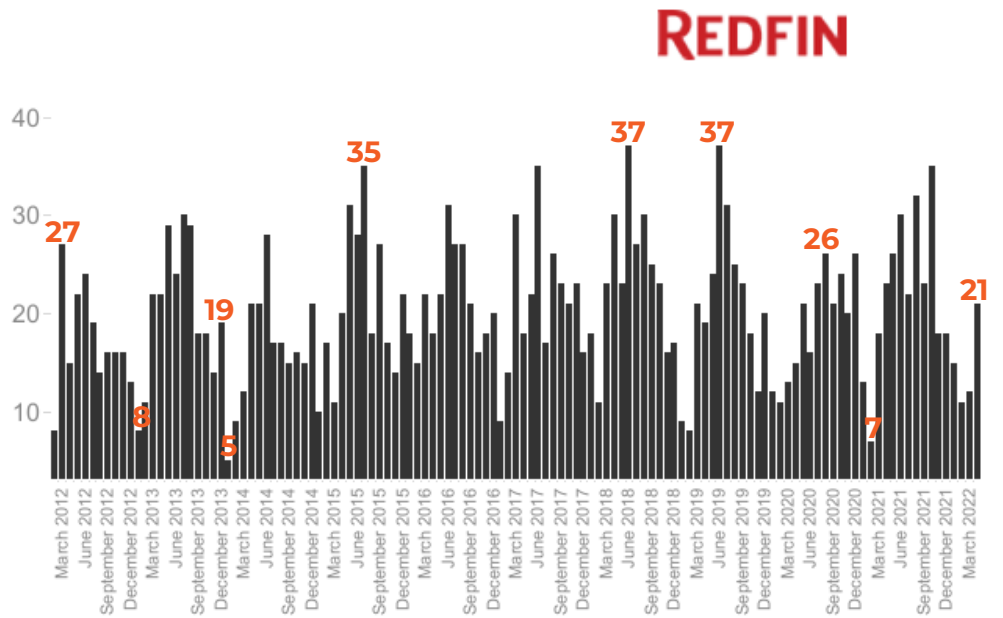


Figure 1.17 - Home Sales in Misison (January 2012 - April 2022)

Source: Redfin (2022)

Active Listings

Figure 1.18 shows the total number of active listings for homes in Mission between January 2012 and April 2022. There has been a steady decline in the number of active listings in Mission, which would indicate a lower inventory than 5 to 10 years ago. Again, this is on par with national trends in the housing market.

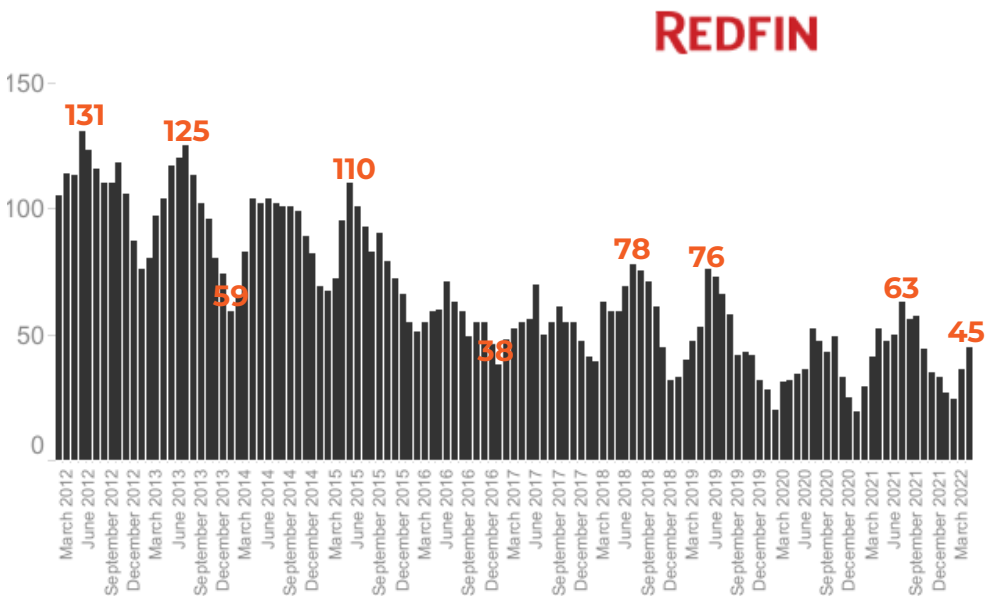


Figure 1.18 - Active Listings in Mission (January 2012 - April 2022)

Source: Redfin (2022)

1.6 Housing

Median Sale Price

Between January 2012 and April 2022, there was a steady increase in median sale price in Mission. Median sale price went from \$116k in 2012 up to \$313k in 2022. This represents over a nearly 170% increase in median sale price in less than ten years.

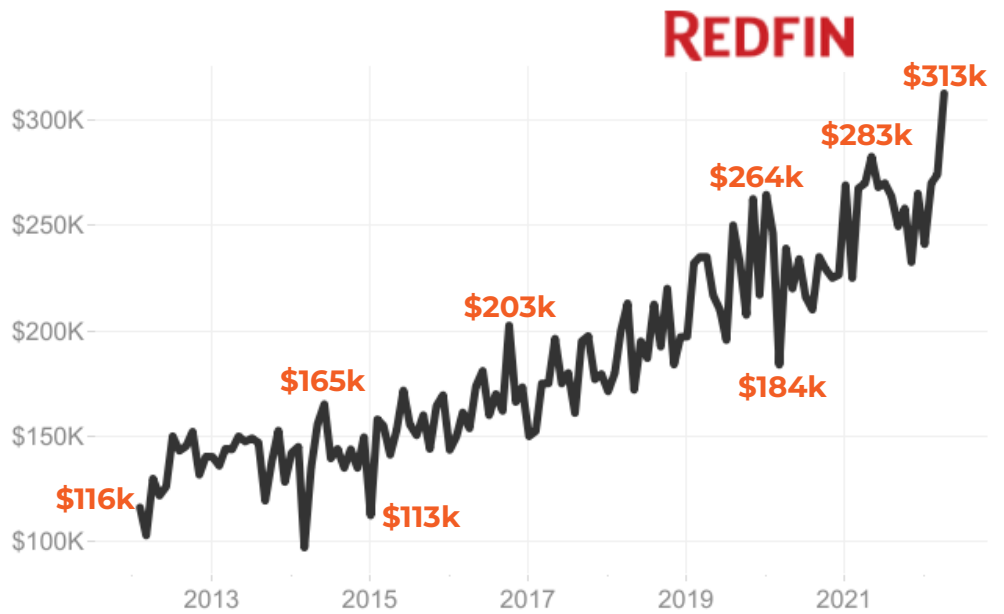


Figure 1.19 - Median Sale Price in Mission (January 2012 - April 2022)

Source: Redfin (2022)

Median Sale Price - Comparison

Figure 1.20 shows the median sale price for homes in Mission, Johnson County, the Kansas City metro area, the State of Kansas between January 2012 and April 2022. While Mission is still below the median sale price for the other geographies, it has followed the overall trend. Johnson County as a whole has a recent median sale price of close to \$400k, highlighting Mission's relative affordability within the affluent Johnson County area despite sale price increases.

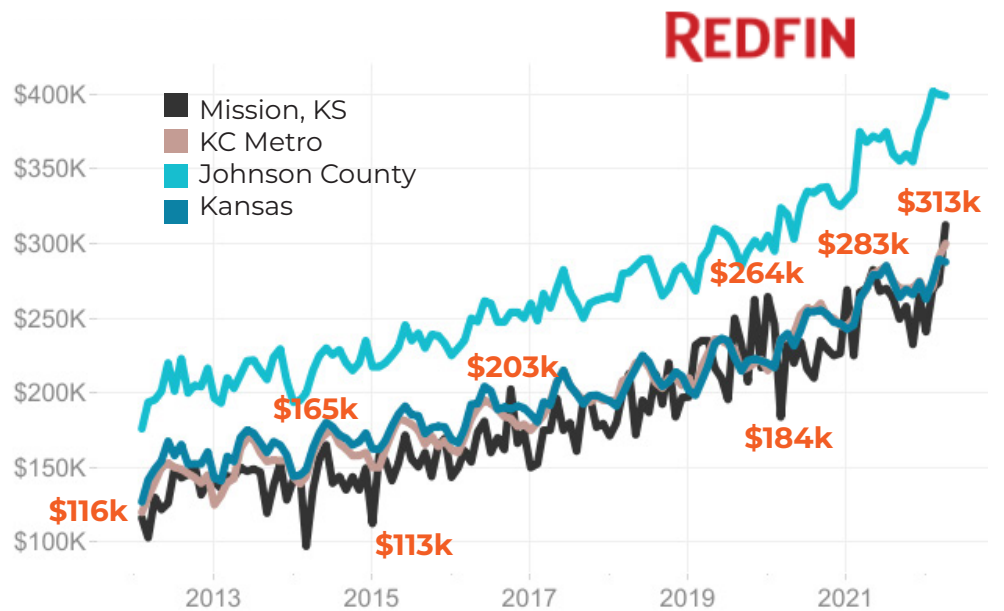


Figure 1.20 - Median Sale Price Comparison (January 2012 - April 2022)

Source: Redfin (2022)

1.6 Housing

Median Days on Market

For data collected between January 2012 and April 2022, Mission had a peak high of 193 days on the market in late 2013. Since this time, the trend has been for the median days on market to drop dramatically. As of April 2022, the median sale price was only 9 days.

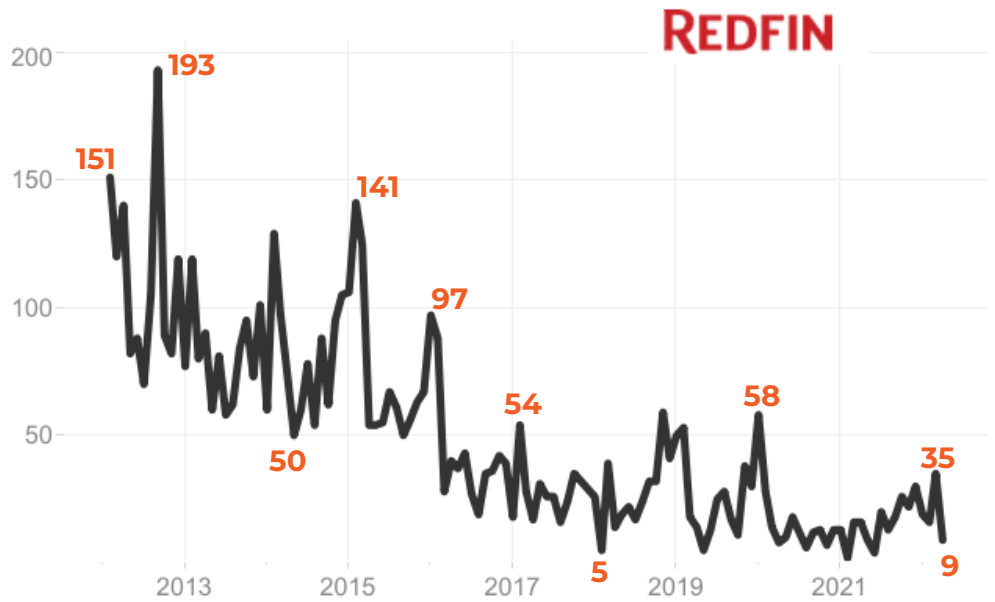


Figure 1.21 - Median Days on Market - Mission (January 2012 - April 2022)
Source: Redfin (2022)

Median Days on Market - Comparison

Figure 1.22 shows the median days on market for homes in Mission, Johnson County, the Kansas City metro area, and the State of Kansas. Once again, Mission has followed regional and state trends of significantly faster sale times than in previous years. In the past few years, Mission has had a lower median number of days on market than the other areas. These results can be attributed to many factors, some of which being the price points of available homes, COVID-19 housing-related trends, and general housing stock competition.

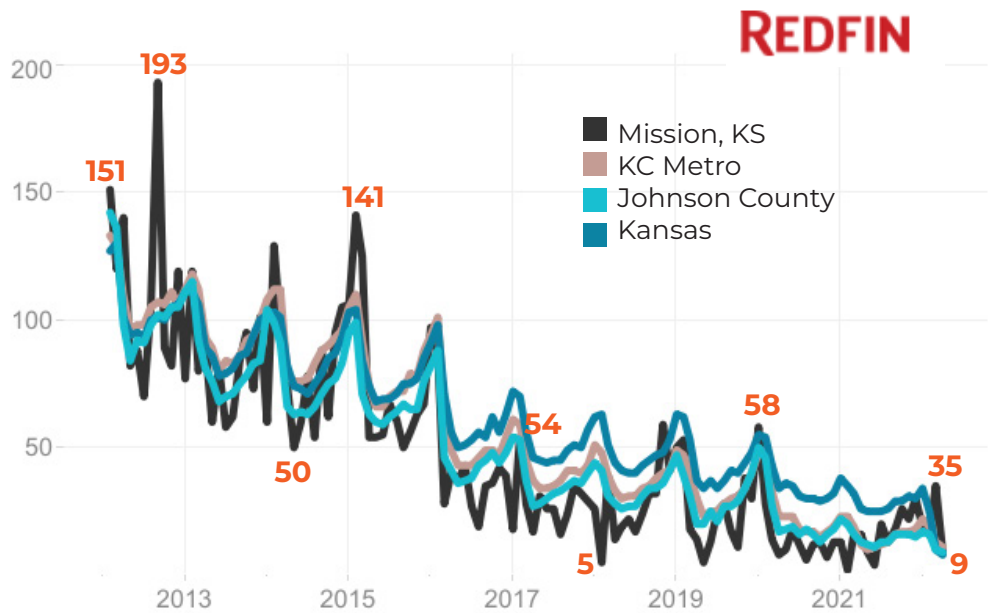


Figure 1.22 - Median Days on Market - Comparison (January 2012 - April 2022)
Source: Redfin (2022)

2.0 Physical Profile

2.1 Housing

Mission Housing

Figure 2.1 shows housing by density and type for Mission. Yellow shows low-density housing such as single-family detached homes. Orange shows residential options such as townhomes, rowhouses, or duplexes. High-Density Residential is shown in brown and includes any apartment buildings or other vertically aligned housing.

The housing analysis on the following pages is taken from Johnson County parcel data. Throughout the existing conditions report there is housing data pulled from both the county tax assessor and the U.S. Census Bureau. These estimates will have some variation in totals but are meant more so to identify trends, issues, and opportunities within the housing stock in Mission.

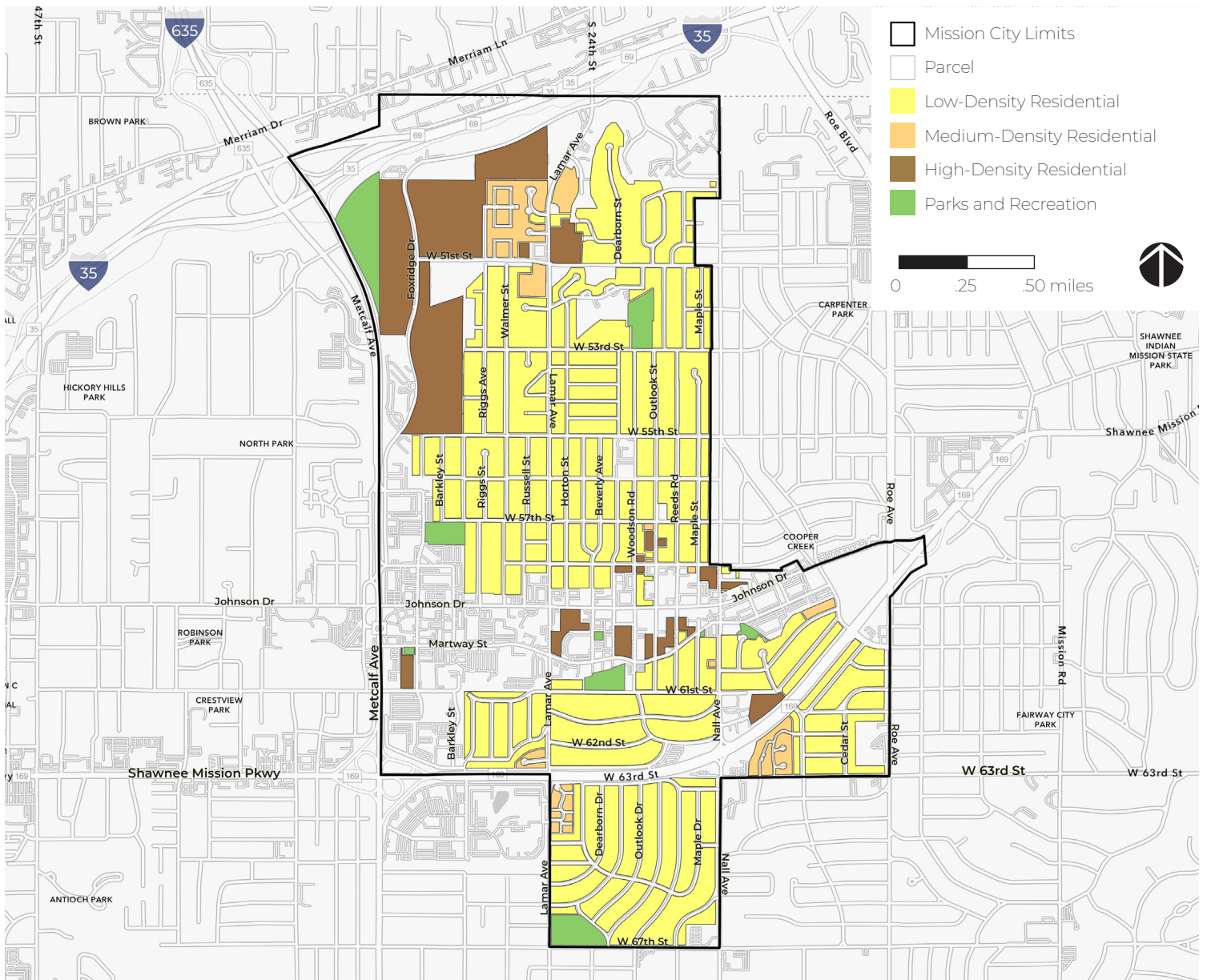


Figure 2.1 - Residential Land

2.1 Housing

Mission Housing Style

The dominant home styles in Mission are ranch and conventional styles (Figure 2.2). Ranch style homes are typically a long, thin home with wide open layouts. Conventional style homes are those that follow more historical appearances with formally defined spaces. These differ from contemporary homes that have open floor plans and non-traditional characteristics.

Mission Housing Year Built

Most of Mission's homes were built in between 1950 and 1959 (Figure 2.3).

Mission Housing Total Valuation

Homes south of Martway Street are typically higher value than the homes found in north and central Mission (Figure 2.4).

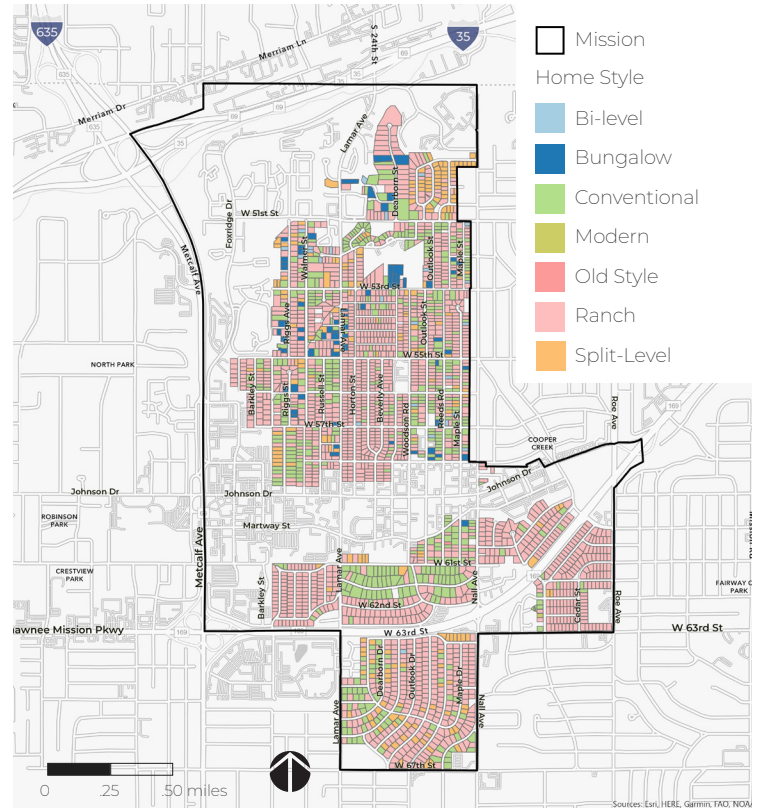


Figure 2.2 - Home Styles (2019)

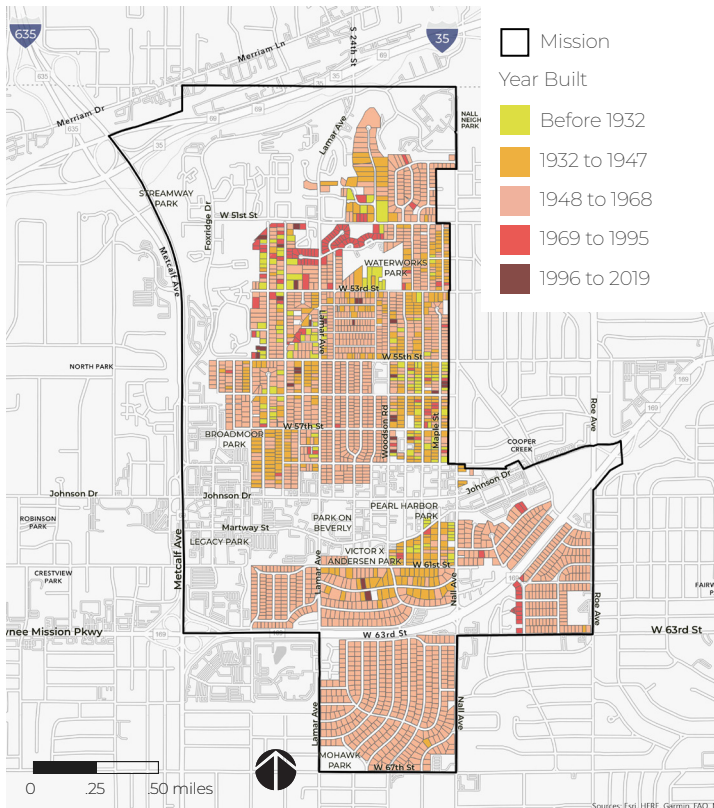


Figure 2.3 - Single-Family Homes Year Built

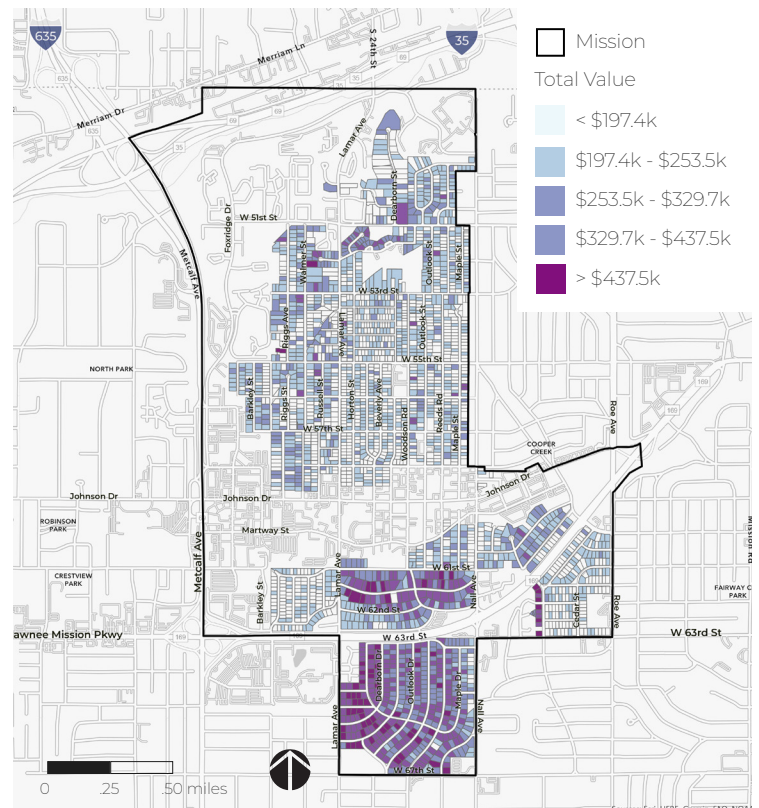


Figure 2.4 - Residential Parcels - Total Valuation

2.1 Housing

Dwelling Units

Table 2.1 shows the number of dwelling units in Mission by housing type according to Johnson County parcel data. There are approximately 3,164 residential parcels in Mission, which include approximately 5,985 dwelling units. While around 80% of the residential parcels in Mission are detached single-family homes, when the number of total dwelling units is considered the distribution of dwelling units is roughly evenly split between detached single-family homes (42.5%, 2,544 dwelling units) and garden style apartments (40.0%, 2,392 dwelling units).

Mission has one 101-unit senior housing development. There is also a nursing home with an adult care facility that has 147 dwelling units.

There are 33 garden style apartment buildings, which mean they are buildings with three stories or less. Combined, these 33 apartment buildings have 2,392 units. This equates to approximately 72 units per building. There is one high-rise apartment building, known as The Locale, with approximately 200 dwelling units.

MOST DWELLING UNITS IN MISSION ARE EITHER DETACHED SINGLE-FAMILY RESIDENTIAL HOMES (42.5%) OR GARDEN APARTMENT UNITS (40.0%)

Housing Type	Parcels	Percent (%)	Dwelling Units	Percent (%)
Duplex/Triplex/Quadraplex	206	6.5%	218	3.6%
Condominium/Apartment Unit	374	11.8%	374	6.2%
Detached Single-Family Residential	2,544	80.4%	2,544	42.5%
Dwelling Converted to Apartment	2	0.1%	8	0.1%
Garden Apartment (3 stories or less)	33	1.0%	2,392	40.0%
High-Rise Apartment	1	0.0%	200	3.3%
Mixed Residential/Commercial	1	0.0%	1	0.0%
Nursing Home with Adult Care Facility	2	0.1%	147	2.5%
Senior Housing	1	0.0%	101	1.7%
TOTAL	3,164	100%	5,985	100%

Table 2.1 - Residential Parcels and Dwelling Units Breakdown

Source: Johnson County Tax Assessor Data, AIMS

2.1 Housing

Low-Density Residential

Low-density residential housing includes all single-family detached homes (Figure 2.5). The lower-density residential areas are divided into three main groupings: those areas north of Johnson Drive but south of Foxridge Dr, those south of Johnson Drive and north of Shawnee Mission Parkway, and those south of Shawnee Mission Parkway.

Much of Mission's single-family housing stock was built after World War II (1945-1959). The homes north of Johnson Drive were constructed on more of a traditional gridded street system. The homes south of Johnson Drive were constructed along more curvilinear roads.

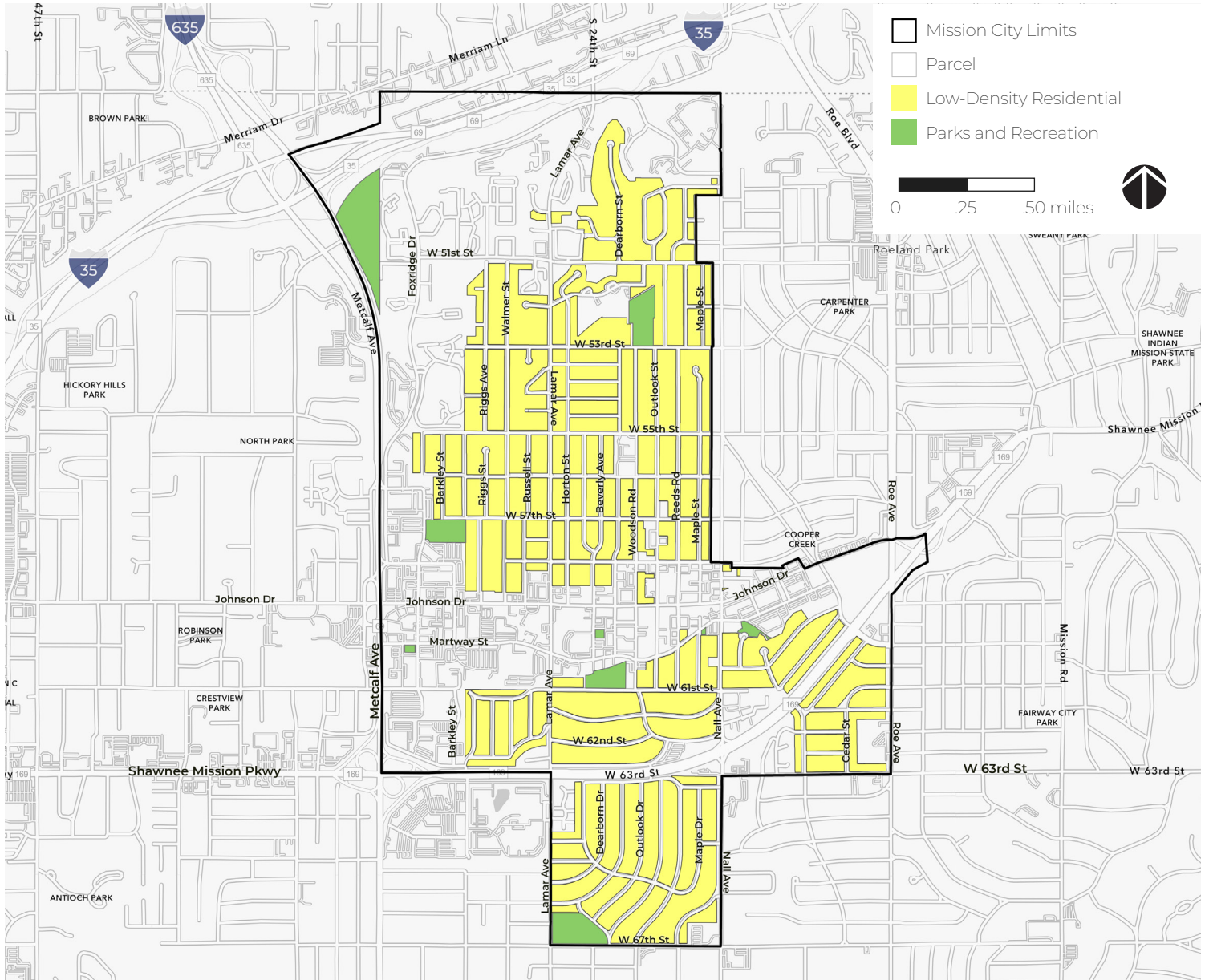


Figure 2.5 - Single-Family Residential Land

2.1 Housing

Overview - Detached Single-Family

According to Johnson County parcel data, there are 2,544 detached single-family residential homes in Mission. The roughly 2,500 homes are spread across 652 acres with an average lot size of 0.26 acres. The smallest lot in Mission for a single-family home is 0.04 acres and the largest is 2.74 acres.

Home Styles

The majority of Mission's single-family homes are ranch style, which represents around 65% of the homes. The second most popular home style is conventional with 20% of homes. The remaining homes are mostly split-level (7.8%), raised ranch (3.5%) or bungalow (2.6%). Of those style types, the one with the highest median home value is conventional with \$283,406. Bungalows are the lowest with \$185,589.

Year Built

Most of Mission's detached single-family homes were built in the years following World War II. Approximately 58% of homes were constructed between 1950 and 1959. Another 11% between 1960 to 1969. Nearly 14% were built in the 1940s. Since 1970, there have been approximately 156 homes constructed within Mission. Table 2.2 shows the year built breakdown along with the median value of the age categories. The newest (2010-2019) homes have the highest median value (\$410,968).

Home Values

The average value of a Mission detached single-family home is \$236,490. The lowest value home is \$88,200 and the highest value home is valued at \$843,900. The two value ranges with the most homes in Mission are between \$150,000 to \$199,999 (36.2%) and between \$200,000 and \$249,999 (29.8%). Less than 5% of homes are valued more than \$400,000. Likewise, less than 5% are valued below \$150,000.

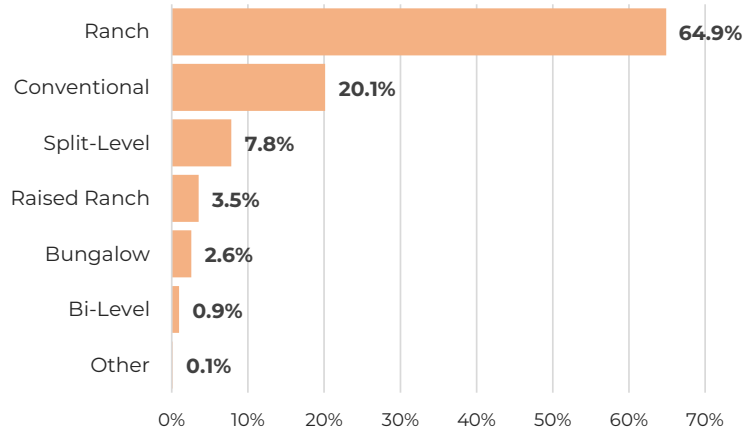


Figure 2.6 - Single-Family Home Styles

Year Built	Count	Percent (%)	Median Value
Pre-1900	4	0.2%	\$265,500
1900 - 1929	84	3.3%	\$187,724
1930 - 1939	183	7.2%	\$212,790
1940 - 1949	354	13.9%	\$226,460
1950 - 1959	1,483	58.3%	\$245,664
1960 - 1969	280	11.0%	\$330,719
1970 - 1979	41	1.6%	\$270,961
1980 - 1989	64	2.5%	\$295,945
1990 - 1999	27	1.1%	\$291,022
2000 - 2009	10	0.4%	\$319,920
2010-2019	14	0.6%	\$410,968
TOTAL	2,544	100%	\$236,490

Table 2.2 - Home Built Year and Median Value

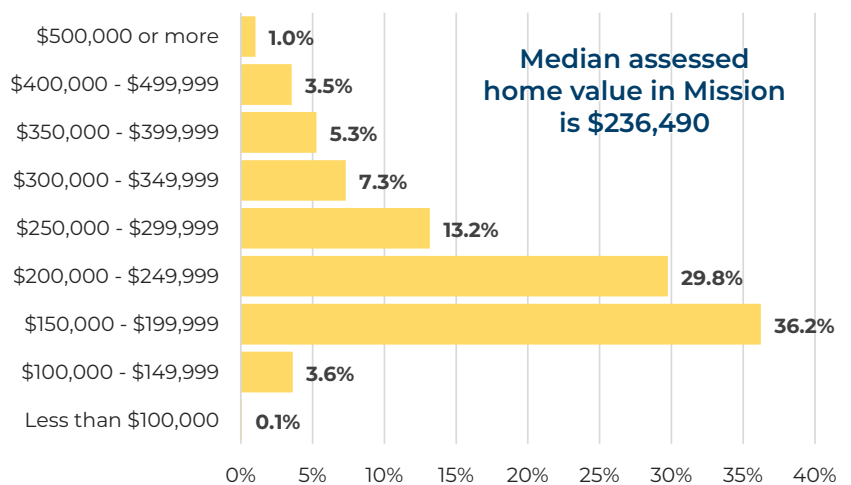


Figure 2.7 - Assessor Valuations for Single-Family Homes

2.1 Housing

Multi-Family Housing in Mission

Multi-family residential land includes everything from duplexes to high-rise apartments (Figure 2.8). In total, there are approximately 620 parcels with 3,441 dwelling units (Table 2.3). Most of the multi-family housing is either in northwest Mission or along the Johnson Drive corridor, which is a growing spot for multi-family housing in the community.

Housing Type	Parcels	Percent (%)	Dwelling Units	Percent (%)
Duplex/Triplex/Quadraplex	206	6.5%	218	6.3%
Condominium/Apartment Unit	374	11.8%	374	0.1%
Dwelling Converted to Apartment	2	0.1%	8	10.9%
Garden Apartment (3 stories or less)	33	1.0%	2,392	40.0%
High-Rise Apartment	1	0.0%	200	3.3%
Mixed Residential/Commercial	1	0.0%	1	0.0%
Nursing Home with Adult Care Facility	2	0.1%	147	2.5%
Senior Housing	1	0.0%	101	1.7%
TOTAL	620	100%	3,441	100%

Table 2.3 - Residential Parcels and Dwelling Units Breakdown

Source: Johnson County Tax Assessor Data, AIMS

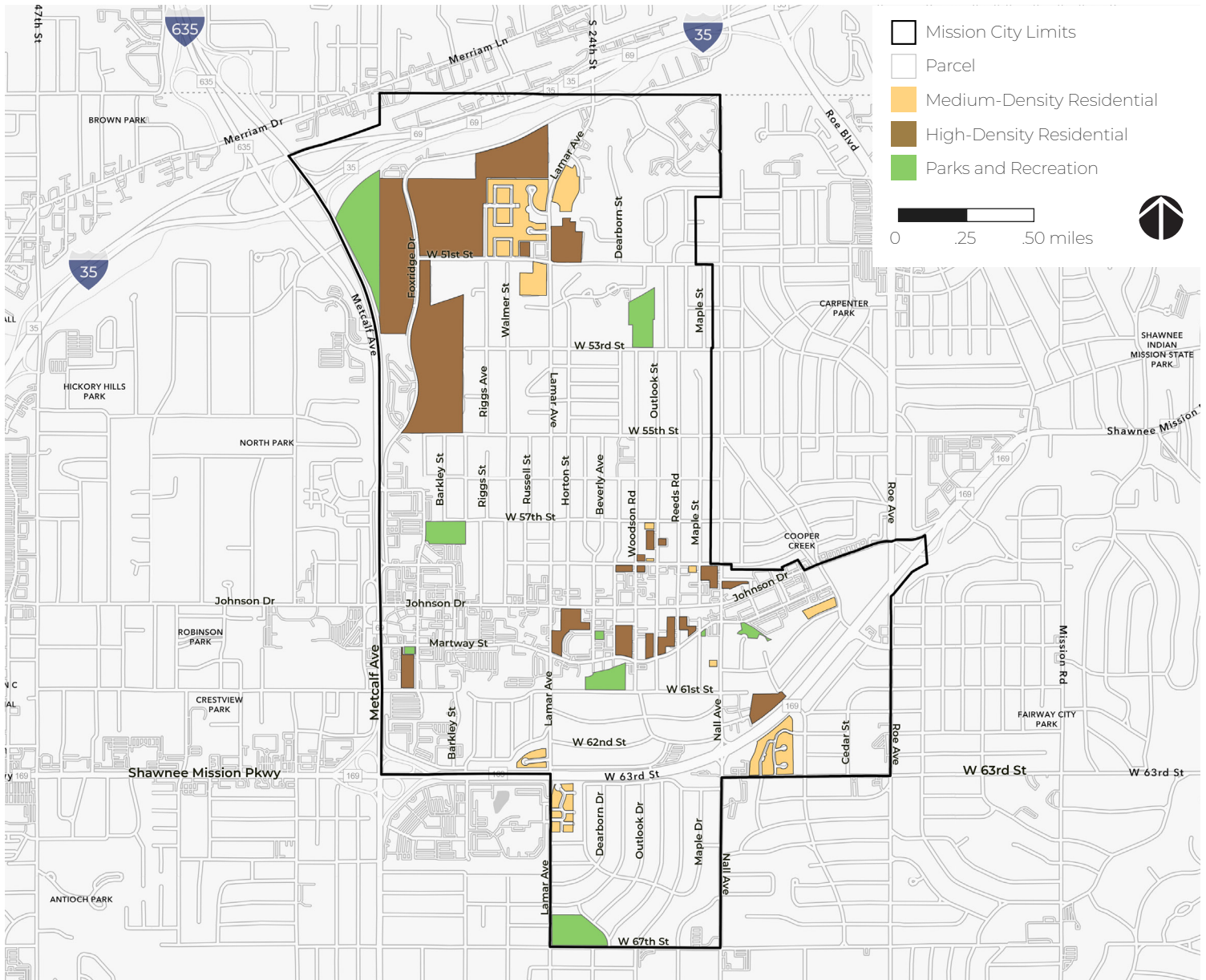


Figure 2.8 - Multi-Family Residential Land

2.2 Existing Land Use

Existing Land Use in Mission

Mission's existing land uses were divided into the following categories based on use or occupancy by parcel (Figure 2.9).

Low-Density Residential
Includes single-family detached homes.

Medium-Density Residential
Includes duplexes or townhomes.

High-Density Residential
Includes apartments or condos.

City/Public/Semi-Public
Includes city or other government-owned land, schools, and churches.

Parks and Recreation
Includes parks and pathway areas.

Open Space/Undeveloped
Includes all undeveloped or vacant land that is not a park.

Commercial
Includes retail, restaurants, and other service/commercial uses.

Office
Includes office and medical uses.

Light Industrial/Warehouse
Includes all industrial, light industrial, and warehouse uses.

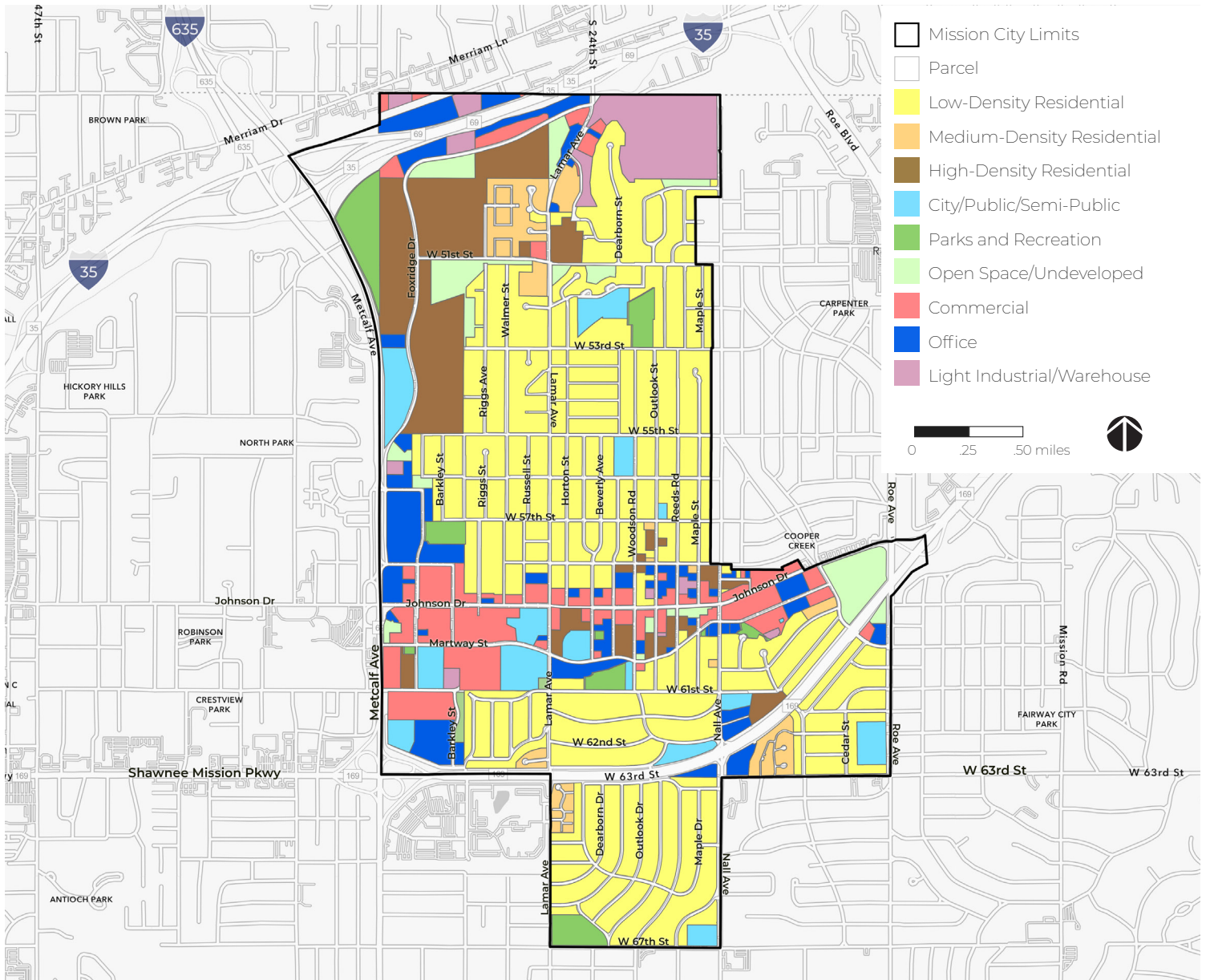


Figure 2.9 - Existing Land Use

2.2 Existing Land Use

Existing Land Use in Mission

The most dominant existing land use in Mission is low-density residential, which accounts for just over half of the community. This is not surprising for a suburban community. Collectively, close to 15% of the land use is multi-family (medium or high density). There is a mix of commercial (8.8%), office (7.8%), and light industrial (5.0%) of land uses, which is typically viewed as employment land. Just over 4% of land remains open space or undeveloped, which includes the vacant West Gateway site.

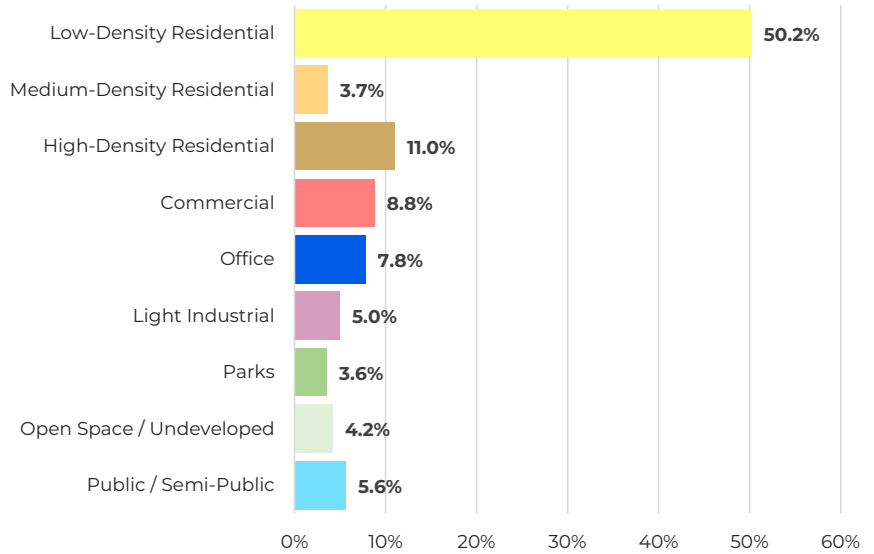


Figure 2.10 - Existing Land Use Percentage

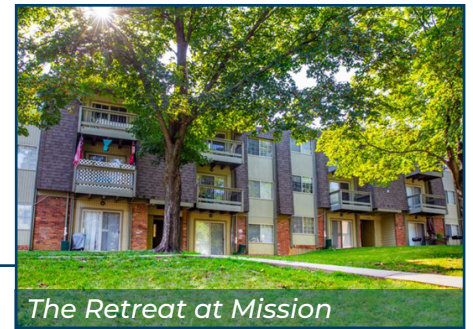
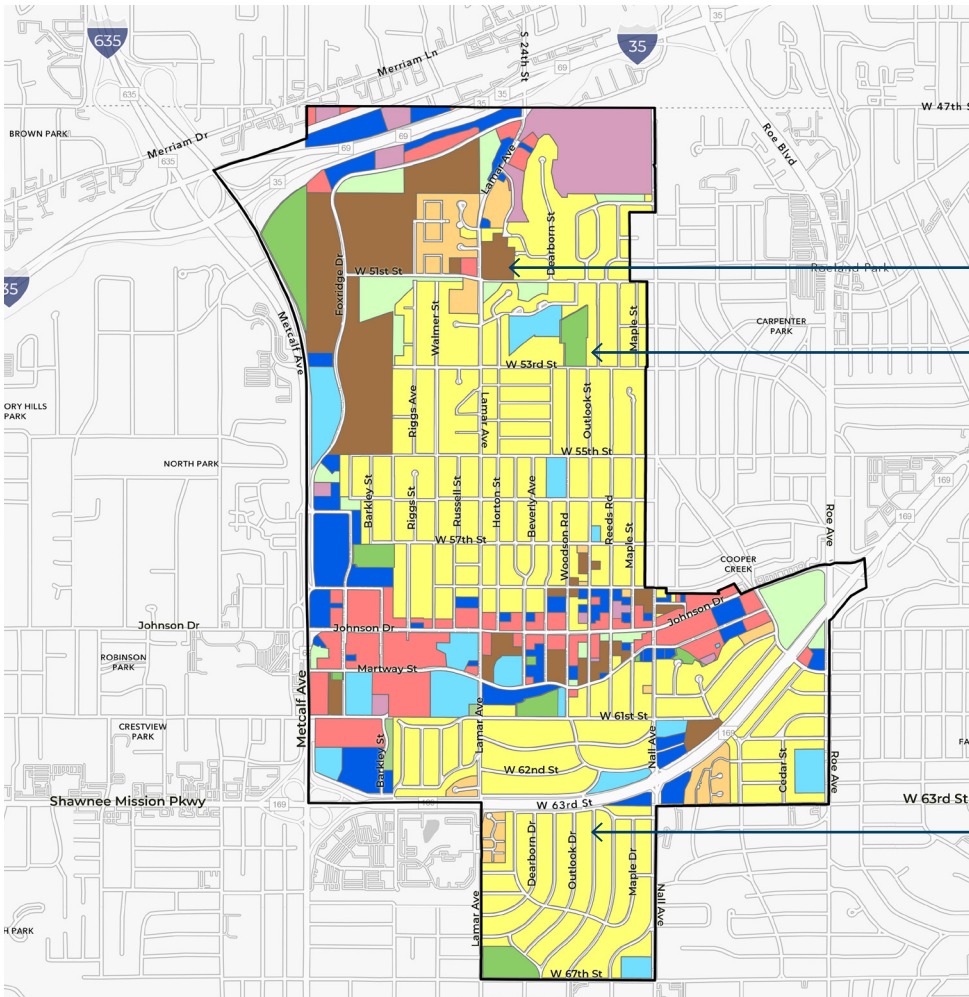


Figure 2.11 - Existing Land Use

2.2 Existing Land Use

Office, Commercial, and Industrial Land Overview

Office, commercial, and industrial lands are typically viewed as tax positives that help fund community needs and improvements. Most of the office, commercial, and industrial land in Mission is either along the Johnson Drive corridor or in the north end of the community along Lamar and Foxridge Drive. These two locations will likely continue

to serve as Mission's main office, commercial, and industrial areas because of a lack of undeveloped land and the overall character of other areas in the community. Mixed-use redevelopment of sites along either location could help expand the number of businesses presence in Mission.

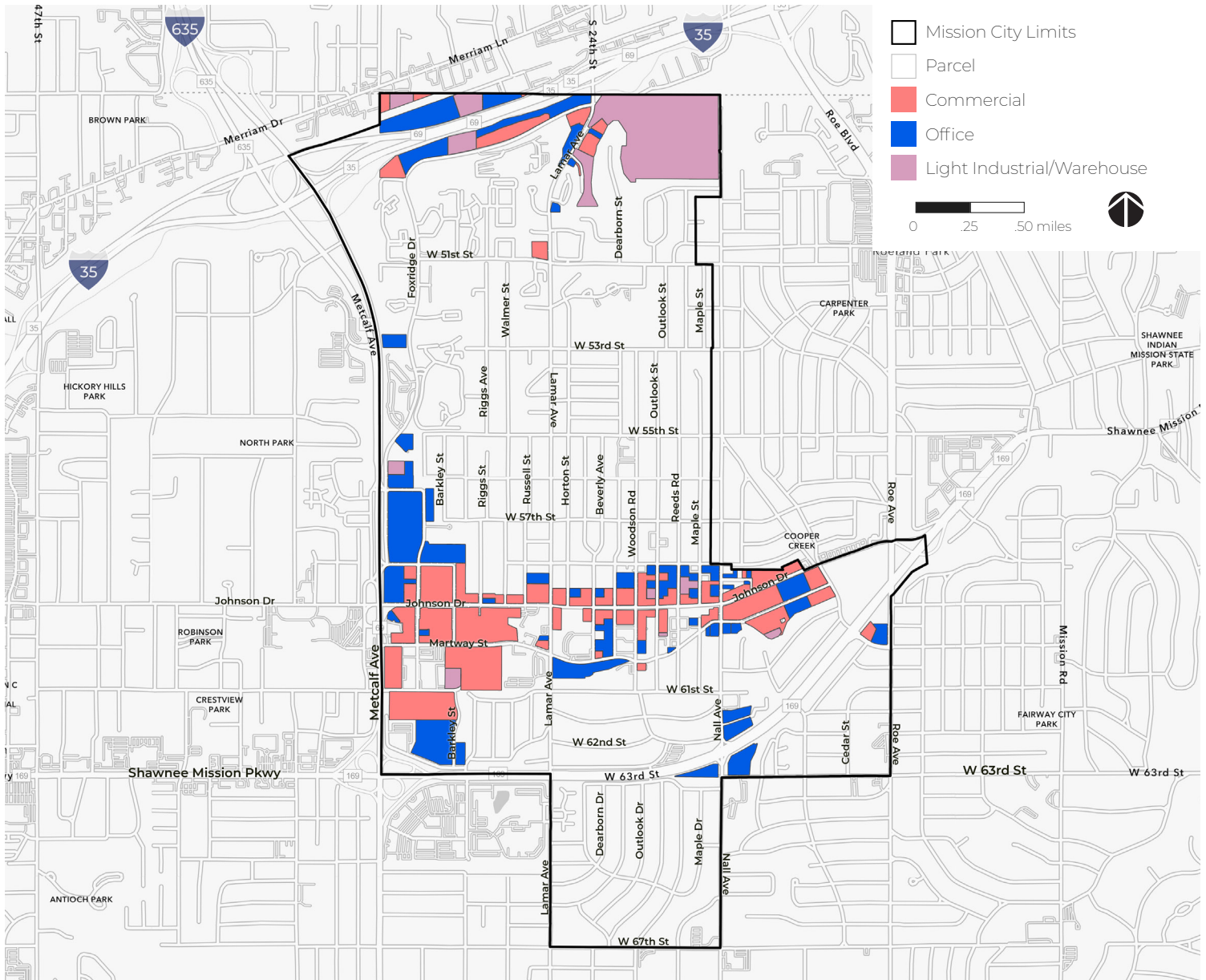


Figure 2.12 - Office, Commercial, and Industrial Land

2.2 Existing Land Use

Parks, Public, Open, and Undeveloped Land Overview

Parks and recreation, open space, undeveloped, and public/semi-public land is spread throughout the entire community. However, there is a cluster of city and public/semi-public land near the civic campus in the Johnson Drive corridor.

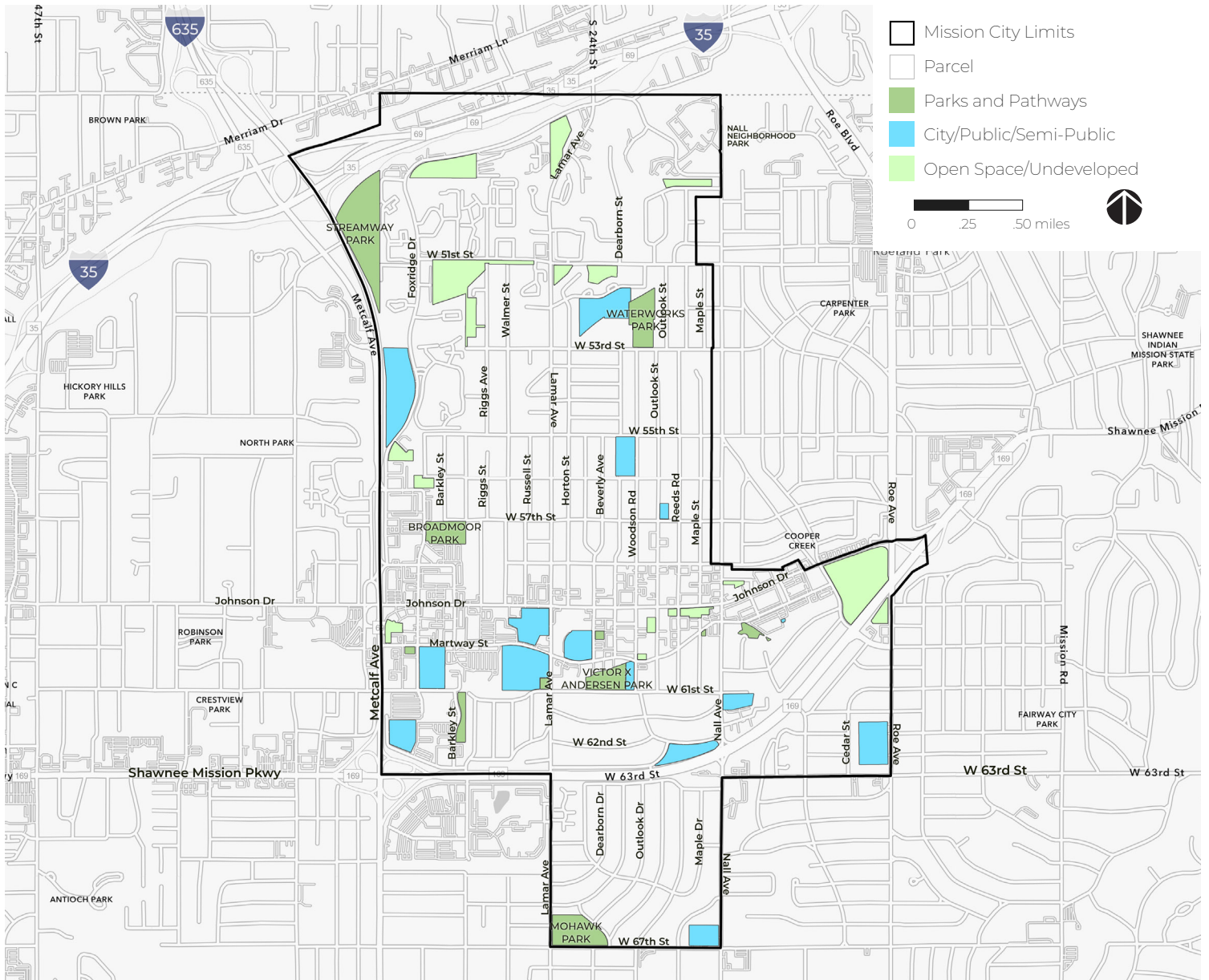


Figure 2.13 - Parks, Public/Semi-Public, Open Space, and Undeveloped Land

2.3 Parks and Recreation

Park Amenities



Andersen Park

- 5901 W. 61st Street
- Restrooms (summer only)
 - Picnic shelters (2)
 - Playground
 - Tennis courts (2)
 - Water fountains
 - Picnic tables
 - Barbeque grill



Mohawk Park

- 67th and Lamar
- Playground
 - 4-miles of walking trails
 - Soccer fields (2)
 - Picnic tables



Streamway Park

- 51st and Foxridge
- Picnic shelter
 - 4-miles of walking trails
 - Picnic tables



Broadmoor Park

- 5701 Broadmoor
- Restrooms (summer only)
 - Picnic shelter
 - Playground
 - Baseball diamond
 - Soccer field
 - 3-miles of walking trails
 - Open-use green space
 - Picnic tables
 - Barbeque grills



Waterworks Park

- 53rd and Woodson
- Picnic shelter
 - Playground
 - 3-miles of walking trails
 - Open-use green space
 - Picnic tables
 - Barbeque grill



Park on Beverly

- 5935 Beverly
- Pergola/shade structure



Legacy Park

- 6000 Broadmoor
- Pergola/shade structure
 - Picnic tables



Pearl Harbor Park

- Martway and Maple
- Pergola/shade structure

2.3 Parks and Recreation

Level of Service Analysis

In total, there are approximately 24.25 acres of parkland in Mission. Table 2.4 shows the acre totals by park.

The National Recreation and Park Association (NRPA) level of service standards suggest a community should aim to provide around 9.9 acres of park for every 1,000 residents. Based on Mission's 2020 population, the community currently falls below this level of service recommendation (Table 2.5). However, Mission has over 80 acres of parks in nearby communities (Table 2.6) that help to provide additional park services. When these additional acres are accounted for in the level of service analysis Mission residents exceed the recommendation.

Park	Acres
Andersen Park	2
Broadmoor Park	5
Mohawk Park	8
Legacy Park	0.5
Park on Beverly	0.5
Pearl Harbor Park	0.25
Streamway Park	5
Waterworks Park	3
TOTAL	<i>24.25 acres</i>

Table 2.4 - Existing Park Acres

Level of Service Analysis	Total
Existing Park Acres	24.25
Existing Population (2020)	9,961
Level of Service Total Park Acres/1,000 residents	2.45
TOTAL	<i>24.25 acres</i>

Table 2.5 - Existing Park Level of Service (LOS) Analysis

Nearby Parks	Acres
North Park	5.9
Hickory Hills Park	10.5
Robinson Park	1.1
Crestview Park	1.9
Antioch Park	43.6
Fairway City Park	3.0
R Park	5.4
Cooper Creek	1.6
Nall Neighborhood Park	10.5
TOTAL	<i>83.5 acres</i>

Table 2.6 - Nearby Park Acres

MISSION ALONE DOES NOT MEET THE LEVEL OF SERVICE RECOMMENDED FOR TOTAL PARK ACRES BASED ON ITS 2020 POPULATION

HOWEVER, THERE ARE OVER 80 ACRES OF ADDITIONAL PARK SPACE NEARBY IN NEIGHBORING COMMUNITIES TO HELP FILL THE LEVEL OF SERVICE GAPS

2.3 Parks and Recreation

Park Walk Time Analysis

A walk time analysis was completed for Mission parks. The darker blue shows those areas of Mission that are within a 5-minute walk time of a park. The lighter blue shows the 10-minute service area and the yellow a 15-minute service area (Figure 2.15). All three are considered reasonable walk times for parkland.

As shown below, there are only a few isolated spots within Mission that are not served by city-owned parks. However, there are other community's parks that are very close to the Mission city limits. The areas of Mission not within the walk time service areas are very near to other community parks which are completing the overall level of service.

Emphasis should be placed on creating a safe and welcoming trail or sidewalk to get to each park from neighborhoods in Mission.

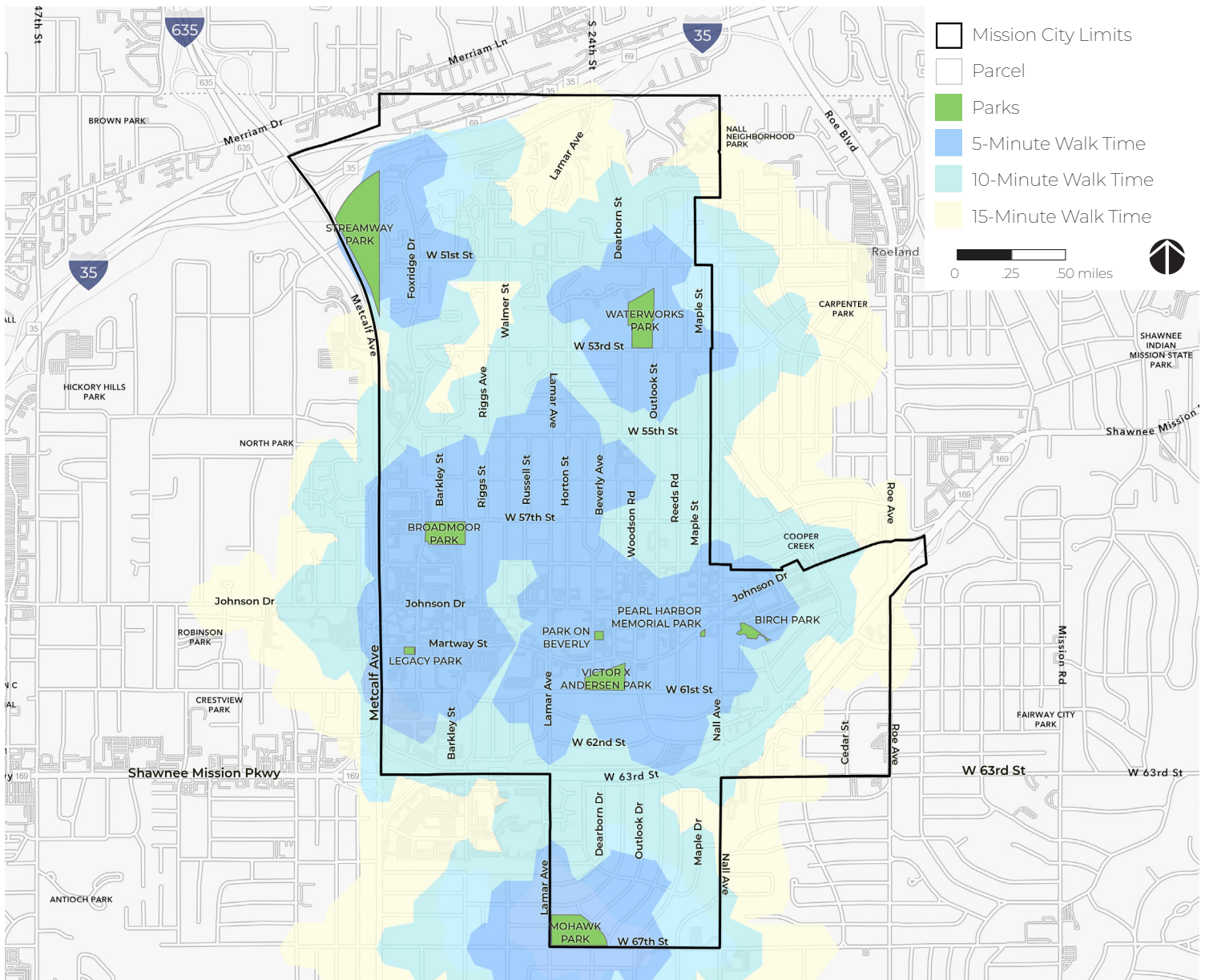


Figure 2.15 - Park Walk Time Analysis

2.3 Parks and Recreation

Trails and Bike Facilities Overview

The five main trail and bikeway types found in the Mission area include:

- Bike Lane
- Marked Shared Road
- Pedestrian Hike Trail
- Shared Use Path
- Marked Bike Route

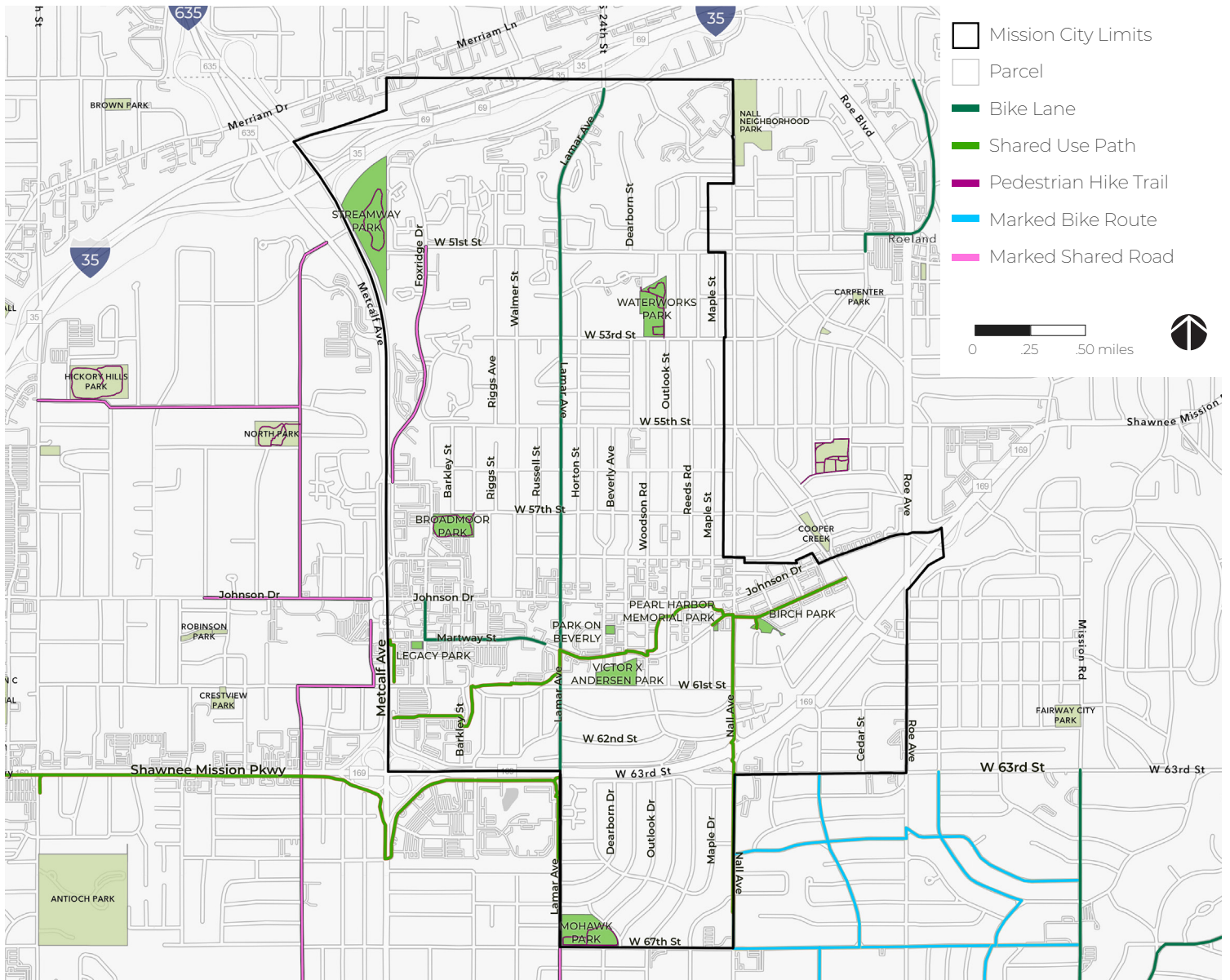


Figure 2.16 - Existing Trails and Bike Facilities

2.4 Street System

Street System Overview

Mission's street system is made up of local, collector, and arterial roadways.

Local Street

Local streets (shown in blue in Figure 2.17) are primarily used to gain access to properties often in residential areas. Local roads provide limited mobility and are typically low speed with limits between 20 and 30 miles/hour.

Collector Street

Collector streets (shown in yellow) are major and minor roads that connect local roads with arterials. Collectors have lower speeds and shorter distances than arterials with speed limits between 35 and 55 miles/hour.

Arterial Streets

Arterial streets (shown in orange) are high-capacity urban roads whose function is to deliver traffic from

collector roads to major arterials (interstates or freeways).

Major Arterial Streets

Major Arterials (shown in red) are the highest classification of roadways. They provide the highest mobility and speeds (55 to 75 miles/hour) with limited access points.

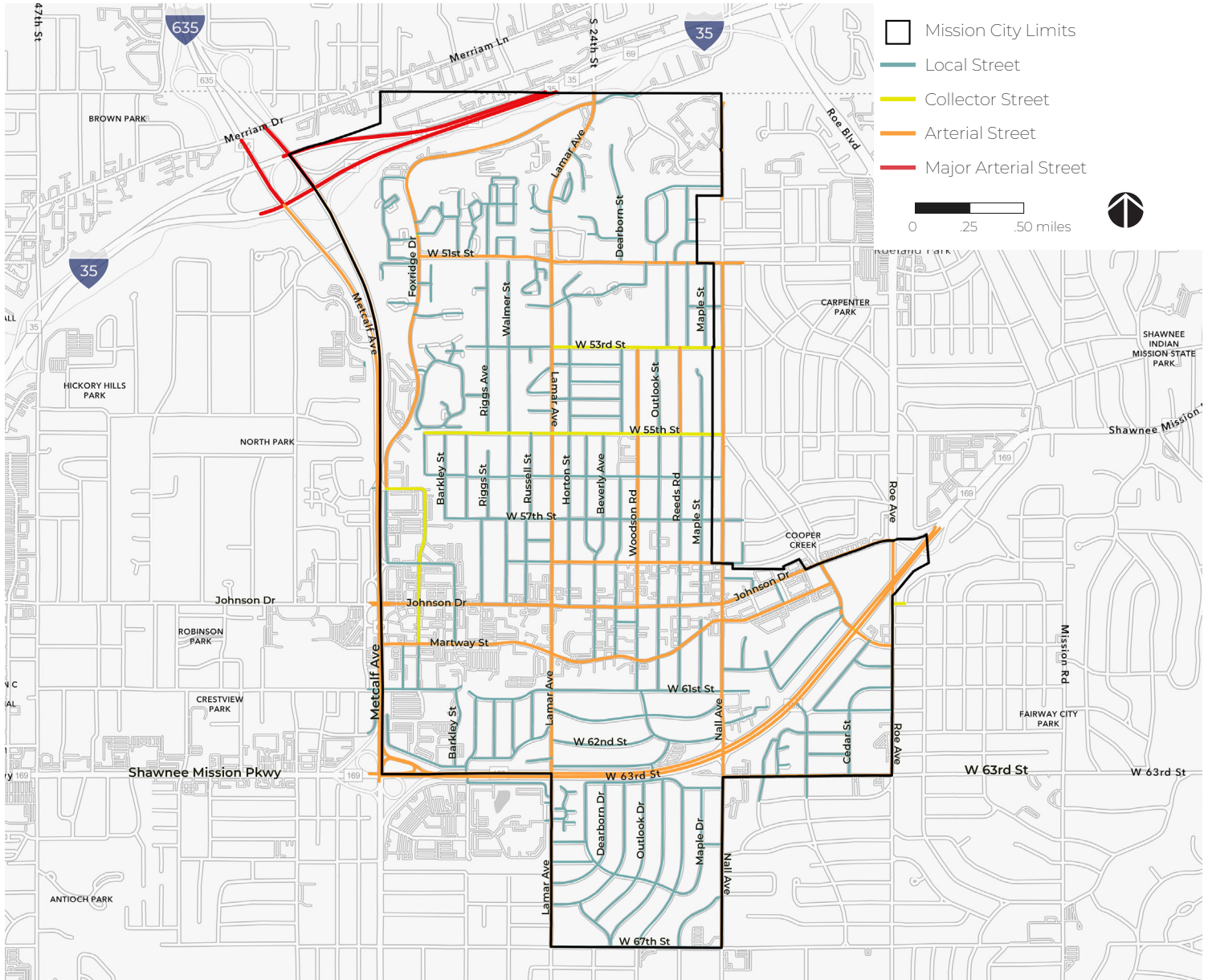


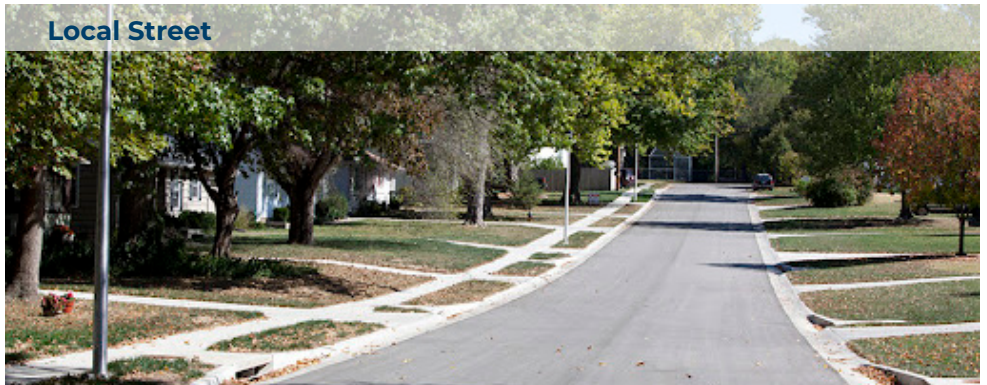
Figure 2.17 - Street System

2.4 Street System

Street Typologies in Mission

Local Streets

Mission is mostly composed of local streets. Local streets connect residents and visitors to collector roads such as Lamar Avenue. Examples of local streets include most low-density residential facing streets such as W 57th Street, Maple Street, or Dearborn Street.



Collector Streets

The main collector roads in Mission include W 51st Street, Lamar Avenue, Martway Street, Metcalf Avenue, Woodson Road, and Foxridge Drive. These collector streets connect neighborhoods to the main arterials in the community including Johnson Drive or Shawnee Mission Parkway.



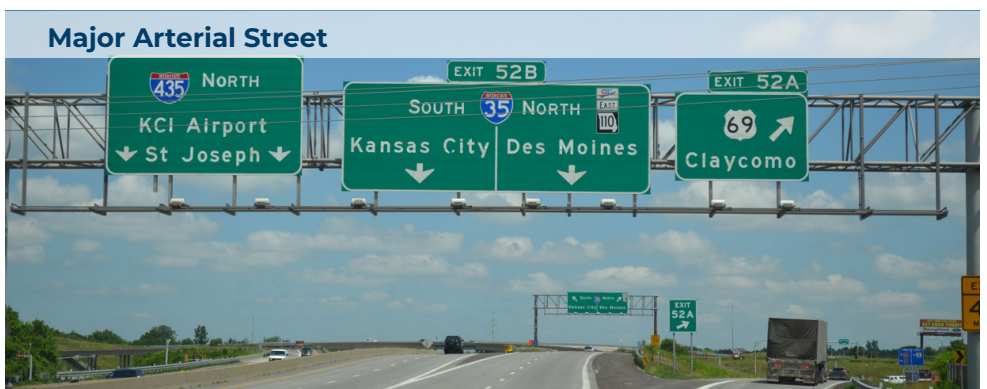
Arterial Streets

The arterial streets in Mission include Shawnee Mission Parkway, Johnson Drive, W 55th Street, Nall Avenue, and Metcalf Avenue. These are the main thoroughfares through the community and serve both local traffic as well as through traffic.



Major Arterial Streets

The only major arterial street in Mission includes the portions of Interstate 35 that run through the northern border of the community.



2.4 Street System

Posted Speed Limits

A majority of Mission's roads are 20 or 25 miles per hour (Figure 2.18). These speed limits mostly correspond to the location of local roads. Collector roads in Mission typically have speed limits of 30 to 35 miles per hour including Foxridge Drive, Martway Street, and portions of Johnson Drive.

Avenue and Shawnee Mission Parkway. The street with the highest posted speed is the short stretch of Interstate 35 near the northern city limits of Mission.

Streets with higher speeds of 45 miles per hour or faster include Metcalf

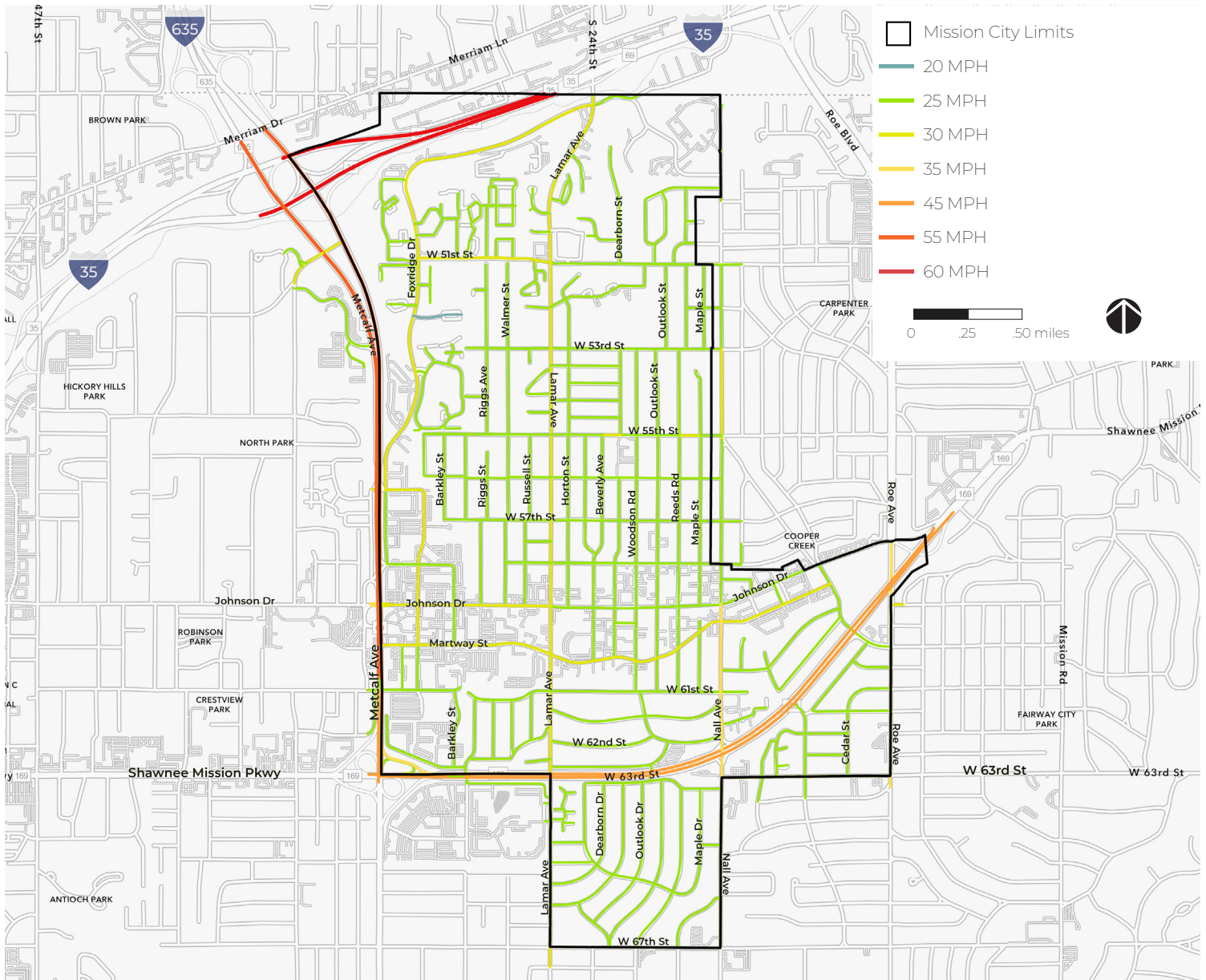


Figure 2.18 - Posted Speed Limits

2.4 Street System

Vehicles Per Day

Data is available that shows the average number of vehicles per day for roads in Mission (Figure 2.19). Roads shown in orange have the lowest number of vehicles per day of less than 5,000. This mostly is local neighborhood streets. Roads shown in green have between 5,001 and 10,000 vehicles per day and include Lamar Avenue and portions of Martway Street, Broadmoor Street, and Nall Avenue.

Johnson Drive has an average number of between 10,001 and 25,000 vehicles per day. Metcalf Avenue and Shawnee Mission Parkway both are higher volume streets with between 25,001 and 45,000 vehicles per day. Interstate 35 has the most vehicles per day with more than 45,000.

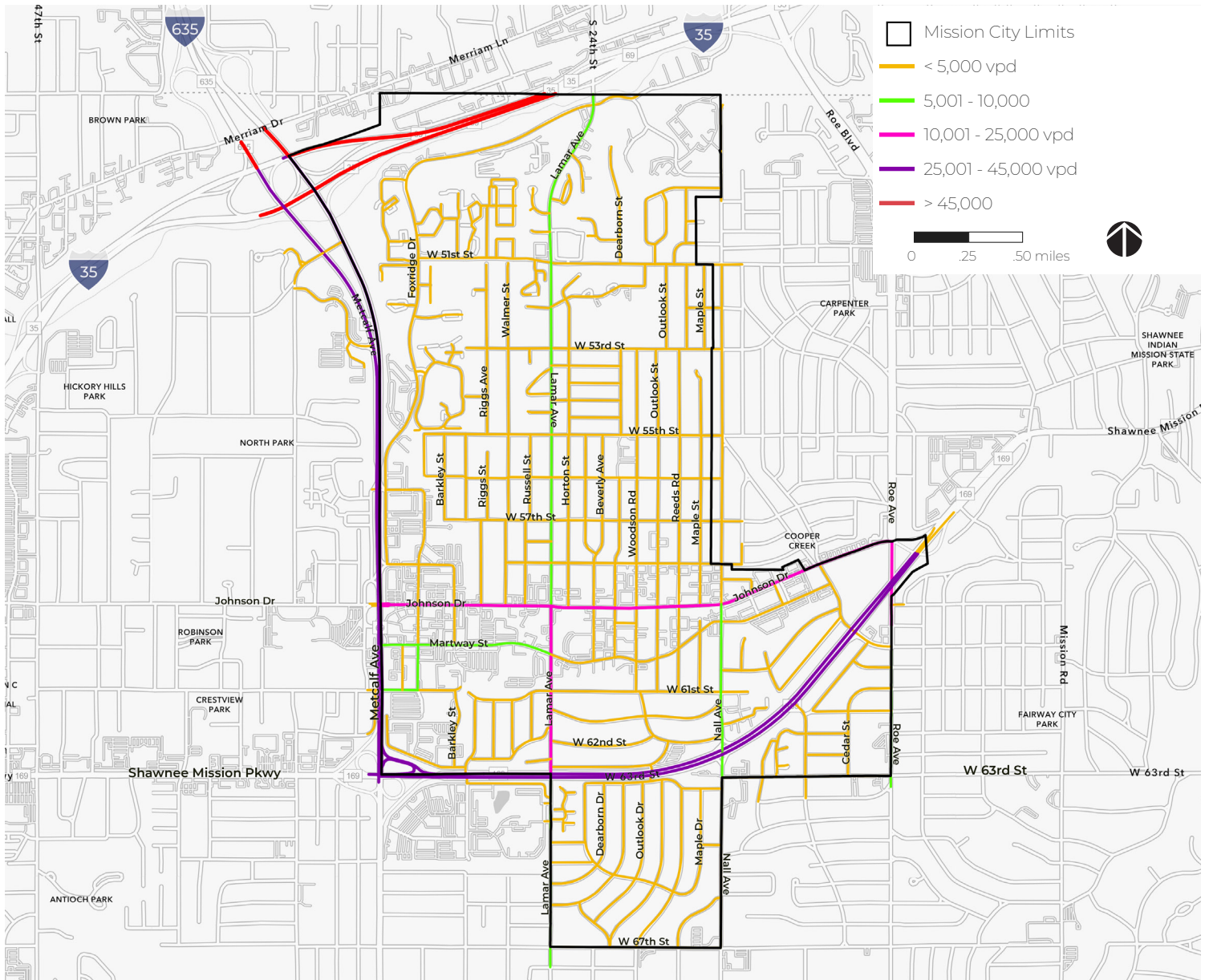


Figure 2.19 - Estimated Volumes

2.5 Transit System

Transit System Overview

Mission is fortunate to have a transit center that enables Mission to be significantly more transit served than other suburban communities.

or along the Johnson Drive and Martway Street corridors. Mission's bus stop locations are shown below in maroon asterisk.

The main routes through Mission include 401 Metcalf-Plaza, 402 Johnson-Quivira, 403 Antioch-Olathe, and 435 JoCo to Downtown. Most of these routes run near

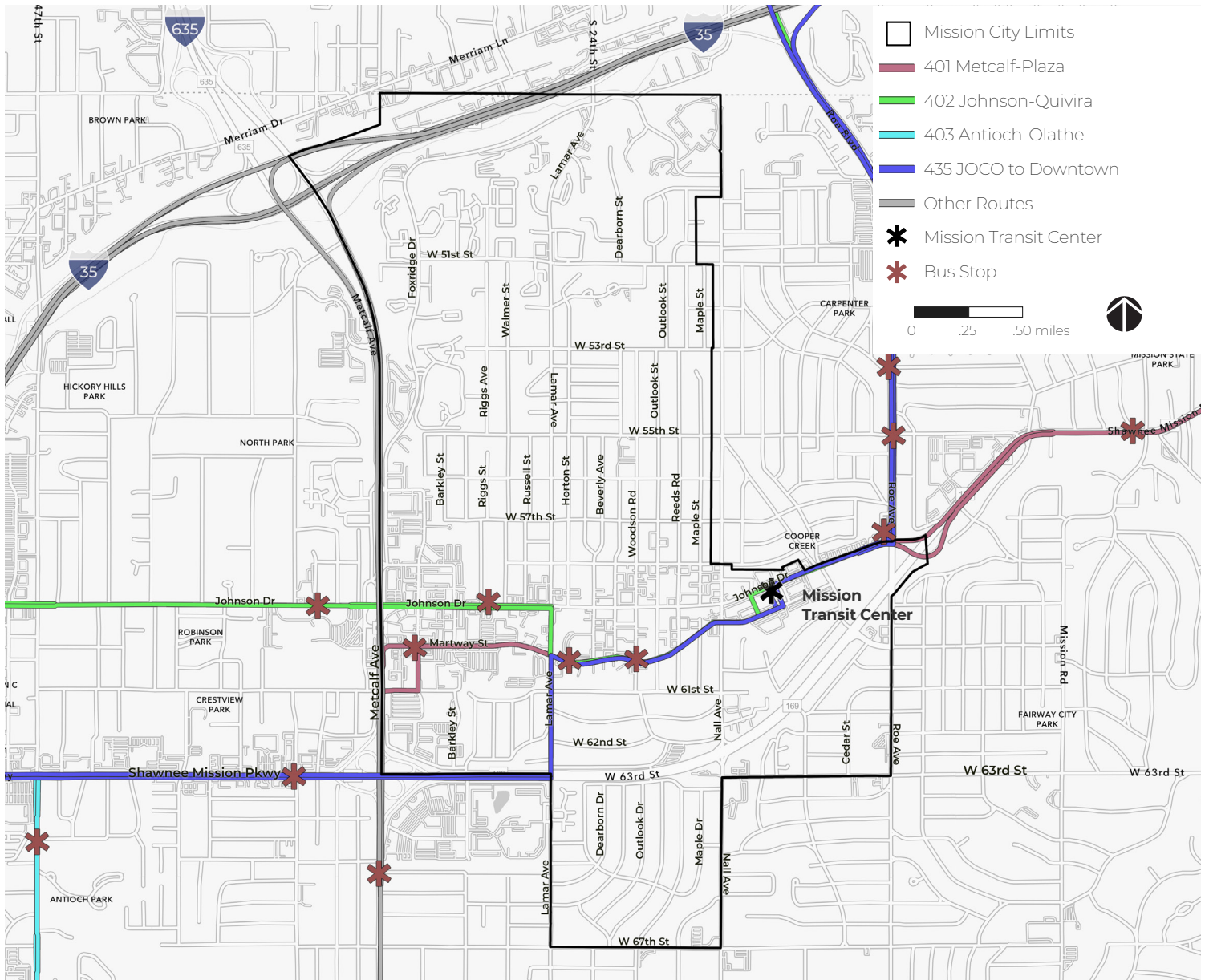


Figure 2.20 - Transit System

2.6 Natural Features

Floodplain

Floodplains are land that is inundated with water during and after heavy rainfall or snowmelt. Typically, floodplain is found in the low-lying land adjacent to streams and other water bodies. Development should typically not occur within the floodplain because of the negative environmental side effects of having permeable land in the floodplain.

100- and 500-Year Floodplain

Floodplain land can be divided into many categories, but the two most common ones are the 100-year and the 500-year floodplain. The 100-year floodplain includes land that has a 0.1% chance of flooding in any given year and the 500-year floodplain is land that has a 0.2% chance of flooding in any given year.

Floodplain in Mission

Mission's two areas with floodplain are along Turkey Creek (near the northern boundary) and along Rock Creek south of Johnson Drive (Figure 2.21).

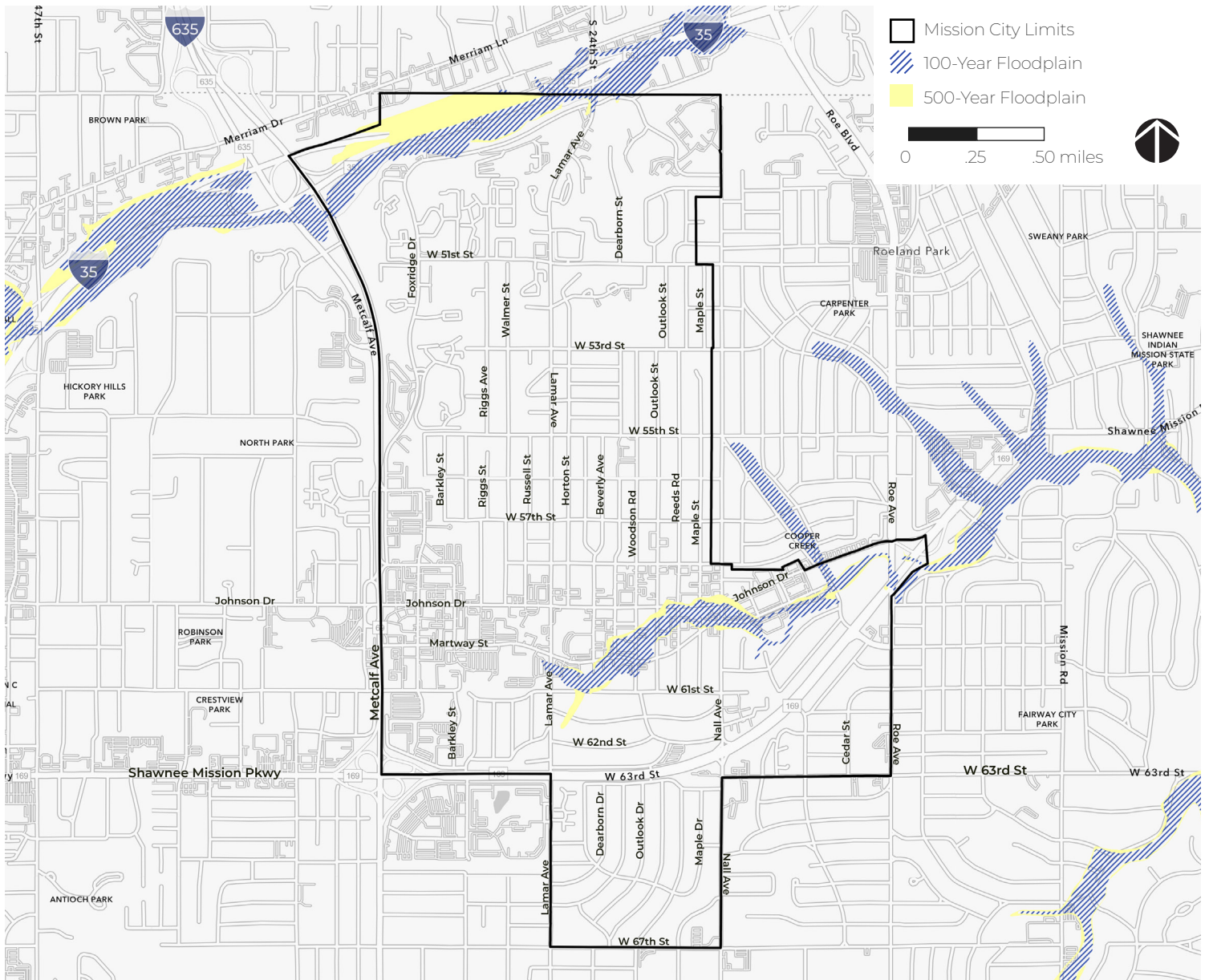


Figure 2.21 - Floodplain

2.6 Natural Features

Tree and Grass Cover

Tree cover estimates were gathered using i-tree canopy. I-Tree Canopy is a program that estimates tree cover and tree benefits for a given area with a random image sampling process that classifies ground cover types. I-Tree Canopy is supported by the U.S. Forest Service, the Arbor Day Foundation, and the Woodland Trust among other partners.

there are approximately 515 acres of tree cover with a margin of error of around +/- 42 acres. This translates to a percentage tree cover of around 30.64% with a margin of error of around +/- 2.48%.

	Acres	%
Tree Cover	515.0	30.6%
<i>margin of error</i>	+/- 42	+/- 2.5%

Table 2.7 - Tree Cover Percentage Estimates

According to the analysis, an additional 403.8 acres of Mission's land cover is grass or other herbaceous cover (typically permeable) with a margin of error of +/- 38.6 acres.

According to the i-Tree Canopy analysis completed for Mission,

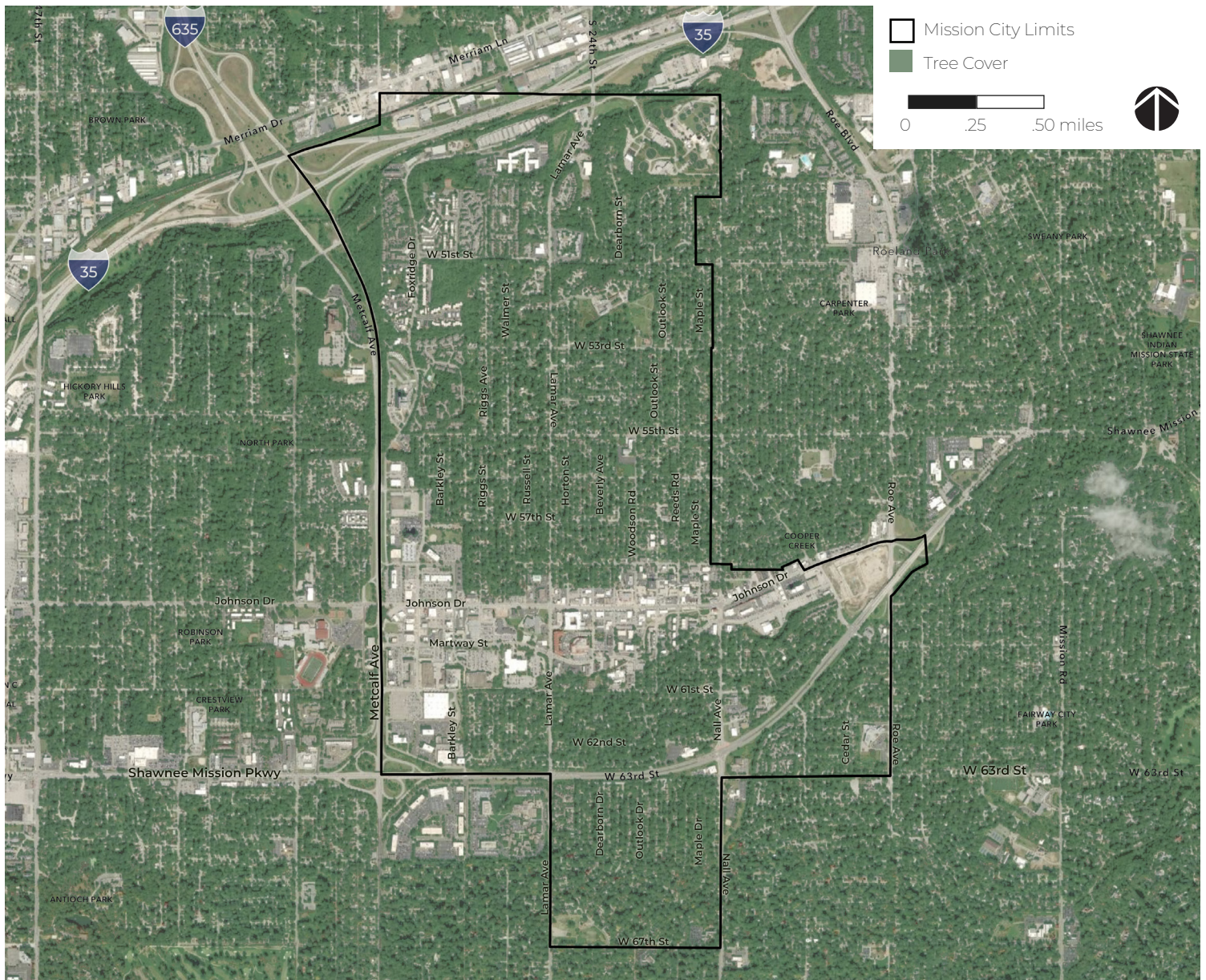


Figure 2.21 - Tree Cover

3.0 Market Analysis

3.1 Regional Context

Mission in the Kansas City Region

The City of Mission is centrally located within the Kansas City metro area, on the northern edge of Johnson County. Its nearest neighbors are Kansas City (KS) to the north, Roeland Park, Fairway, and Prairie Village to the east, and Overland Park to the south and west.

Figure 3.1 shows areas accessible within a 15- and 30-minute drive of Mission. Transportation to and from Mission is facilitated by the interstate network, particularly Interstates 35 and 635, which are immediately adjacent to the city. Downtown Kansas City is located within a 15-minute drive, as are portions of Kansas City, Kansas, Kansas City, Missouri, Overland Park, Lenexa, and Shawnee. A large portion

of the metropolitan area is accessible within a 30-minute drive, including the airport, and most of the municipalities within Johnson, Wyandotte, Platte, and Clay counties. This proximity to jobs, services, and other Kansas City metro area amenities makes Mission a great residential location.

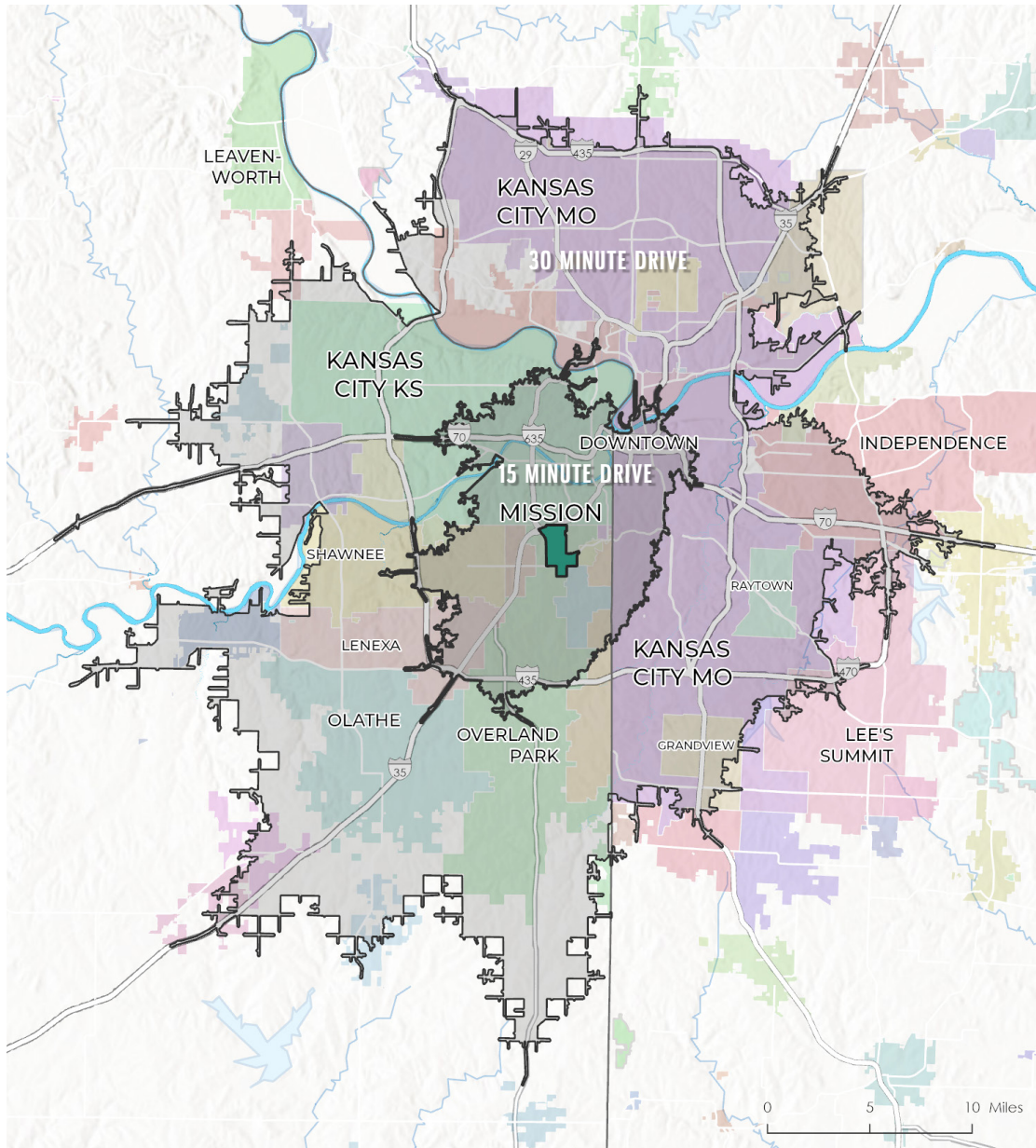


Figure 3.1 - Drive Time Analysis

3.2 Market Area

Mission in the Kansas City Region Cont'd

Figure 3.2 shows the Mission market area, a geographic area used to analyze the supply and demand of different types of real estate, including single family housing, multifamily housing, retail, hospitality, office, and industrial uses.

In the case of Mission, its central and convenient location gives it a wide market area that includes northeast Johnson County and parts of Kansas City, Missouri, and Kansas City, Kansas. Looking at supply and demand indicators within Mission and the surrounding communities can show what types of development are most desirable within this portion of the

Kansas City metropolitan area and give insight into the types of development that would have the highest degree of success in Mission.

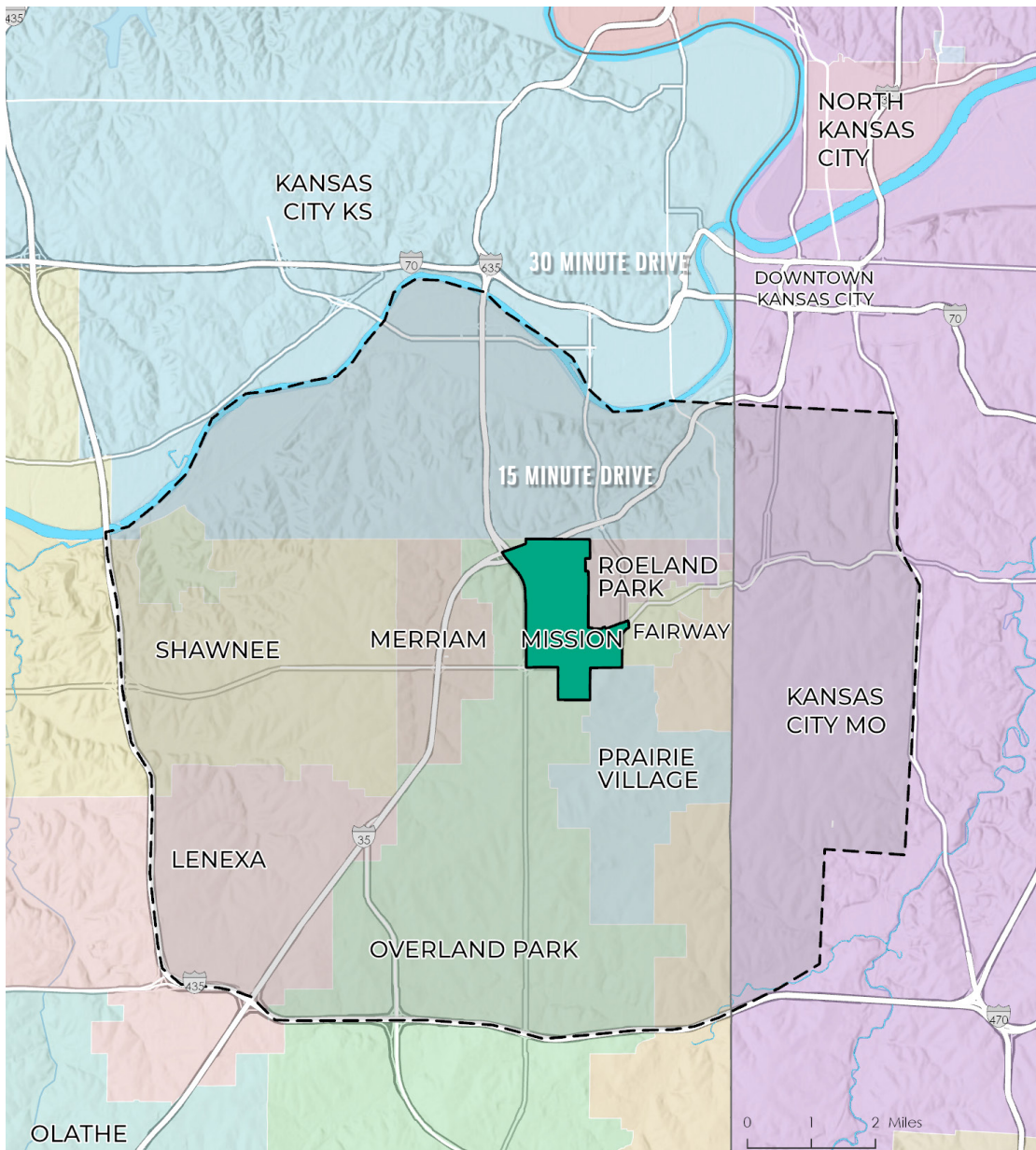


Figure 3.2 - Market Area

3.3 Population Overview

Population and Population Density

Mission was home to nearly 10,000 residents in 2020. The population of Mission and surrounding geographies is shown in Table 3.1

Mission is located in an area of the Kansas City Metro Area that has relatively medium to high population density. Figure 3.3 designates lower densities with lighter shades of blues and greens while high density areas are designated with darker blues. Denser concentrations of population are located to the northeast, within the central core of Kansas City, Missouri, as well as to the north in Kansas City, Kansas. Areas of medium to high population density continue along Johnson County municipalities along I-35, including Merriam, Lenexa, Overland Park, and Leawood.

Approximately 17% of the metro area's population lives within the boundaries of the market area shown to the right. The population of Mission makes up 2.7% of the market area population and one half percent of the metro area's population.

Geography	Population
Mission	9,523
Market Area	370,600
KC Metro Area	2,179,100

Table 3.1 - Residential Parcels and Dwelling Units Breakdown

Source: Esri (2020)

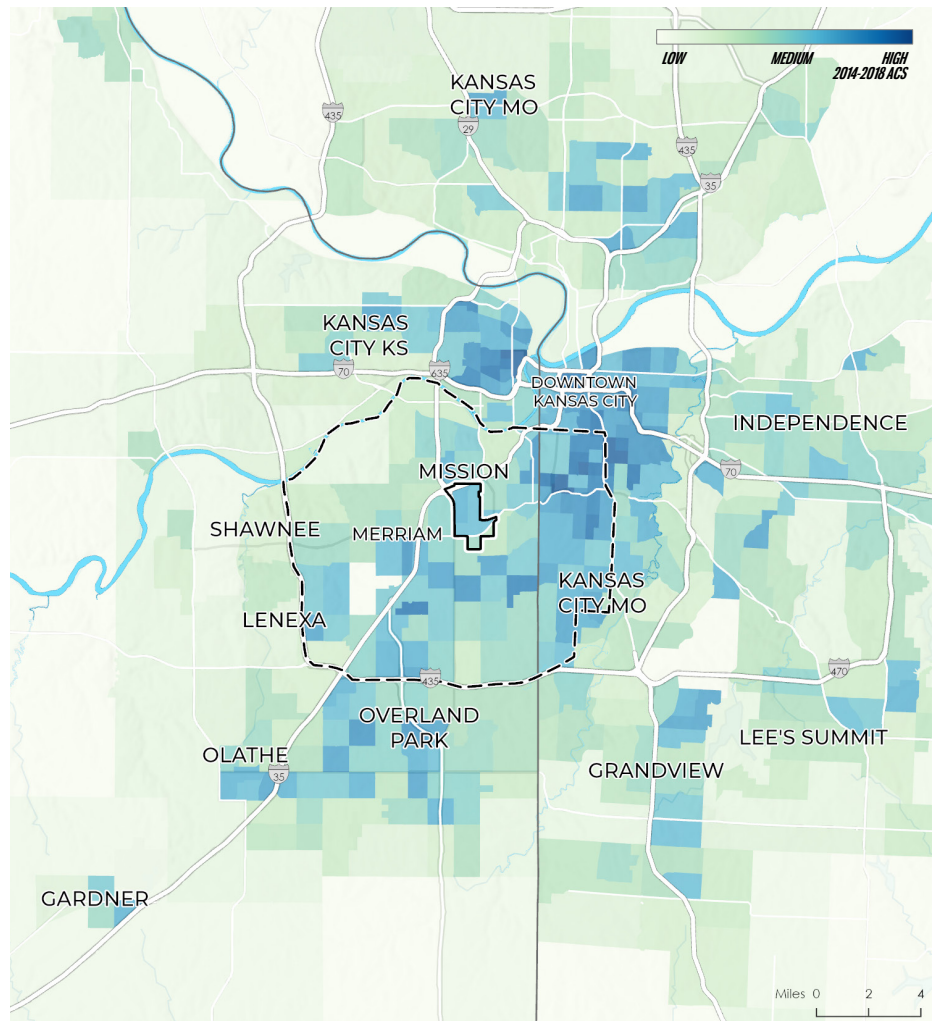


Figure 3.3 - Market Area - Densities

3.3 Population Overview

Population Change

Growth in the Kansas City region is taking place along and immediately outside of the I-435 and I-470 corridors, where sites for new homes are available and transportation facilitates access to employment opportunities. Though Johnson County has traditionally been the center of growth within the region, areas north of the river have added population in recent years.

The peak of Mission's population was in 2000, when the city was home to nearly 10,000 residents. The city lost about 600 residents between 2000 and 2010, but has since added most back, and today has an estimated population of about 9,960 residents.

The market area followed a similar trajectory, losing residents between 2000 and 2010, but gaining back lost population since 2010. The entire metro area has steadily added residents since 2000, and has added almost 182,000 new residents over the past decade.

All three areas – Mission, the market area, and the region – are projected to add new residents over the next five years.

Geography	Population Change 2000 - 2010	Population Change 2010 - 2020	Projected Pop. Change 2020 - 2025
Mission	-0.7%	0.7%	1.6%
Market Area	-0.4%	0.5%	0.8%
KC Metro Area	1.0%	0.9%	0.8%

Table 3.2 - Residential Parcels and Dwelling Units Breakdown

Source: Esri (2020)

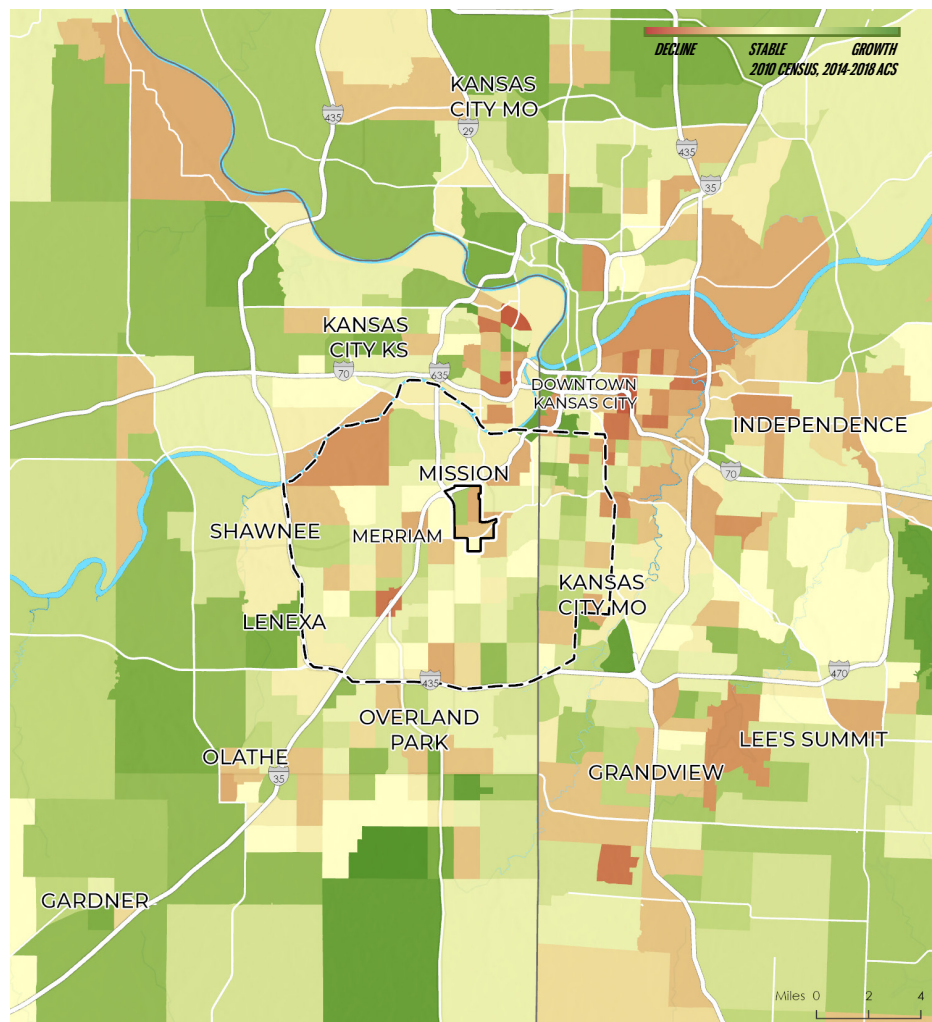


Figure 3.4 - Market Area - Population Decline/Growth

3.3 Population Overview

Migration to the Kansas City Area

Part of the Kansas City metro area's growth is due to immigration from other communities. Migration data is collected each year by the U.S. Census Bureau and provides a snapshot of the communities of origin for new Kansas City residents. The table at the right shows the top ten communities that moved to Kansas City between 2013 and 2017. The Asian continent is the greatest source of new residents, followed by a number of Midwestern cities in Kansas and Missouri. There is a mix of larger metro areas, smaller cities, and college towns.

It is also interesting to note that a large number of Kansas City residents move each year, with an average of over 240,000 residents reporting a move in the previous year from one home in the metro area to another.

As Mission grows, it will likely attract a mixture of these two groups. Residents from elsewhere in the Kansas City metro area will make up a large percentage of those looking for housing in Mission, while transplants from other communities will make up a smaller proportion.

Geography	Population
Asian Continent	3,600
Lawrence, Kansas	3,300
St. Louis, Missouri	2,900
Springfield, Missouri	2,500
Topeka, Kansas	2,400
Wichita, Kansas	2,100
Omaha, Nebraska	1,900
St. Joseph	1,900
Columbia, Missouri	1,400
Phoenix, Arizona	1,400
Moved within KC Metro Area	241,700

Table 3.3 - Top Place of Origin for Households Moving to Kansas City

Source: 2013 - 2017 American Community Survey Metro Area to Metro Area Migration Flows

3.4 Households

Households by Type

Different communities attract different kinds of households based on a number of factors, including location, types of housing available, school district, safety, and public amenities.

Mission has a higher proportion of single-member households and nonfamily households (unmarried partners or roommates) than the market area and the metro area.

Nearly half (47%) of households are made up of one person, compared to 28% in the Kansas City Metro Area. Data from the 2019 American Community Survey (5 Year) estimate that, of these roughly 1,900 households, about 500 are made up of residents age 65 and older.

Family households with children at home make up 18% of households and family households without children at home make up 25% of households,

which is a lower proportion than the market area or metro area.

Looking at households by number of members shows that Mission is home to more smaller households than the market area or metro area. Households with one or two people make up 81% of all households, compared to 70% in the market area and 61% in the metro area.

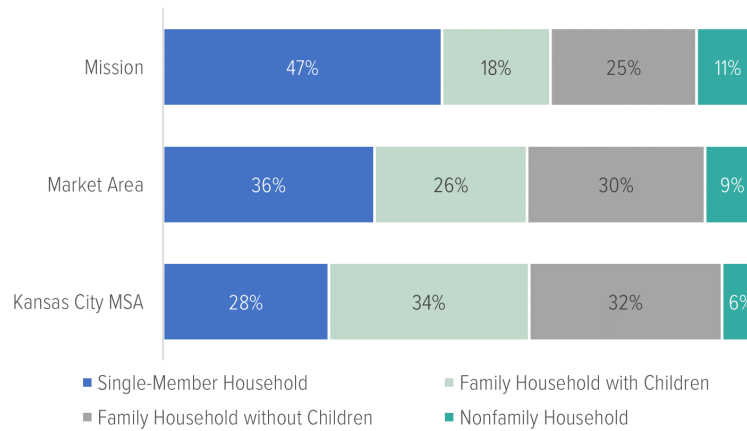


Figure 3.5 - Households by Composition

Source: Esri (2020)

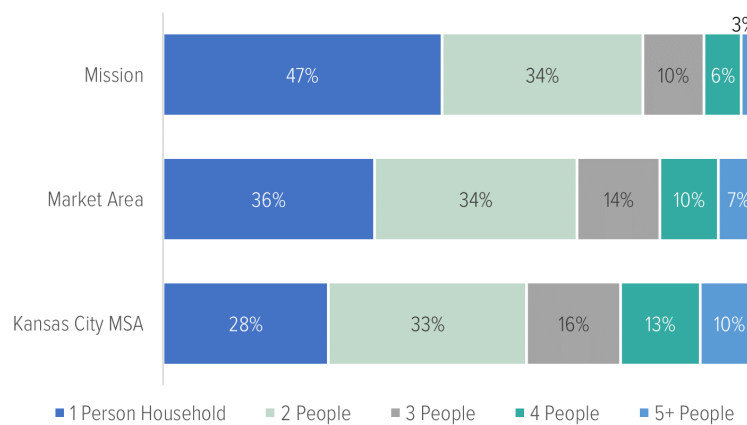


Figure 3.6 - Households by Number of Residents

Source: Esri (2020)

3.5 Employment

Employment Location and Industries

Residents of Mission work throughout the Kansas City region, but the majority work either in or around Downtown and Midtown Kansas City, along I-35, or along I-435, all of which are major regional employment destinations. The map at right shows employment density by census tract. The lines indicate the top 25 census tracts where residents of Mission work. The majority of commuters travel to the northeast to work in and around Downtown Kansas City or travel south to work in and around Overland Park. Mission is well-located for households working in the region's major employment nodes, particularly two-earner households where members may work in two different communities.

Figure 3.8 shows the share of employees by occupational category. Mission residents are more likely to be employed in white collar occupations (76%) than residents of the market area (72%) or metro area (65%).

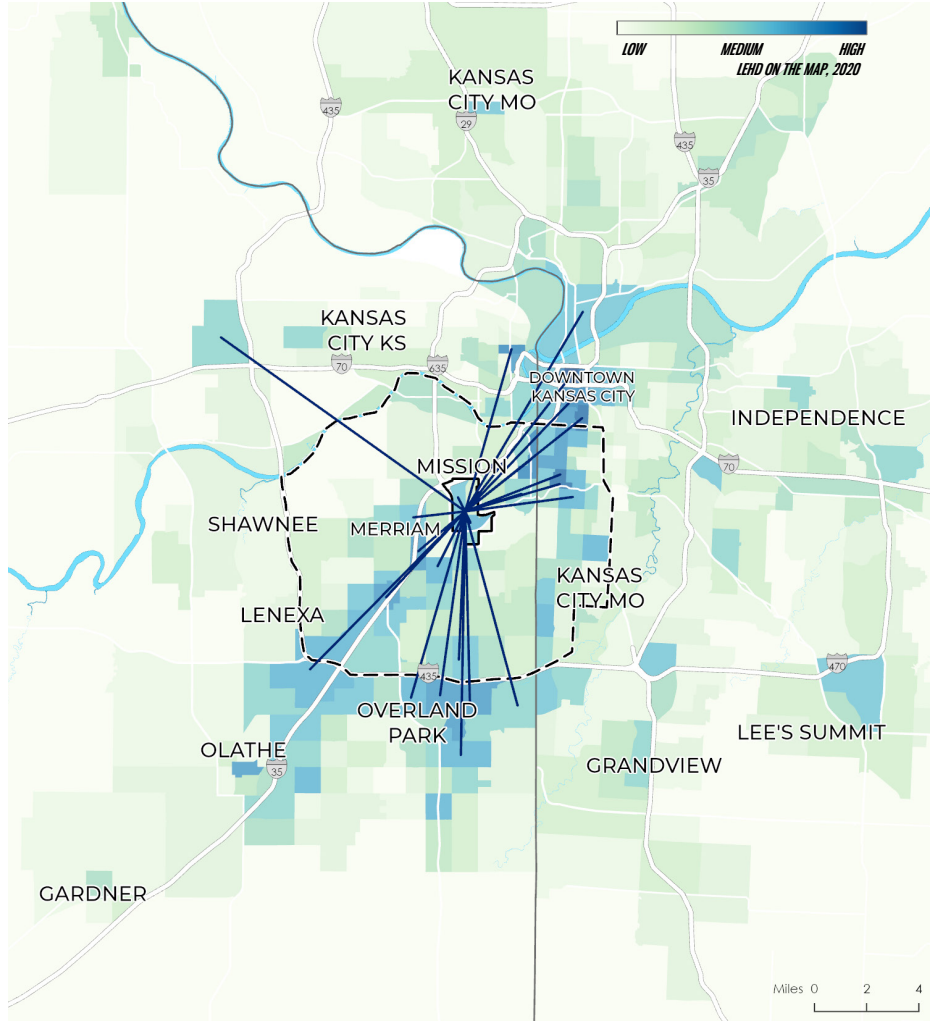


Figure 3.7 - Employment Density and Top Commuting Locations for Mission Residents
Source: U.S. Census LEHD OnTheMap (2020)

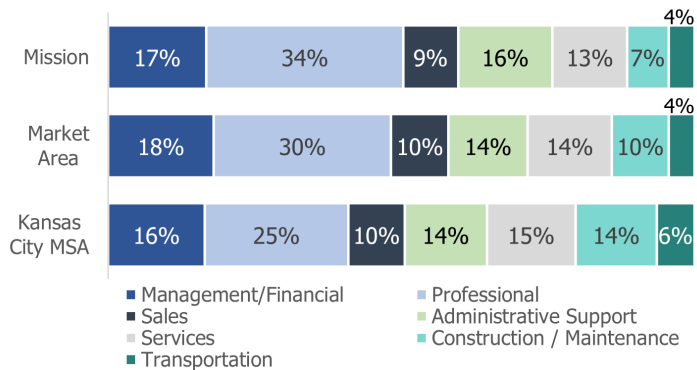


Figure 3.8 - Share of Population by Occupation
Source: Esri (2020)

3.5 Employment

Workers Commuting to Mission

Similar to residents of Mission, who commute to other communities in the region to work, workers also come to Mission from all over the region. There are nearly 8,900 jobs in Mission, in a mix of sectors that includes professional services, administration, manufacturing, retail, accommodation, and food service.

Top origin cities include Overland Park and Kansas City, Missouri, each of which sends about 1,200 workers (about 14% of all workers) to Mission daily. This is followed by Kansas City, Kansas, that sends nearly 1,000 workers to Mission daily (about 11%). About 280 Mission residents also work within the city (3% of workers). Many of the other top origin communities are located in Johnson County.

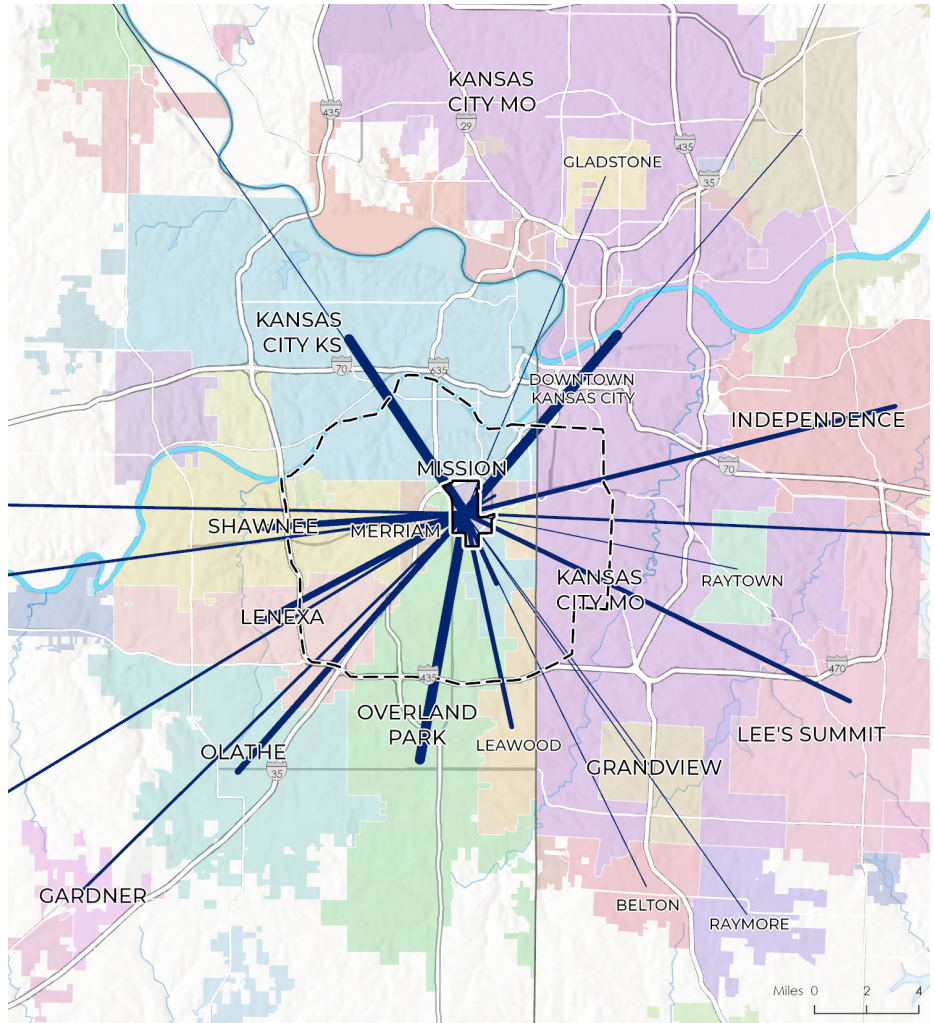


Figure 3.9 - Top Cities of Origin for People Who Work in Mission

Source: U.S. Census LEHD OnTheMap (2021)

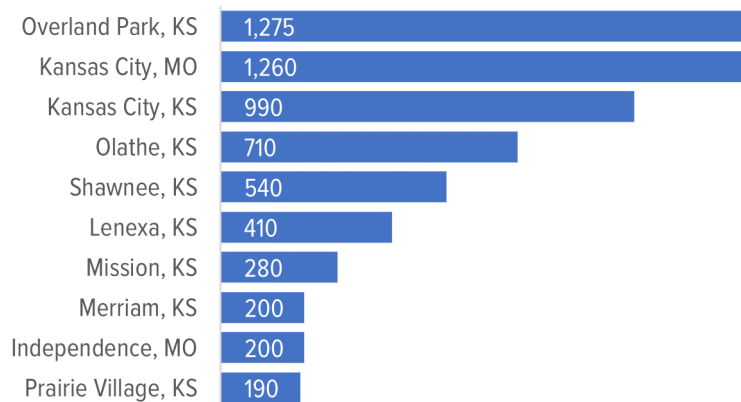


Figure 3.10 - Top 10 Cities of Origin for People Who Work in Mission

Source: U.S. Census LEHD OnTheMap (2021)

3.6 Incomes

Median Household Income

Median household income throughout the region is shown in Figure 3.11. Mission is largely a middle-income community, with a median household income of \$59,400, compared to \$64,800 in the market area, and \$66,400 in the metro area. Higher income areas are found to the southeast along the state line in Mission hills, Prairie Village, and Leawood. Lower income areas are found to the north in Kansas City, Kansas.

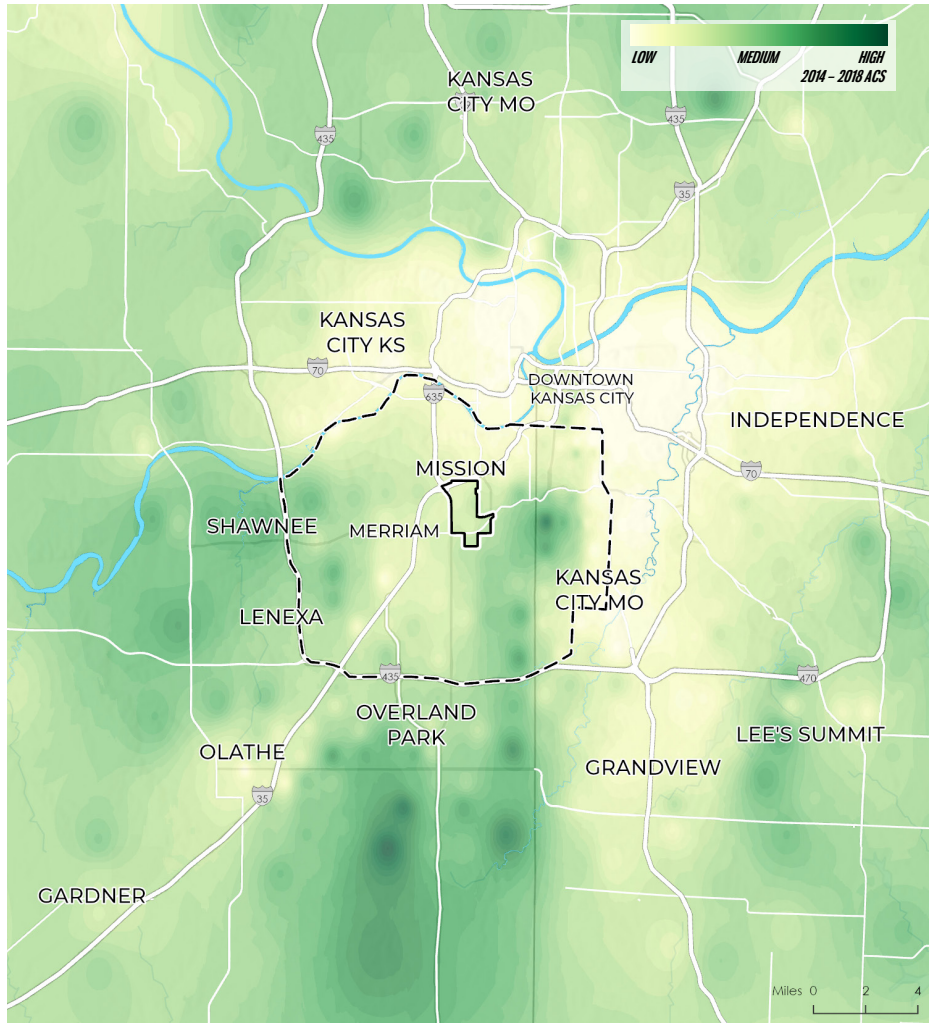


Figure 3.11 - Median Household Income

Source: Esri (2020)

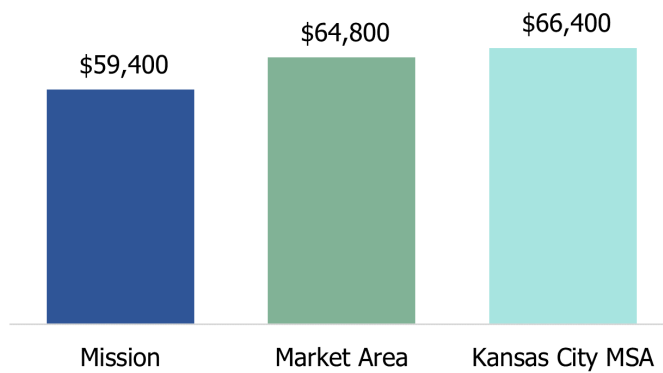


Figure 3.12 - Median Household Income

Source: Esri (2020)

3.6 Incomes

Income Density

Income density multiplies the number of households by average household income in order to understand which parts of the metro area have the most spending power. These areas will be the most likely to attract retail and entertainment tenants due to the amount of disposable income within the community. The areas of the Kansas City metro area with the highest income density are largely located in western and southern Johnson County, while Kansas City, Kansas, and Kansas City, Missouri, have comparatively lower income densities. Mission falls in the middle of these two areas, with a medium amount of income density.

Average consumer spending is lower in Mission (\$42,900) than in the market area (\$48,800) and the metro area (\$51,000).

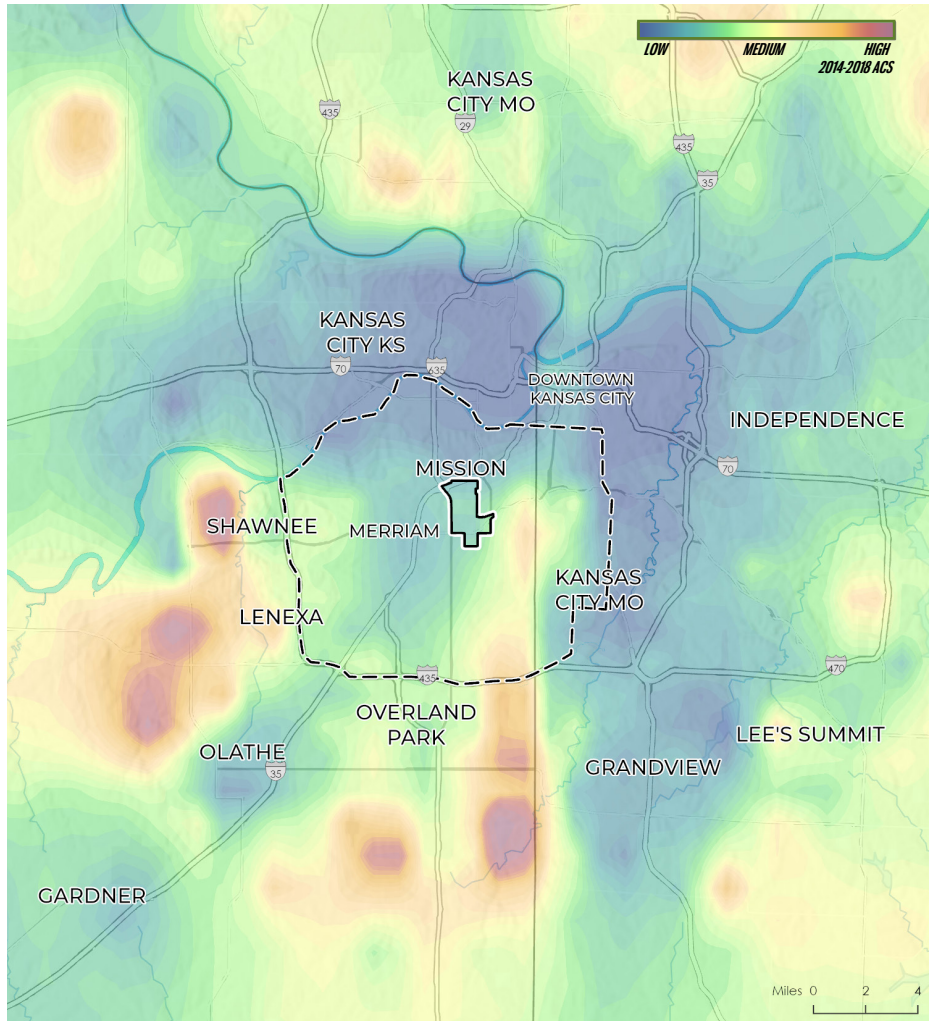


Figure 3.13 - Income Density

Source: 2014 - 2018 American Community Survey

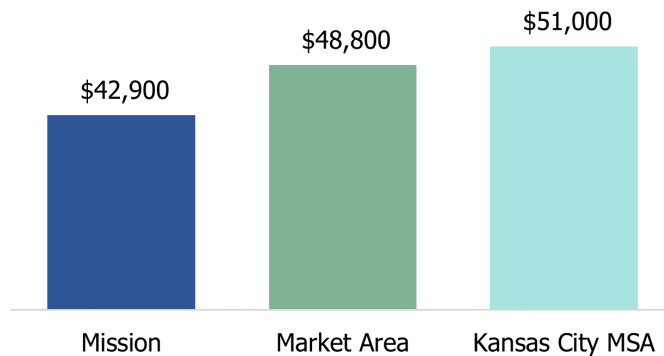


Figure 3.14 - Average Annual Household Spending

Source: Esri 2020

3.7 Housing

Median Housing Values

Figure 3.15 shows median housing value by census tract throughout the Kansas City metro area. The highest housing values in the market area are found along the state line in Johnson County, while lower housing values are found to the north in Kansas City, Kansas.

Median housing values in Mission (\$192,200) are very similar to those of the region (\$196,200), and slightly lower than those in the market area (\$212,000).

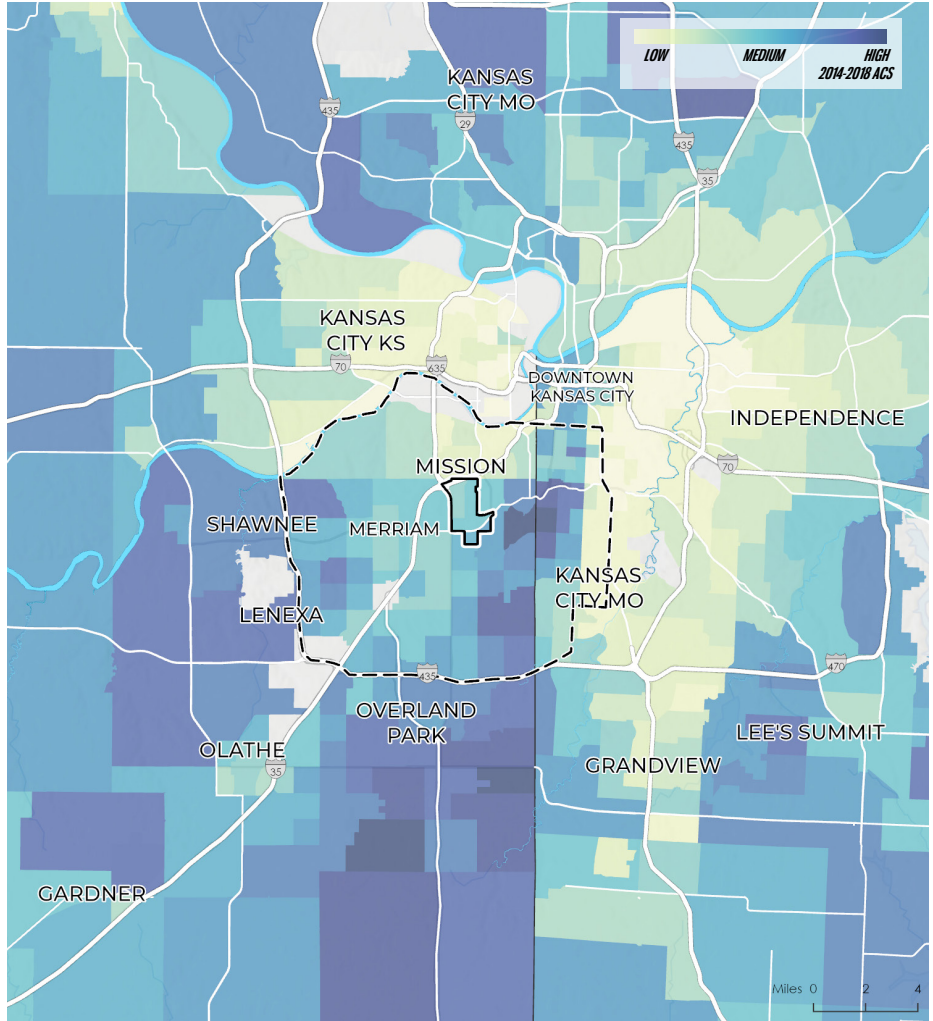


Figure 3.15 - Median Housing Value

Source: 2014 - 2018 American Community Survey

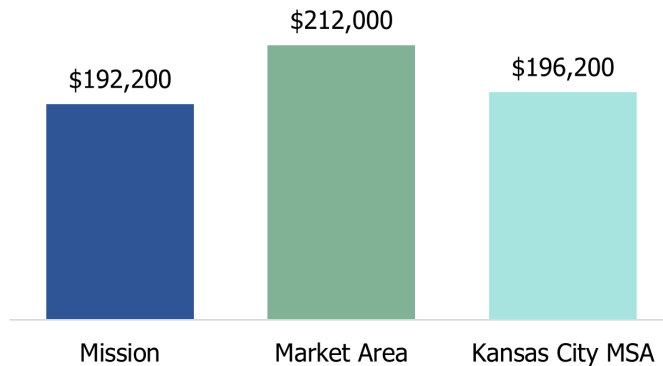


Figure 3.16 - Median Housing Value

Source: 2014 - 2018 American Community Survey

3.7 Housing

Housing Age

Figure 3.17 gives the decade of construction for single family residential buildings in Mission (townhomes, condos, and detached single family). Overall, the busiest era of housing construction in Mission was during the 1950s with a later, smaller peak in the 1980s. There has been relatively little construction since 1990, likely a result of a lack of available sites. The newest construction is found south of Shawnee Mission Parkway and near Lamar Avenue in the southern part of the city.

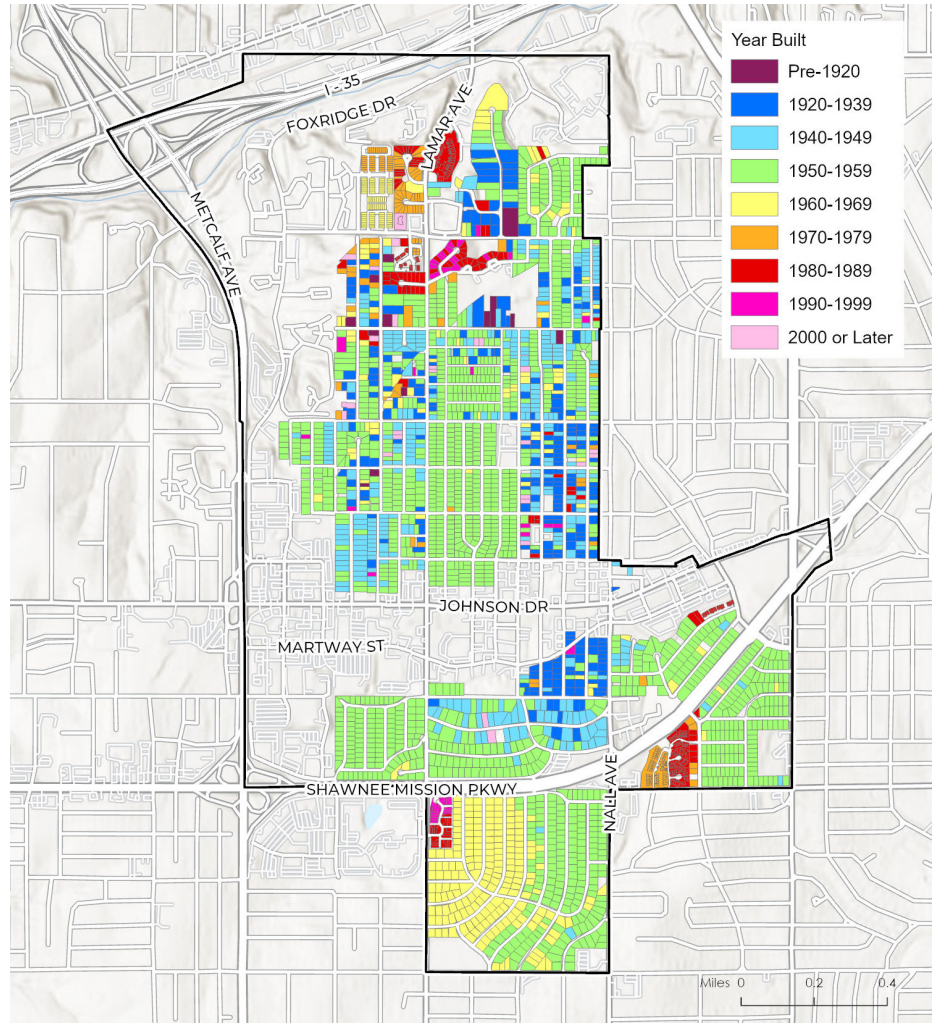


Figure 3.17 - Year of Housing Construction

Source: City of Mission (2020)

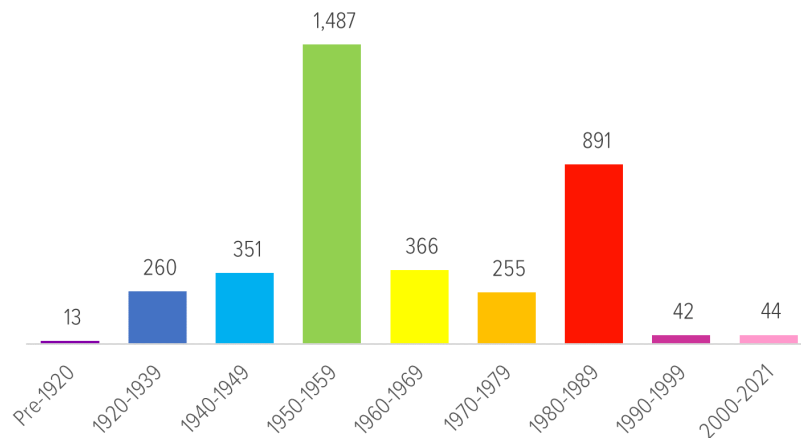


Figure 3.18 - Year of Housing Construction

Source: City of Mission (2020)

3.0 MARKET ANALYSIS

3.7 Housing

Housing by Size

Figure 3.19 shows the size of single family homes (condos, townhomes, and detached single family homes) that were sold in the five years leading up to April 2021. The majority of homes (81%) were between 650 and 2,000 square feet, with the greatest share (31%) in the 1,001 to 1,500 square feet range.

Looking at homes that changed ownership over the past five years, the majority (61%) had three bedrooms. Two-bedroom homes were the second most common, at 19%, followed by four-bedroom homes, which made up 15% of sold homes. Homes with five bedrooms or more made up 5% of sold homes, and one-bedroom homes were the least common, at less than 1%.

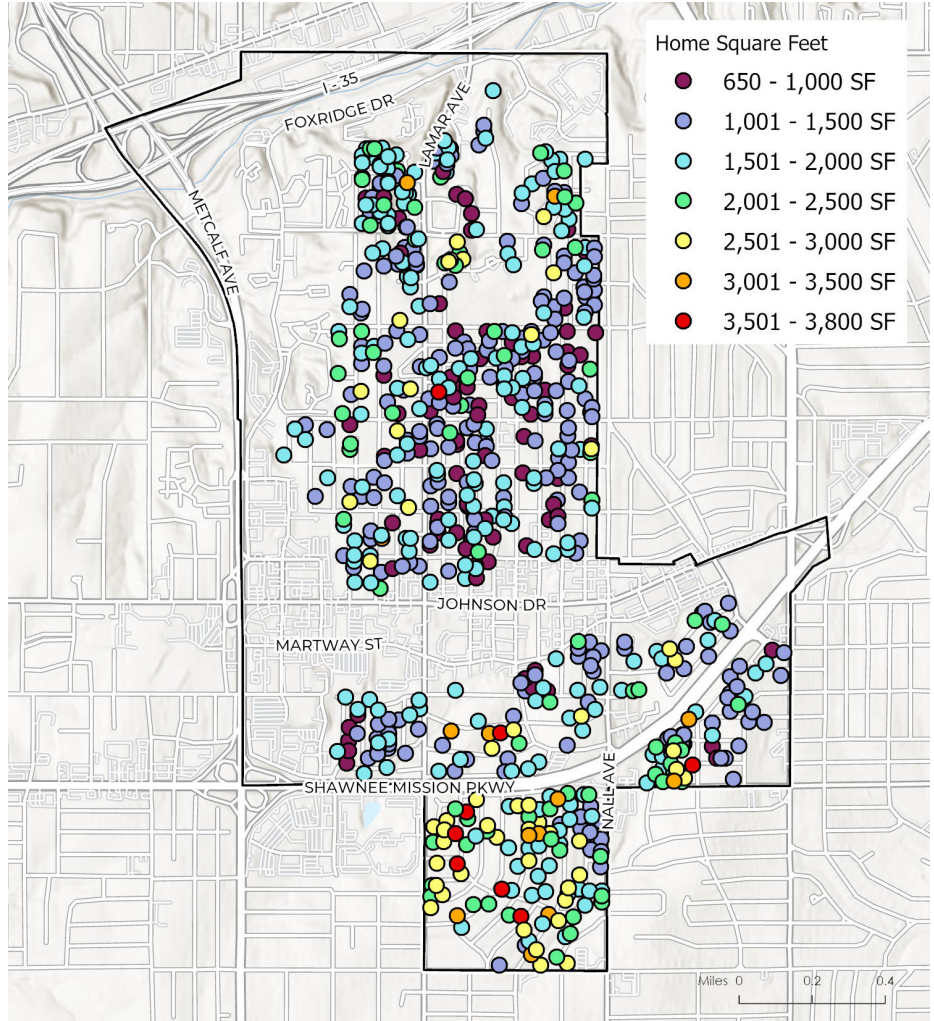


Figure 3.19 - Homes Sold Between 2016 and 2021 by Size (Sq. Ft.)

Source: Redfin (2021)

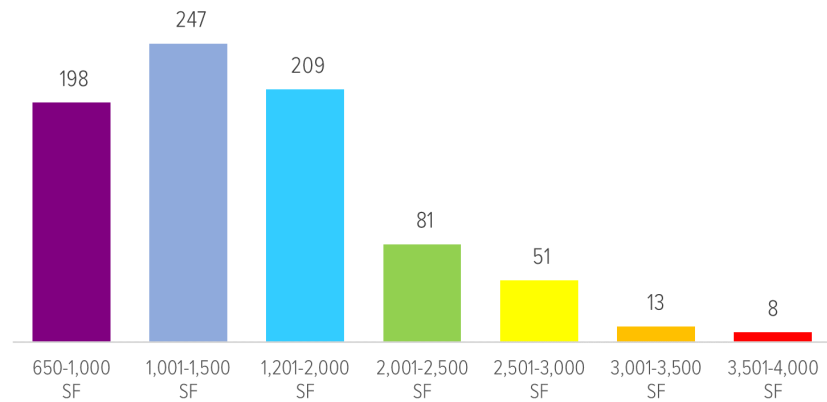


Figure 3.20 - Homes Sold Between 2016 and 2021 by Size (Sq. Ft.)

Source: Redfin (2021)

3.7 Housing

Regional Single-Family Home Trends

Figure 3.21 shows single family homebuilding activity in and around the market area over the past 20 years. Unsurprisingly, the greatest number of housing units have been developed at the metro area's periphery, where land is available for new construction.

Within the market area, which is largely built out, the majority of new housing over the past twenty years has been infill housing. This trend has been particularly pronounced within Prairie Village, adjacent to Mission, where teardown/rebuild construction has grown more common in recent years. This type of infill construction indicates demand for housing within a particular community and is a positive indicator of a community's attractiveness. In the outer portions of Shawnee, Lenexa, and Olathe, housing construction has occurred in larger, multi-home developments due to the availability of large tracts of buildable land.

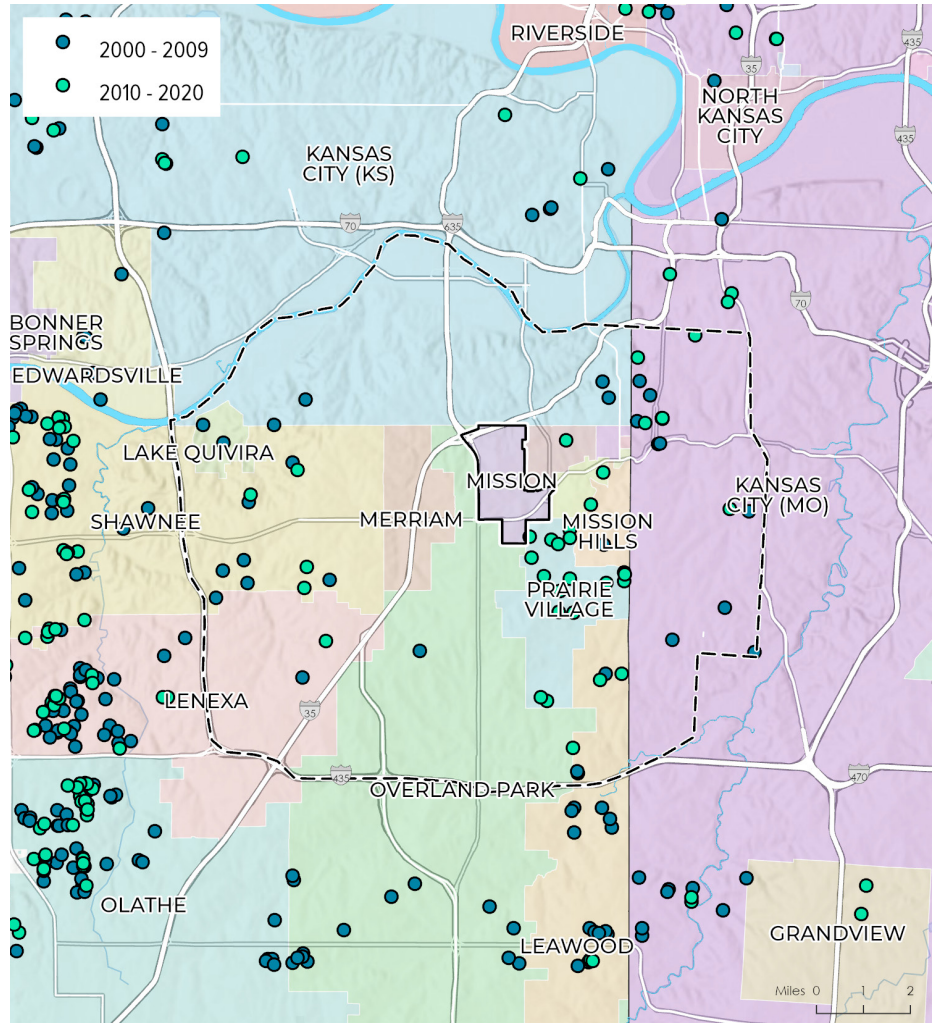


Figure 3.21 - Single-Family Homebuilding Activity In and Around the Market Area

Source: Redfin (2021)

3.7 Housing

Local Single-Family Housing Values

Figure 3.22 gives a closer look at sales prices for single family homes (detached single family homes, townhomes, and condos) for properties sold in the five year period between April 2016 and April 2021. Over the past five years, there were a total of 807 home sales, representing 22% of all housing units in Mission. Homes in the \$150,000 to \$200,000 range made up the greatest number of sales (308 sales, or 38%), followed closely by homes in the \$200,000 to \$300,000 range (300 sales, or 37%). A smaller number of homes sold for \$300,000 or more. The majority of these homes were located in the Countryside and Milhaven subdivisions south of Shawnee Mission Parkway.

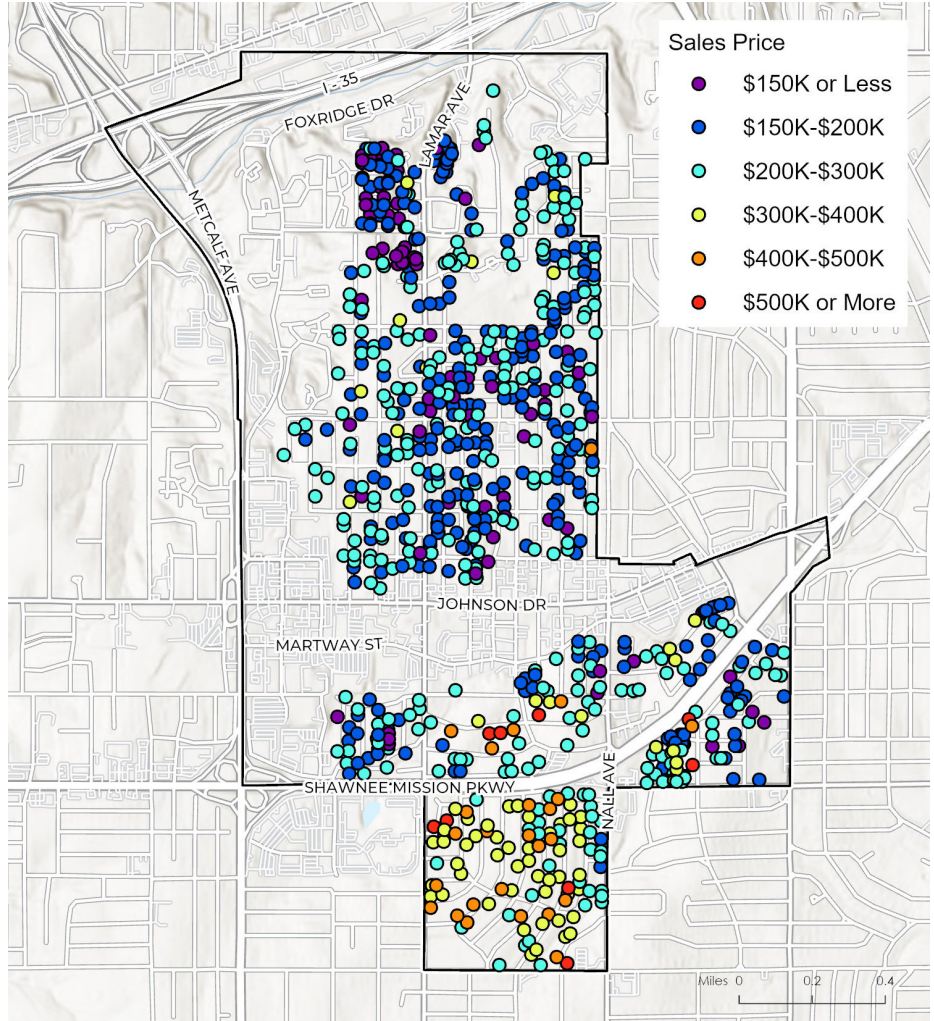


Figure 3.22 - Single-Family Home Sales by Price (April 2016 - April 2021)

Source: Redfin (2021)

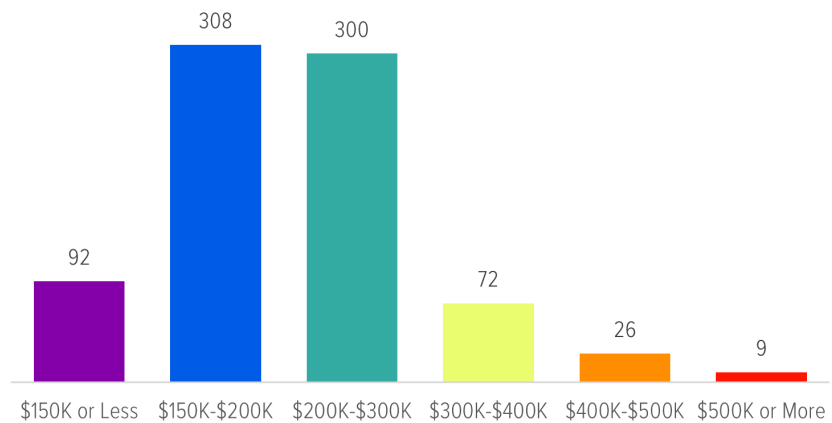


Figure 3.23 - Single-Family Home Sales by Price (April 2016 - April 2021)

Source: Redfin (2021)

3.7 Housing

Local Single-Family Sales Trends

Home sales data provides an insight into the market value of homes with different characteristics. Figure 3.24 shows the sales price per square foot by decade of construction. There is an interesting correlation between housing age and market value, which forms a V-shape in which the newest homes (built in 2000 or later) and historic/midcentury homes (from the 1960s or earlier) have the highest market value per square foot, while homes built between 1961 and 1990 have lower values per square foot. This pattern has been common throughout the U.S. over the course of the 20th century, where housing tends to be least valued as it approaches the 50 year mark, as these homes are perceived as outdated but not yet unique and worthy of preservation.

Figure 3.25 shows the change in average price per square foot for all single family housing units between 2016 and 2021. Similar to nationwide trends, the price of housing has increased over the past five years. This trend has been particularly pronounced in attractive communities such as Mission, which saw housing prices per square foot increase from \$124 in 2016 to \$188 in 2021. Likewise, the number of home sales has increased. From 2016-2018, there were fewer than 120 sales per year, while in 2019 and 2020, there were more than 160 sales.

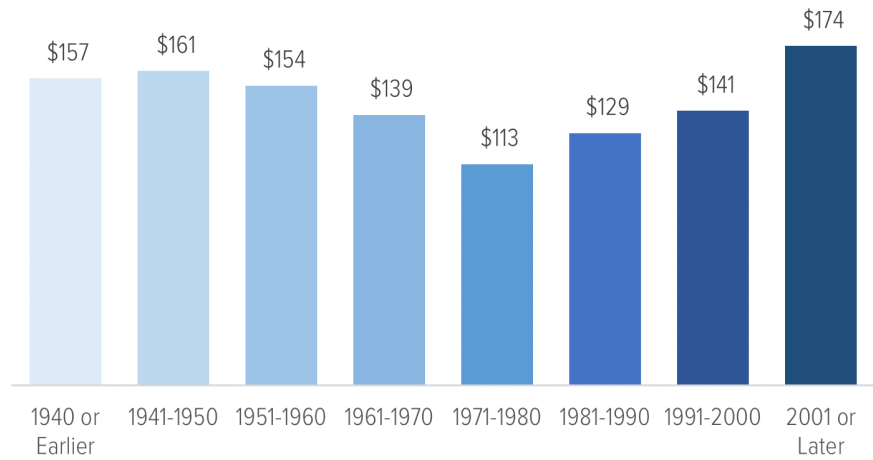


Figure 3.24 - Sales Price per Square Foot by Decade of Construction Single-Family Home Sales Between April 2016 - April 2021

Source: Redfin (2021)

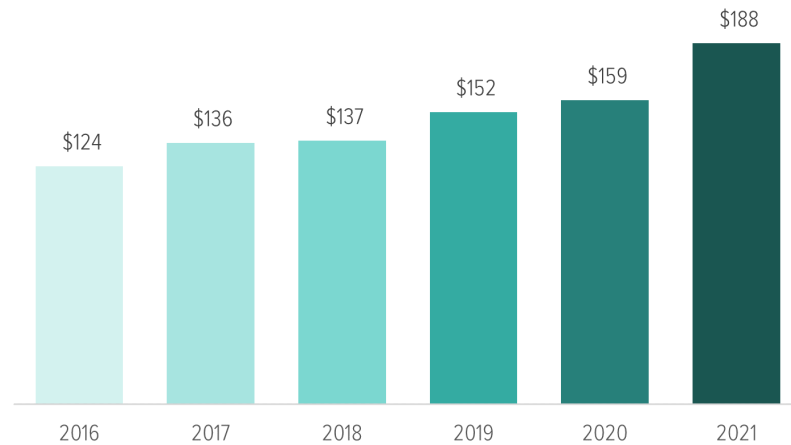


Figure 3.25 - Sales Price per Square Foot by Decade of Construction Single-Family Home Sales Between April 2016 - April 2021

Source: Redfin (2021)

3.7 Housing

National Single-Family Residential Trends

Home size is slowly trending downward.

The average size of single family housing has decreased after a long period of increase beginning in the 1980s. In 2020, new single-family homes had a median floor area of about 2,260 sq. ft., down from a peak of about 2,470 sq. ft. in 2015. Median lot size has also shown a downward trend in recent years. The median single family lot measured about 7,820 sq. ft. in 2019, down from a high of 10,000 sq. ft. in 1990 and 10,125 sq. ft. in 1976. Particularly in suburban areas, preferences are shifting from a large house on a large lot to a large (but slightly smaller) home with less outdoor space.

Home preferences are shifting due to the COVID-19 pandemic.

Due to the effects of the COVID-19 pandemic, Americans are spending more time at home than usual. While many people worked and entertained themselves outside of the home pre-pandemic, the suspension of public gatherings and activities mean that work and entertainment are now taking place at home to a greater degree.

If these trends toward spending more time at home persist, due to increased remote work, for example, it is possible that housing preference may shift as a result. First, there could be greater demand for home office space, as well as demand for more indoor and outdoor recreational space, particularly for households with children. This could cause families to seek out larger homes. Second, for households that no

longer need to commute, there could be greater demand for housing at longer distances from the workplace, including suburban, exurban, and rural housing, internet connectivity permitting. For people who moved during COVID-19, lower cost of living, greater quality of life, and proximity to family or recreational opportunities played an important role in choosing where to move.

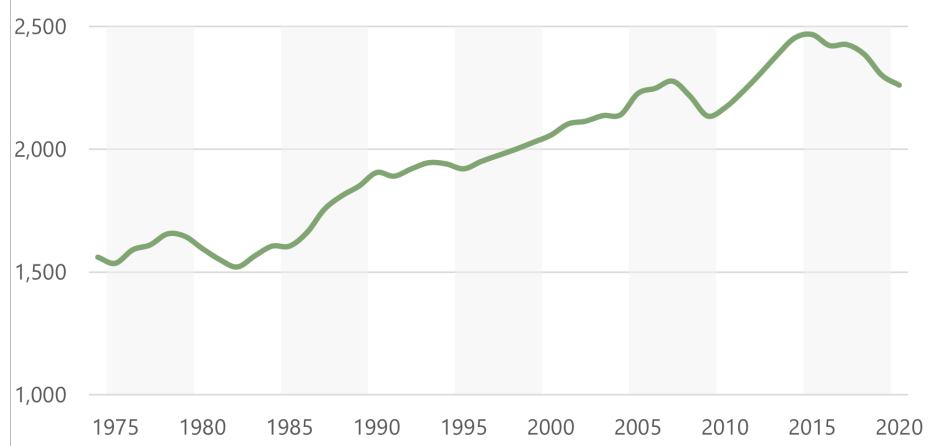


Figure 3.26 - Median Square Feet of New Single-Family Housing Units

Source: U.S. Census (2021)

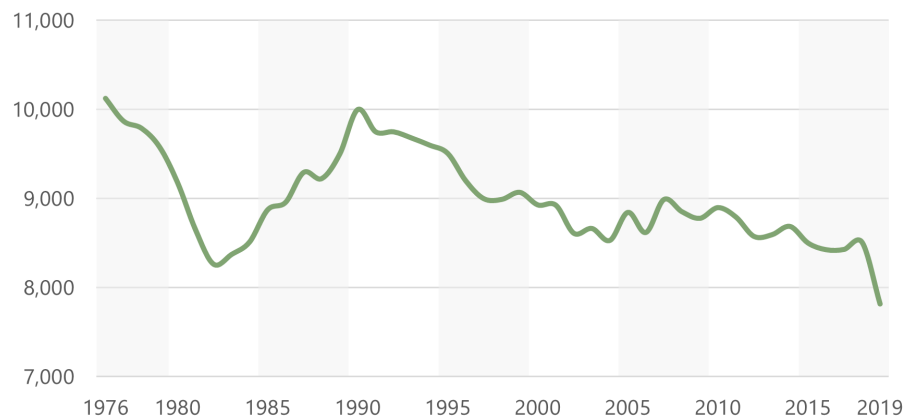


Figure 3.27 - Median Square Feet of New Single-Family Housing Units

Source: U.S. Census (2021)

3.7 Housing

National Single-Family Residential Trends

Home prices are increasing due to supply constraints.

Another major trend in single-family residential housing is a national housing shortage. As shown in Figure 3.28, this trend originated beginning with the Great Recession of 2007-2009, when building activity slowed down dramatically. It continued to decrease until 2011, before slowly increasing, but has not yet reached pre-recession levels. In 2020, the number of completed housing units was 65% of that in 2006.

At the same time, population growth continued in the U.S. in a largely linear fashion, meaning that demand for housing grew during the same period. Other factors leading to an increase in housing prices include increases in construction labor cost due to worker scarcity (another effect of the Great Recession), increases in the cost of materials, particularly lumber, as well as low mortgage interest rates. As a result, housing prices have grown more quickly than per capita personal income and the consumer price index, one measure of inflation. These values, indexed to 2000, are shown in Figure 3.29.

What Mission can do.

Because of this convergence of factors, housing affordability is becoming more of an issue, particularly for lower-income households with fewer resources. Cities can address this housing shortage in various ways, including permitting a greater number of units, a greater density of units, or expediting the permitting process to bring units to market more quickly. Because Mission offers high quality of life, any new housing units built within Mission are likely to be sold or rented quickly.

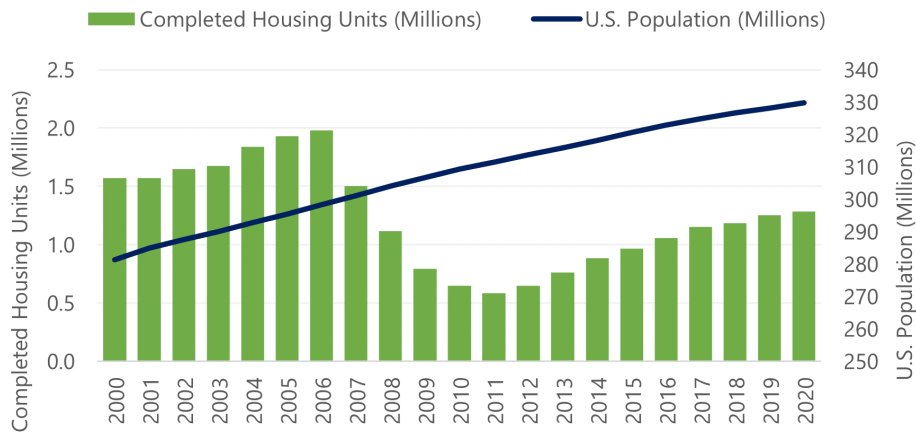


Figure 3.28 - Completed Housing Units by Year (2000 - 2020)

Source: U.S. Census (2021)

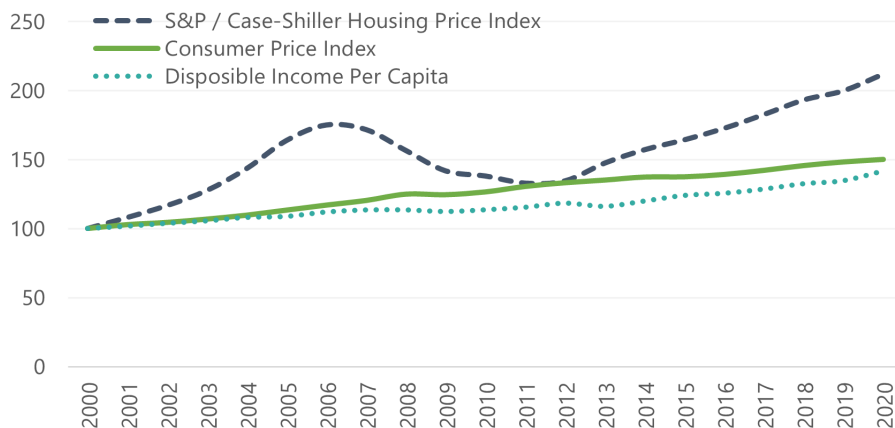


Figure 3.29 - Housing Prices Compared to Disposable Income and Inflation

Source: Federal Reserve Bank of St. Louis (2021)

3.7 Housing

Multi-Family Housing

Figure 3.30 shows multifamily housing construction since 2000. Major multifamily growth areas include downtown and midtown Kansas City, in Lenexa along I-435, and along West 135th Street in south Johnson County.

Within Mission, recent multifamily construction includes The Locale (2020, 201 luxury units plus ground floor retail and restaurant space), The Welstone at Mission Crossing (2016, 100 senior units), and Mission Square (2010, 55 senior units).

Figure 3.31 shows the percent of housing units that are owner-occupied, renter-occupied (apartments or single-family homes), or vacant. Of the three geographies, Mission has the highest percentage of renter-occupied units, accounting for almost half of all housing units.

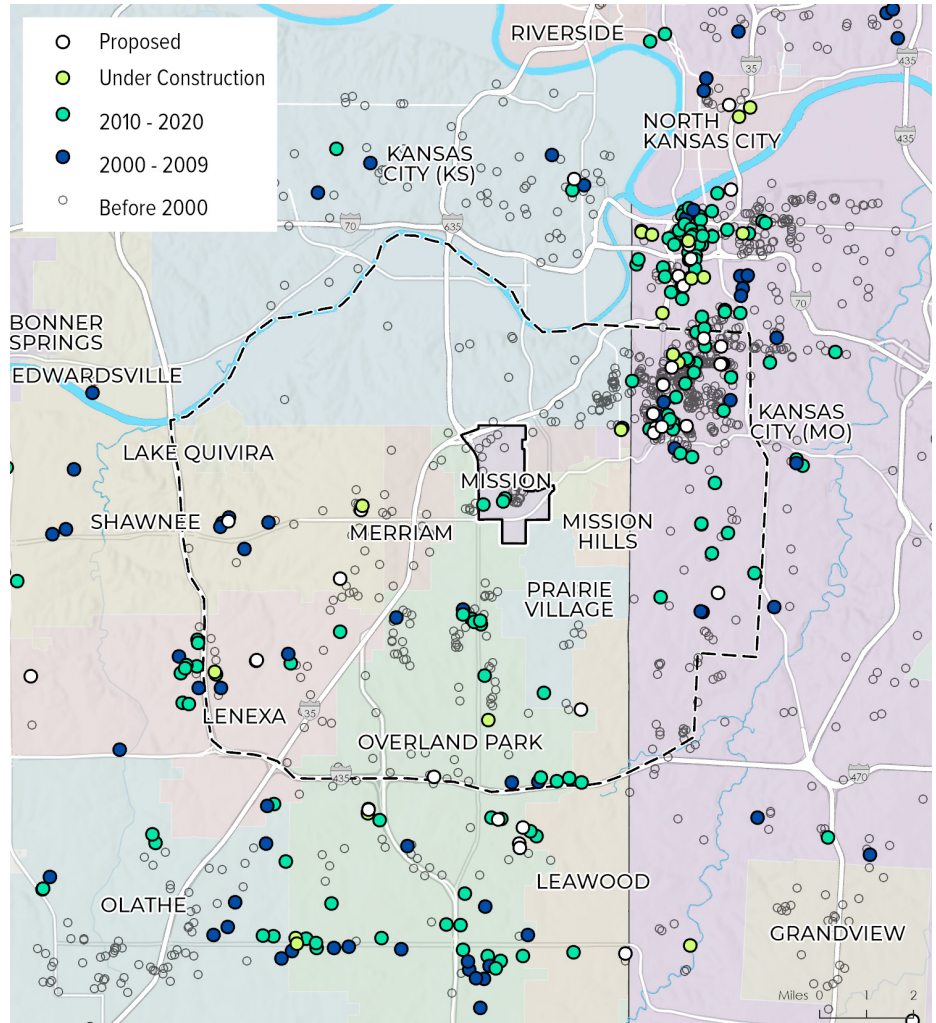


Figure 3.30 - Multi-Family Development In and Around the Market Area

Source: Costar (2020)

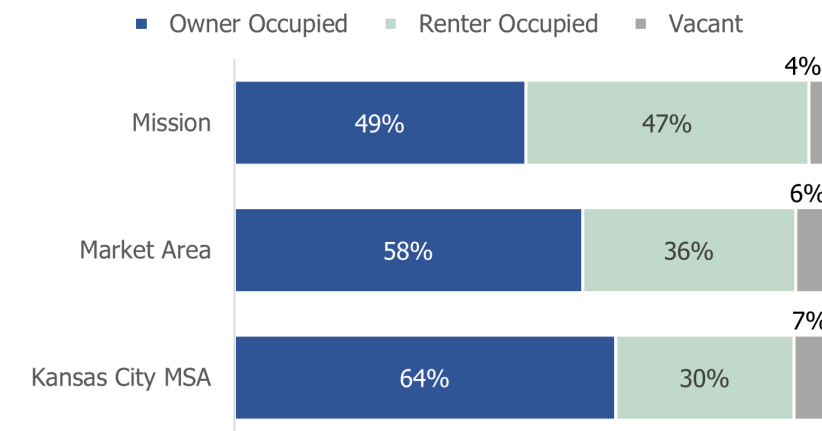


Figure 3.31 - Share of Housing Units by Tenancy

Source: Esri (2020)

3.7 Housing

Multi-Family Residential Trends

Units are decreasing in size.

The average size of a multifamily unit has decreased after a long period of increase beginning in the 1980s. In 2019, new multifamily units had a median floor area of about 1,075 square feet, down from a high of about 1,200 in 2007.

The number of renters is growing more quickly than homeowners.

Many cities have experienced significant gains in renting over recent years while adding fewer homeowners than before. The chart at bottom right shows growth in the number of units by type in Mission and in the Kansas City Metro Area overall. Since 2000, the number of housing units has increased by 22% in the metro area and by 3% locally in Mission; however, owner-occupied units have increased at a slower rate in the metro area (16%) and decreased in Mission (-2%). Conversely, renter-occupied households increased by 34% in the metro area and 8% in Mission. It is important to note that the renter population includes both households that rent space within apartment buildings and those that rent single family housing.

Growing demand for suburban apartments.

In recent decades, there has also been growing interest in multifamily housing in suburban communities, as well as a diversification of the renting population, which includes renters of all ages, incomes, and life stages.

Apartments are competing based on amenities.

There has been an increase in highly amenitized and luxury multifamily housing, with upscale finishes and features such as pools, gym facilities, and clubhouses. While the use of these shared spaces is temporarily paused, it is expected that new upscale multifamily developments will continue to include these types of features to make them more attractive to future tenants.

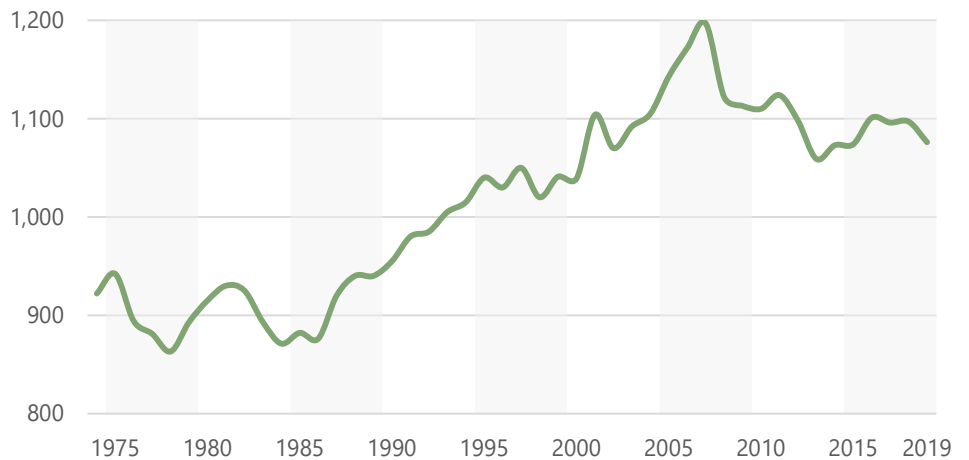


Figure 3.32 - Median Square Feet of New Multi-Family Housing Units

Source: U.S. Census (2020)

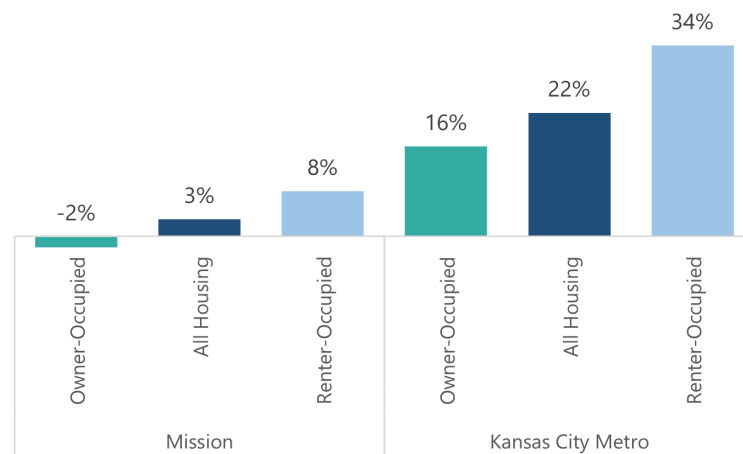


Figure 3.33 - Change in the Number of Housing Units by Type in Mission and the Kansas City Metro Area (2000 - 2020)

Source: Esri (2020)

3.0 MARKET ANALYSIS

3.7 Housing

Housing Demand Projections

Historic population growth and population projections from the Center for Economic Development and Business Research were used to estimate future demand for housing units within Mission, the market area, and Johnson County.

Figure 3.34 shows the projected population for each geography. One clear pattern is that population is expected to grow in Johnson County at a much more rapid rate than in the market area (which encompasses small portions of Wyandotte and Jackson counties) or Mission. This is understandable, given that the market area has been built out for some decades, while Johnson County contains much more land that can accommodate new housing, and therefore, new residents. For this reason, the population of the market area and Mission is expected to remain stable, with new residents added through infill projects and multifamily projects.

Overall, the market area will see demand for about 130 units annually over the next 20 years based on population growth. The location of this population growth will depend on where it can be accommodated, for example, where it is possible to replace lower density homes with higher density single family homes, such as townhomes or larger homes on existing lots that can better accommodate families. Adding multifamily buildings, such as condos or apartments, through adaptive reuse of older buildings or available sites, is also another option. Adding one or two apartment buildings within the market area per year can create 130 housing units.

Given Mission's attractiveness as a community, growth in the metro area's population, and housing market pressures, any type of new housing (single or multifamily) added within Mission is likely to be met with strong demand.

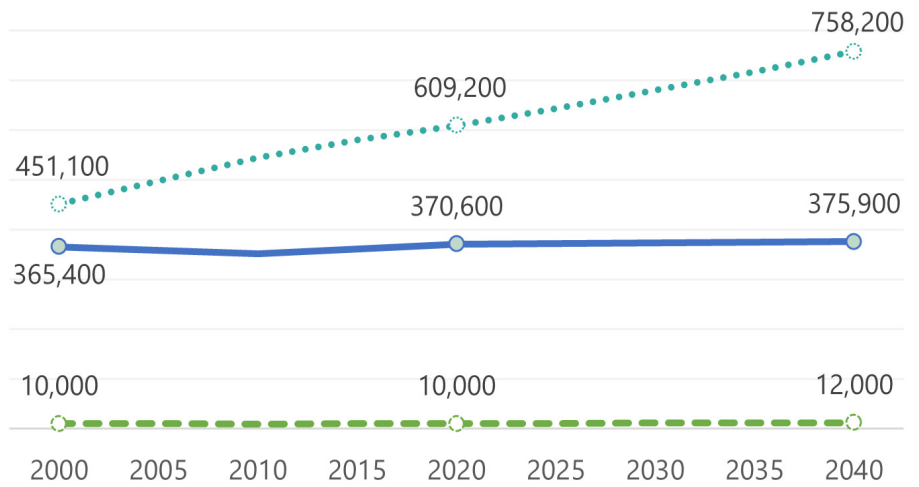


Figure 3.34 - Population Projections and Estimates (2000 - 2040)
Source: U.S. Center for Economic Development and Business Research, LCG (2021)

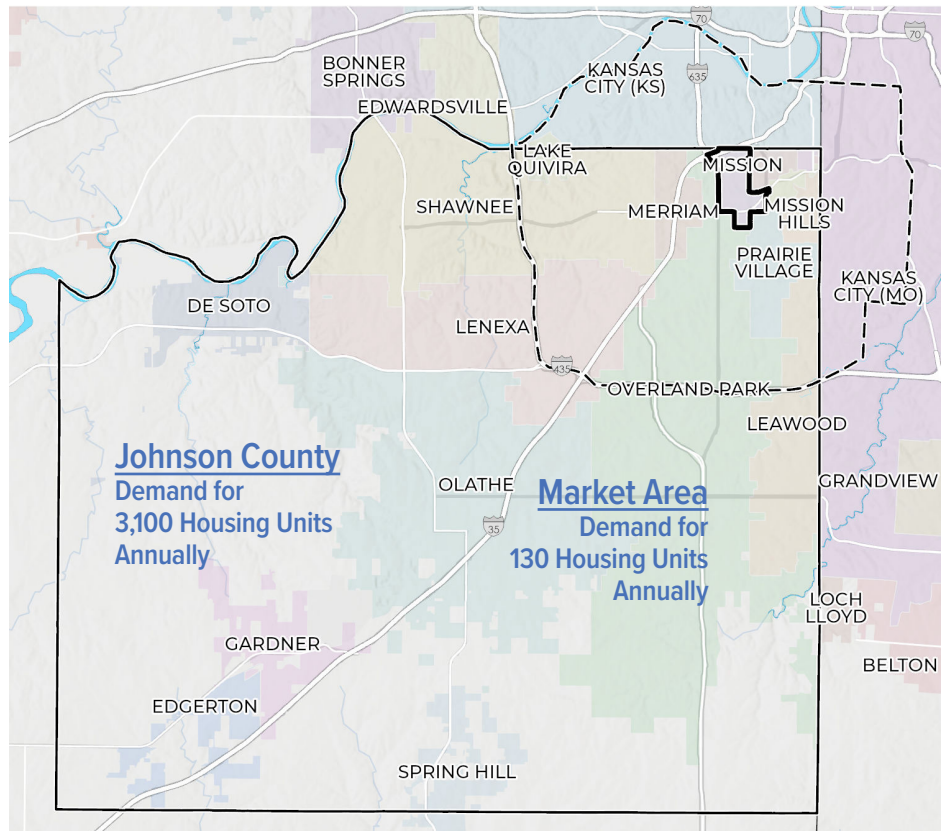


Figure 3.35 - Projected Housing Demand (2020 - 2040)
Source: Esri (2020)

3.8 Commercial Development

Local Commercial Development Overview

Figure 3.36 shows the distribution of commercial land uses within Mission. Markers with dark borders were developed since 2000, while lighter borders are properties developed before 2000.

Commercial land use within Mission is mostly clustered along Johnson Drive, and is characterized by a mix of independent and franchise retailers in one story buildings. Toward the east, retail and office tenants occupy midcentury Main Street style buildings, while toward the west, there are more freestanding retailers and construction is more contemporary. There is a healthy mix of tenants, including a supermarket, general merchandise, restaurants, beauty services, auto services, and fitness facilities, which allows Mission residents to meet many of their shopping needs within a short distance of their home.

In addition to employment at the city's retail establishments, Mission's offices provide local employment opportunities for residents. The University of Kansas Hospital Medical Records Department, ScriptPro, and Vin Solutions provide office-based employment, while public-sector employers include the Department of Motor Vehicles and the US Postal Service.

The Mission Gateway project at the eastern edge of Johnson Drive is a planned mixed-use development that incorporates new multifamily, retail, and hotel development along with regional destinations such as a Cinergy movie theater and a food hall. However, the development has been stalled indefinitely due to the COVID-19 pandemic, and the future of the site is currently unclear (May 2021).

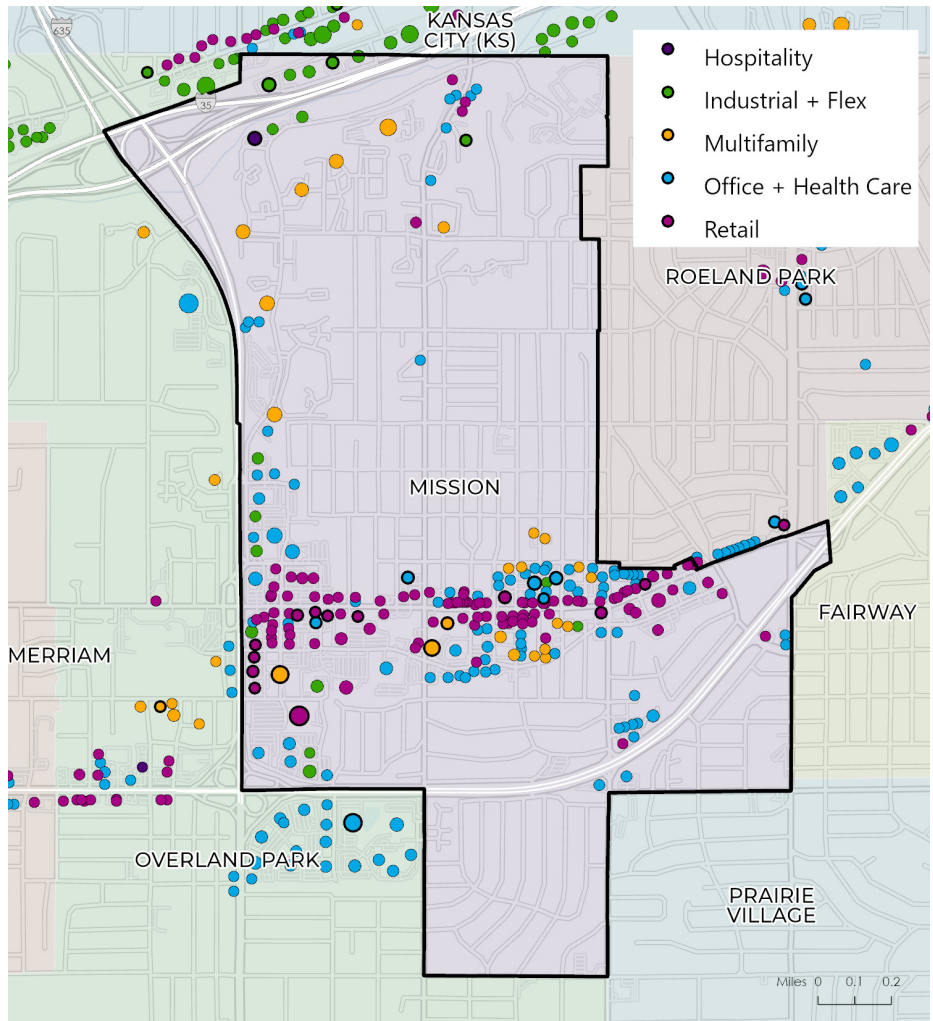


Figure 3.36 - Commercial Properties

Source: Costar (2020)

3.8 Commercial Development

Retail Development

Retail development tends to be less concentrated than other land uses. Neighborhood and everyday retailers tend to develop alongside residential neighborhoods in standalone buildings and smaller shopping centers and serve the local population. These businesses provide daily needs goods and services for residents, who tend to shop at the locations that are most convenient to where they live. Regional retailers, such as big box stores, cluster in larger shopping centers and serve a larger-sized market. In recent years, retail development has occurred throughout Johnson County, most notably along West 135th Street, along other main roads, and within planned mixed-use developments such as Lenexa City Center.

Within Mission, retail construction in the past 20 years includes Cornerstone Commons (2015), Culver's (2012), Chick-Fil-A (2012), and Target (2002).

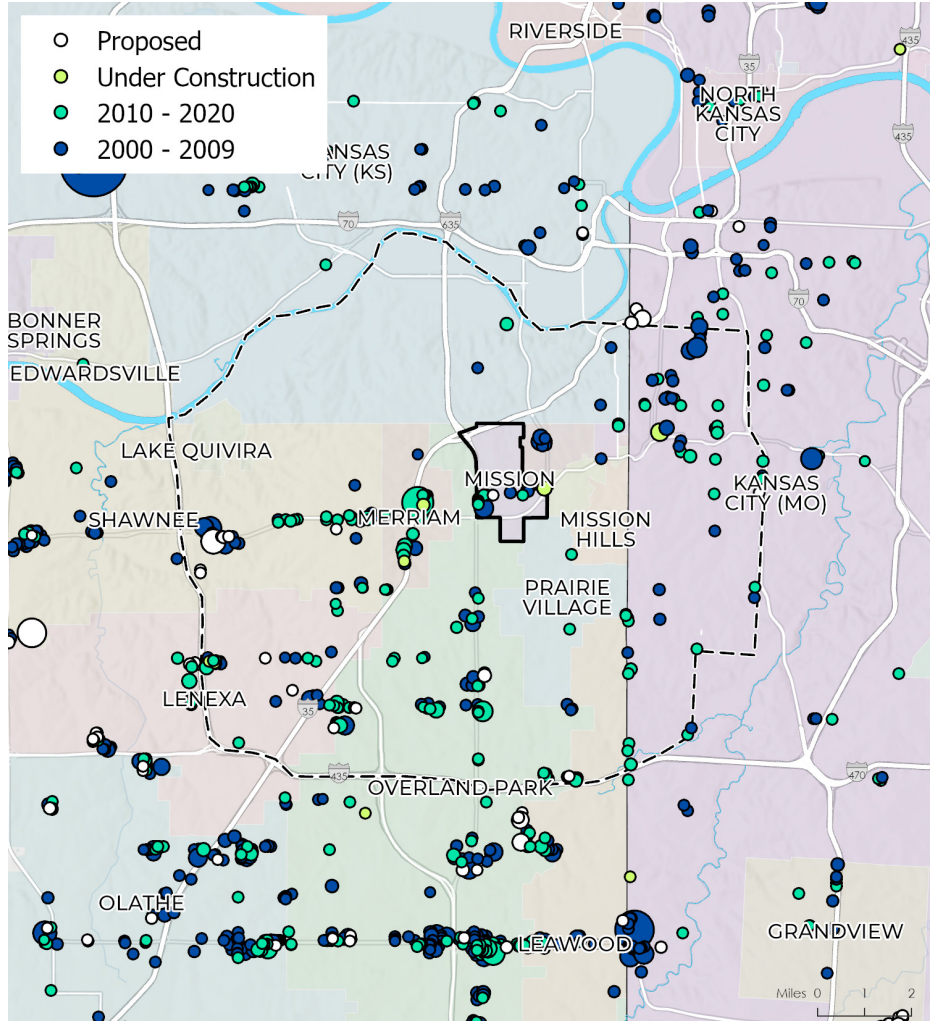


Figure 3.37 - Retail Development In and Around the Market Area

Source: Costar (2020)

3.8 Commercial Development

Retail and Restaurant Space Trends

Retail and consumer preference are continually evolving, with a number of trends emerging in recent years. The Covid-19 pandemic has dramatically transformed shopping and dining behavior at present and the long-term impacts remain to be seen

The rise of online shopping.

Online shopping as a percentage of total retail sales has grown steadily since 2000, and accounted for about 14% of total sales in 2020. The Covid-19 pandemic created a massive surge in demand for online shopping that will likely continue into the near future. Because of this, retail space needs are decreasing, or, in some cases, being shifted to warehouse and distribution space.

The decline of malls and long-standing retailers.

The struggle of malls and traditional mall retailers has been well-documented in recent years. Additionally, many major retailers have filed for bankruptcy in recent years due to evolution in their competition, most notably from online retailers who are able to offer lower prices based on lower overhead cost.

Increased interest in independent, craft, and local establishments.

A number of movements have led to increased demand for local and craft products, including the farm-to-table movement, the rise in craft brewing and other artisan food and beverage production, an increase in food halls and farmers' markets, as well as new markets for independent businesses made possible by online platforms such as Etsy and social media.

A shift in spending away from consumer goods.

The rise of the "experience economy" means that some households prefer to spend more on experiences and less on durable goods than once before, leading to increased demand for concerts, sporting events, travel, and entertainment, and decreased demand for retail space.

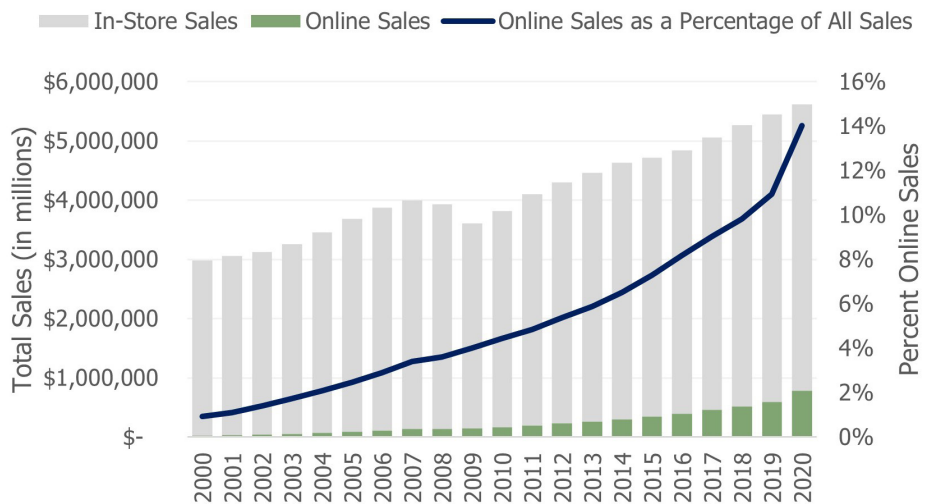


Figure 3.38 - Online Sales (2000 - 2020)

Source: U.S. Census (2021)

3.8 Commercial Development

Retail Projections

Anticipated Retail Demand

New households generate demand for retail goods and services, and therefore areas that are adding new housing will also be the site of the greatest demand for new retail space. Annual retail space demand is much higher in Johnson County than the market area, due to population growth at the outskirts of the county's urbanized areas. Population growth in the county is expected to add demand for about 300,000 square feet of new retail annually, while in the market area, there will be about 66,000 square feet of new retail demand generated annually over the next 20 years. There will be competition among municipalities to capture this retail demand.

Square feet per person.

Currently, there are about 60 square feet of occupied retail space per resident in Johnson County, and about 50 square feet of occupied retail space per resident in the market area. In both cases, the amount of retail space per person has decreased over the past two decades, and is expected to continue to decrease as sales shift to online retailers. For reference, the U.S. average is about 24 square feet per person.

What Mission can do.

Changing habits, preferences, and retail models point to less need for retail space than before. Many households fulfill their daily shopping needs at supermarkets, pharmacies, big box retailers, and online. However, "Main Street" environments, such as Mission's traditional downtown, have found

success as walkable destinations that offer not only independent retailers and restaurants, but also places to walk, people-watch, and socialize. Recognizing this role, Main Street shopping districts should emphasize placemaking, with safe and pleasant sidewalks and crosswalks, and managing parking in a way that does not compromise pedestrians or businesses.

Adding new housing adds support for the retail environment, generating demand for about 50-60 square feet of retail space within the metropolitan region, a small portion of which can be captured locally. Moreover, retail in proximity to housing, especially traditional, walkable retail environments, is a neighborhood amenity that can create value for nearby housing.

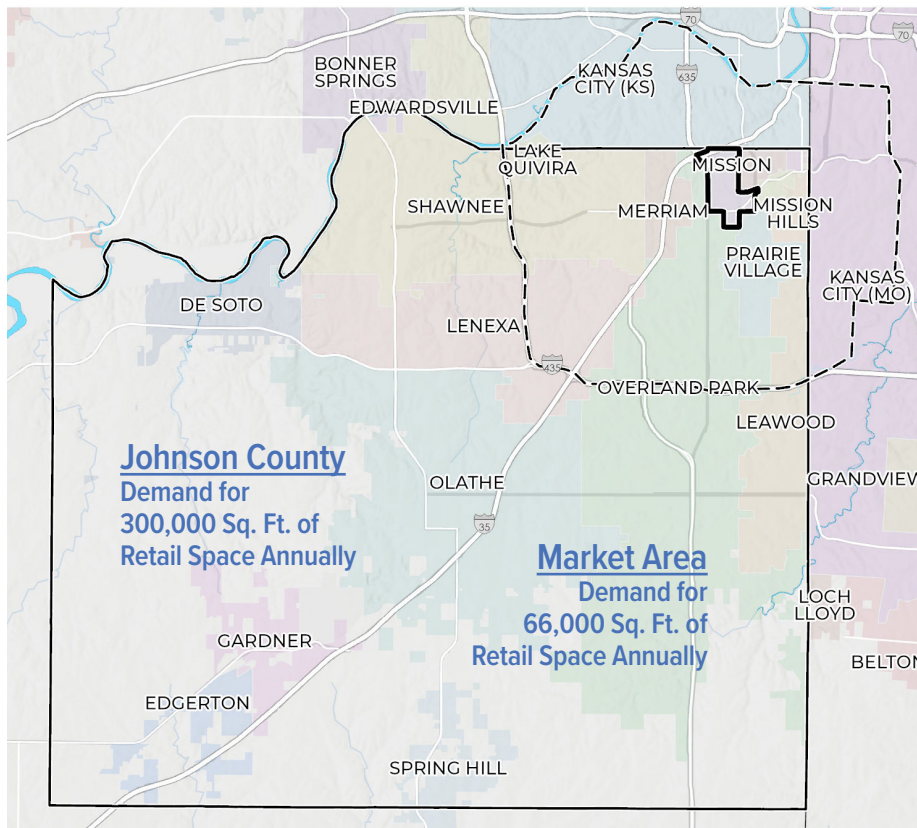


Figure 3.39 - Projected Retail Demand (2020 - 2040)
Source: Esri (2020)

3.9 Office and Health Care Development

Office and Health Care Development

Figure 3.40 shows office and health care construction in and around the market area since 2000. It includes a number of building types, for example, corporate office buildings, smaller multi-tenant buildings, and non-institutional medical office buildings. In the past, major new office construction has taken place in downtown Kansas City as well as in Overland Park. During this time, Mission added about 110,000 square feet of office space, including additions to ScriptPro and the Brill Eye Center.

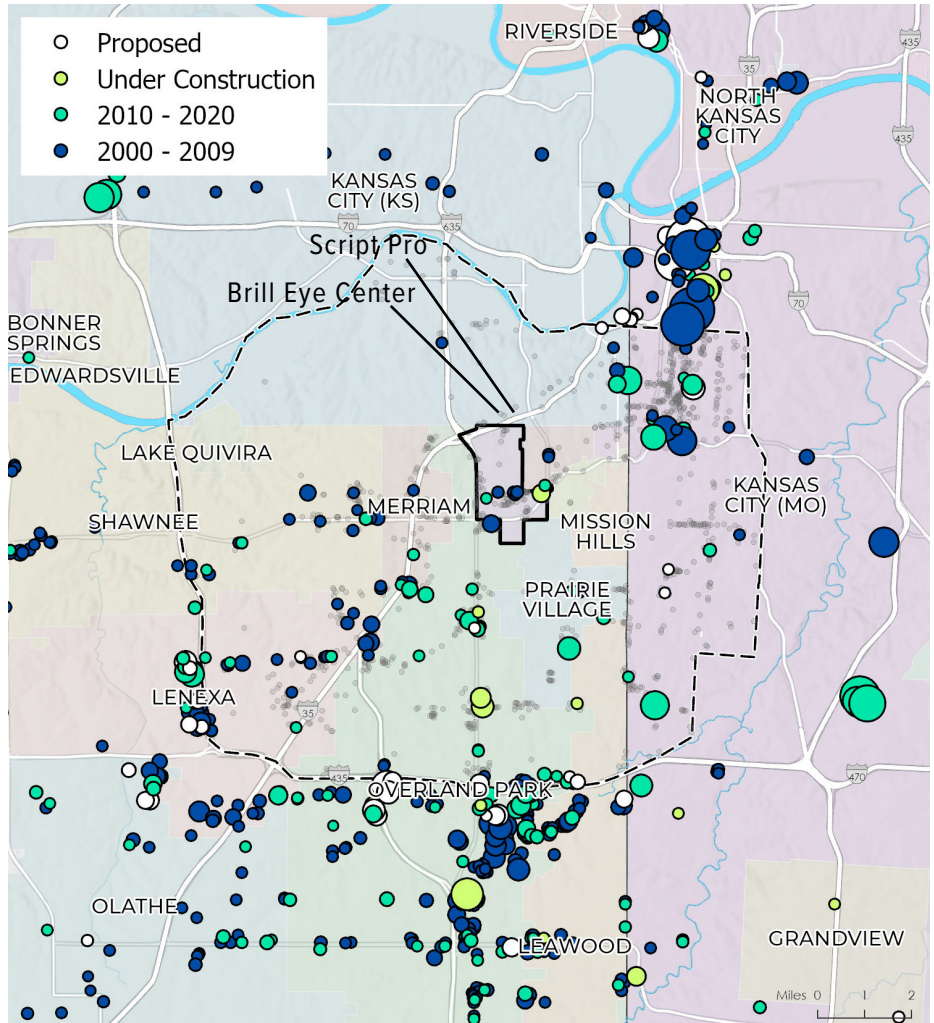


Figure 3.40 - Office and Health Care Development In and Around the Market Area
Source: Costar (2020)

3.9 Office and Health Care Development

Office and Health Care Space Trends

Similar to retail space, office space needs were evolving prior to the COVID-19 pandemic and there is evidence to think the pandemic may cause further reassessment of future office space needs

A decrease in office floor space per employee.

Prior to the COVID-19 pandemic, the average floor space per office employee was decreasing, with some offices adopting open floor plans and shared amenities. It remains to be seen whether the pandemic will bring about a reversal of this trend to maintain greater distances between employees within the workspace.

Emergence of shared spaces.

The proliferation of coworking and communal office spaces, marketed toward freelance, independent, and remote workers seeking a work environment outside of the home, was a well-publicized trend in recent years. This trend has been interrupted by the COVID-19 pandemic and may or may not return to pre-pandemic levels.

Decentralization of office space.

As cities and metropolitan areas have grown in the 20th century, suburban office nodes have emerged, taking the place of one single central business district and dispersing commuting patterns throughout a metropolitan area.

Increase in remote work due to COVID-19.

Many offices shifted abruptly to remote work in March 2020, and it is expected that some employers will continue to permit remote work on a full- or part-time basis into the future.

The rise of teleservices.

The need for social distancing created opportunities for businesses that could easily shift operations to an online format. This was most notable in the healthcare and social services sectors and will likely remain an alternative to a portion of in-person visits in the future.

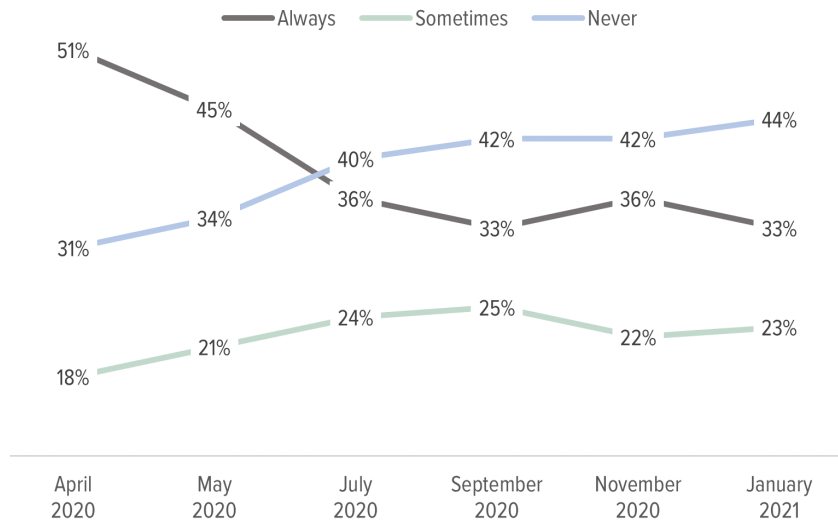


Figure 3.41 - Frequency of Remote Work Due to COVID-19
Source: Gallup (2021)

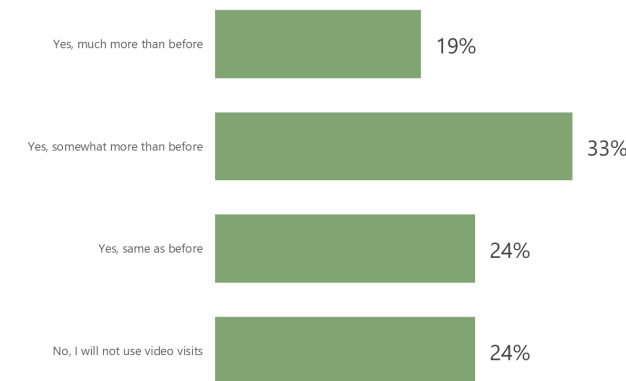


Figure 3.42 - Post-COVID-19, Do you Plan to use Health Care Video Visits?
Source: Amwell Physician and Consumer Survey (2020)

3.9 Office and Health Care Development

Office Projections

Anticipated office demand.

It is estimated based on current population trends that the market area will generate demand for 90,000 square feet of office space per year for the next 20 years, while Johnson County will generate demand for 185,000 square feet of office space per year.

Office space demand is based on the number of employees in industries that are likely to require office space, such as public and private administration, professional services, and healthcare. In general, there's a relationship between the number of households and the number of jobs. Within the market area, the ratio of workers to households has been about 1.1 since 2005, while in

Johnson County, the ratio is 1.5. The smaller ratio of workers to households in the market area is likely related to household size, given that there is a smaller average household size and a greater proportion of single-member households in the market area than in Johnson County.

Though there is a relationship between the number of households and number of workers, ultimately the number of office workers will be determined by where employers choose to locate, which in turn is influenced by available office space or sites for new development and by recruitment efforts.

Office square feet per worker.

Currently, there are about 220 square feet of occupied office space per resident in Johnson County, and about 230 square feet of occupied office space per resident in the market area. For reference, average office space per worker in the U.S. is about 150 square feet. In both geographies, the amount of office space per person has decreased over the past two decades, and is expected to decrease further.

Both the market area and Johnson County are characterized by a high proportion of office workers. Within the market area, 52% of workers work in office-associated sectors, or approximately 1 out of every 3 residents.

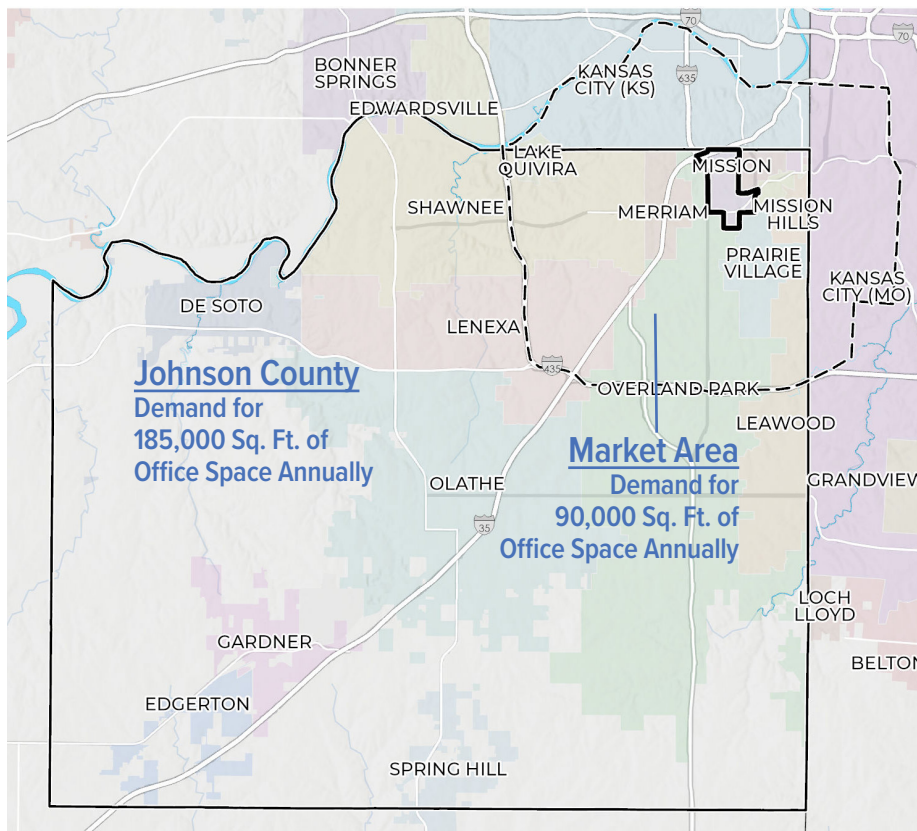


Figure 3.43 - Projected Office Demand (2020 - 2040)

Source: Esri (2020)

3.10 Hotel and Motel Development

Figure 3.44 shows new hotels and motels built since 2000, as well as properties that are planned or under construction. The majority of the region's new hospitality development is taking place in and around downtown Kansas City, and then along and outside of I-435 in the developing areas of Wyandotte and Johnson counties. Growth clusters are also found in Overland Park in proximity to its corporate office parks, as well as in the Kansas Speedway development. Within the boundary of the market area, hotel and motel development has been somewhat limited.

Mission added one hotel in the past 20 years, the 120-suite WoodSpring Suites near I-35, completed in 2007.

Trends

Hotels as a component of Downtown Kansas City revitalization.

Many Midwest cities are subsidizing hotel development in their downtowns in order to create vitality, support convention facilities and downtown businesses, and promote the adaptive reuse of historic buildings. Toward this end, the center of hotel development in the Kansas City region has been its downtown, where it plays an important role in the overall downtown revitalization effort alongside numerous public and private sector efforts.

Projections

On average, the Kansas City metro area has added about 575 hotel rooms annually since 2000. About 9% of this growth, or about 50 hotel rooms per year, has taken place in the market area.

Mission's location along I-35 is suitable for hotel development, and the proposed hotel as part of the Mission Gateway would also be suitable within the context of a larger development that offers visitor amenities. Apart from these two hotels, the market for hotel development in Mission is not as strong as residential, retail, and office demand.

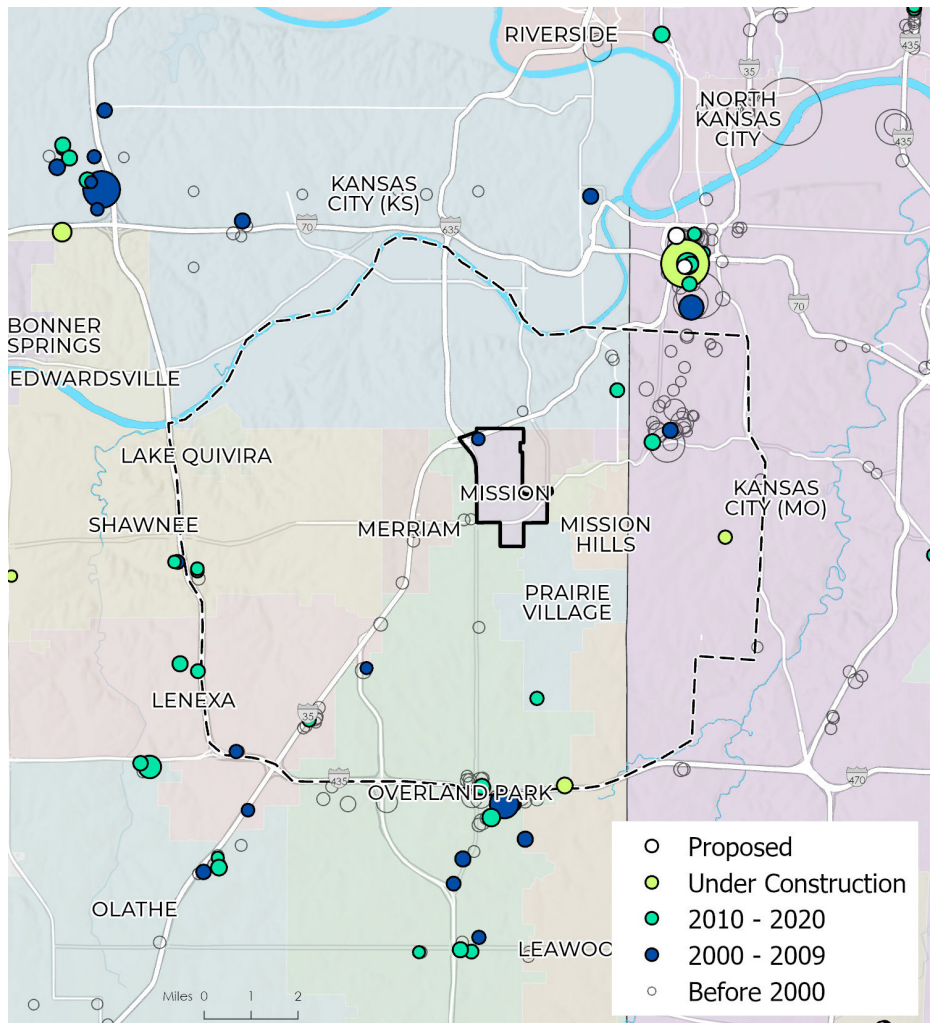


Figure 3.44 - Hotel and Motel Development In and Around the Market Area
Source: Costar (2020)

3.11 Industrial Development

Industrial development tends to occur in clusters near transportation facilities, including along highways and at major highway interchanges, along river shipping routes, freight shipping routes, and near airports, especially where land is unattractive for other types of development. Major industrial clusters can be found along the Missouri and Kansas Rivers, along I-35, and in Kansas City, Kansas.

Within Mission, industrial development is limited to the Freeway Industrial Park along I-35, Turkey Creek, and the rail corridor. In the past 20 years, the city has added about 80,000 square feet of industrial space.

Trends

Transition to 21st century industrial space needs.

The decline of the traditional labor-intensive manufacturing sector as the result of greater automation and offshoring is well-documented. However, manufacturing continues to be a vital sector of the U.S. economy, particularly advanced manufacturing that requires fewer but more highly-educated workers and facilities with modern electrical, internet, ventilation, and space capabilities.

Increase in "last mile" space needs.

The line between retail and industrial space is becoming increasingly blurred with the rise of online retail. These transactions drive demand for warehousing and logistics spaces near urban centers and transportation facilities.

Projections

Mission has about 800,000 square feet of industrial space, which has decreased slightly from 2000, when the city had a little over one million square feet. Since the areas appropriate for industrial development are mostly built out, there are limited opportunities to capture any additional industrial development in the future.

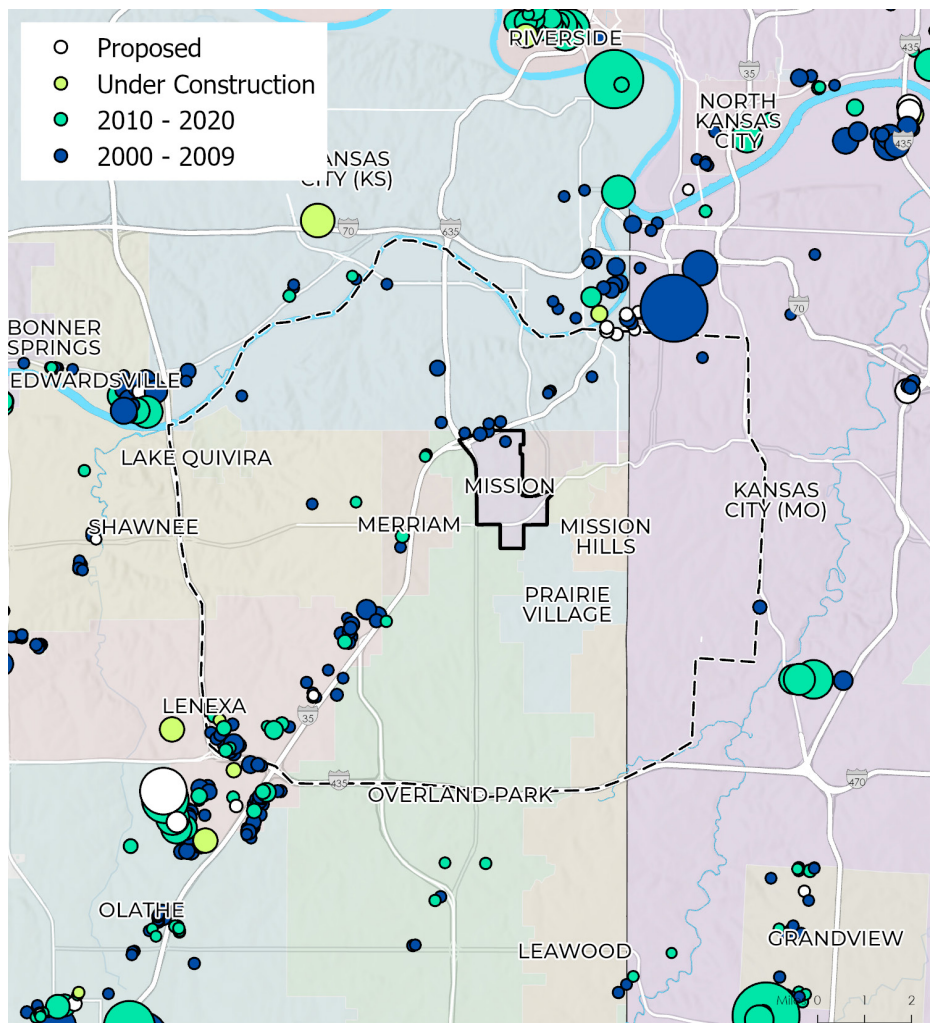


Figure 3.45 - Industrial Development In and Around the Market Area
Source: Costar (2020)

Natural Features and Environment

	Strategy	Previous Priority	New Priority	Previous Time Frame	New Time Frame
Goal 1: Continue investment in Rock Creek flood abatement and storm water infrastructure and prioritize green solutions and BMPs wherever possible. Avoid creating public safety and environmental hazards.					
1A	Add green infrastructure along the Rock Creek corridor	Medium	Medium	3-5 years	3-5 years
1B	Promote green infrastructure on public and private property	Low	Medium	Ongoing	Ongoing
1C	Balance the needs of the environment and economic development along the Rock Creek corridor	Medium	Medium	3-5 years	3-5 years
1D	Integrate principles of design and public safety when evaluating developments or improvements for projects that impact public spaces including open space and parks	Medium	Medium	1-3 years	1-3 years
Goal 2 - Connect development and redevelopment opportunities to sustainable practices and utilize a sustainability “lens” in all city decisions. Connect these opportunities to the Kansas City Regional Climate Action Plan that the City has adopted.					
2A	Consider the economic, equity, and environmental aspects of sustainability when making decisions for the community	High	High	Ongoing	Ongoing
Goal 3 - Implement the City’s climate action policies with consideration to the regional plan.					
3A	Continue Mission's commitment to reaching its Sustainability and Climate Action Plan goals and community targets	Low	High	Ongoing	Ongoing
Goal 4 - As a land-locked community, encourage new development and redevelopment projects to include greenspace, and protect and expand the tree canopy in both commercial and residential areas.					
4A	Expand greenspace and protect tree cover by encouraging both to be integrated into new development or redevelopment	High	High	1-3 years	1-3 years
4B	Preserve and increase Mission's tree canopy	Medium	High	1-3 years	1-3 years
4C	Maintain Mission's natural features	Low	Low	-	Ongoing
4D	Encourage use of native plants throughout Mission	Low	High	-	Ongoing
Goal 5 - Maintain and preserve existing open space and natural features to enhance the character of the built environment, promote neighborhood stability, public health and safety, and provide for outdoor recreation and visual enjoyment.					
5A	Continue to encourage sustainability measures for new development and redevelopment projects in Mission	High	High	1-3 Years	1-3 years

Parks and Recreation

	Strategy	Previous Priority	New Priority	Previous Time Frame	New Time Frame
Goal 1 – Maintain and preserve existing open space and natural features to enhance the character of the built environment, promote neighborhood stability, public health and safety, and provide for outdoor recreation and visual enjoyment.					
1A	Regularly review Mission parks and recreation facilities	Low	High	Ongoing	Ongoing
1B	Increase visibility and awareness of existing park facilities	Medium	High	Ongoing	Ongoing
1C	Improve existing trail network in Mission	High	High	3-5 years	Ongoing
Goal 2 - Enhance parks and recreational spaces that provide for outdoor activities, gathering spaces, and pedestrian and bicycle trails.					
2A	Expand trail network throughout the city so that all residents are within ¼ mile of the trail network	Low	Medium	-	Ongoing
2B	Allow for flexible and creative use of streets/parking areas for public gathering	Medium	Medium	1-3 years	1-3 years
2C	Continue to support the Mission Market	High	High	1-3 years	1-3 years
Goal 3 - Invest in ongoing maintenance and park enhancements, including restrooms, playground equipment, seating options, and other amenities.					
3A	Adequately fund, maintain, and enhance Mission's Parks	High	High	Ongoing	Ongoing
Goal 4 - Ensure parks and recreational spaces are compliant with ADA standards and available to users of all ages and abilities.					
4A	Ensure Mission's parks are accessible and usable to everyone	High	High	Ongoing	Ongoing
Goal 5 - New or newly redeveloped public spaces should include Universal Design Features.					
5A	Incorporate Universal Design features into Mission's parks and recreation spaces	High	Medium	Ongoing	Ongoing

Transportation and Mobility

	Strategy	Previous Priority	New Priority	Previous Time Frame	New Time Frame
Goal 1 - Make pedestrian safety a high priority, especially on Johnson Drive and Downtown.					
1A	Maintain, improve, and expand the sidewalk network throughout the city	High	High	1-3 years	1-3 years
1B	Add and improve key crossing locations along major arterial roadways	High	High	3-5 years	3-5 years
1C	Incorporate traffic calming features along strategic corridors to promote safe and comfortable walkability	Medium	Medium	-	Ongoing
Goal 2 - Plan for a multi-modal transportation/mobility system that supports future-focused transportation such as electric cars, automated vehicles, specialized transport services (including privately owned ride-share and delivery services), and public rental bicycles.					
2A	Support and expand specialized transit services in Mission	Low	Low	Ongoing	Ongoing
2B	Support public bike rentals and mobility hubs	Medium	High	Ongoing	Ongoing
2C	Implement infrastructure to support electric modes of transportation on public and private properties	Medium	Medium	1-3 years	1-3 years
2D	Strategize planning efforts to support future-focused transportation	Medium	Medium	Ongoing	Ongoing
Goal 3 - Develop flexible policies that allow the City to adapt to future needs within prepared criteria regarding safety, efficiency, and access.					
3A	Adopt a Complete Streets Plan city-wide	High	High	3-5 years	1-3 years
3B	Implement a flexible Complete Streets ordinance	Low	High	-	Ongoing
3C	Regularly review policies and enforce codes to maintain safe rights-of-way and visibility at intersections	Medium	Medium	Ongoing	Ongoing
Goal 4 - Tie current and future mobility plans to the City's economic development strategy and neighborhood stabilization.					
4A	Utilize the Future Land Use Plan to support higher-density mixed-use developments where appropriate	High	High	Ongoing	Ongoing
4B	Develop policy to support Transit-Oriented Developments	Medium	Medium	-	Ongoing
4C	Implement infrastructure upgrades and traffic calming elements on local streets as a means of neighborhood preservation	Low	Medium	-	Ongoing
Goal 5 - Recognize Johnson Drive as a major connection for local and metropolitan residents. Slow traffic at key points and provide well-marked crosswalks for pedestrians.					
5A	Continue to improve pedestrian experience along Johnson Drive	High	High	3-5 years	3-5 years
5B	Explore feasibility of extension of Downtown improvements west of Lamar Avenue to Metcalf Avenue	High	High	1-3 years	1-3 years
5C	Improve pedestrian access and safety	High	High	Ongoing	Ongoing

Transportation and Mobility

	Strategy	Previous Priority	Priority	Previous Time Frame	New Time Frame
Goal 6 - Coordinate with MARC, KCATA, and surrounding communities to support the SmartMoves 3.0 Regional Plan and incorporate updated transit technology for transit facilities, transit routes, micro-transit options, electric and automated vehicles, and street cars.					
6A	Engage with KCATA and regional leaders as regional transit plans such as OneRideKC develop	Low	Low	-	Ongoing
6B	Work with Johnson County and transit leaders to prepare the Mission Transit Center for future transit technologies	Low	Low	-	Ongoing
Goal 7 - Explore the feasibility of reconfiguring the Johnson Drive and Metcalf Avenue crossing to encourage economic development activity and improved access to Downtown Mission.					
7A	Commission a feasibility study for the conversion for the interchange at Metcalf Avenue and Johnson Drive covering aspects of traffic safety, traffic operations, and bicycle and pedestrian access	Medium	High	-	1-3 years
Goal 8 - Explore future public street alignments within the Form Based Code District as outlined in the West Gateway FBC to enhance access, connectivity, and redevelopment efforts.					
8A	Look at opportunities for future street alignments in the West Gateway District	Low	Low	-	Ongoing

Economic Revitalization

	Strategy	Previous Priority	New Priority	Previous Time Frame	New Time Frame
Goal 1 - The Johnson Drive Corridor is the longstanding retail district within Mission and should be enriched by future public projects and private redevelopment.					
1A	Continue to enhance the West Gateway District	Medium	Medium	3-5 years	3-5 years
1B	Continue to invest in Downtown Mission	High	High	1-3 years	1-3 years
1C	Create a new community-led vision for the East Gateway District	Low	Low	Ongoing	Ongoing
Goal 2 - Promote a mix of office, retail, and residential uses along the Johnson Drive commercial corridor of the city.					
2A	Promote flexible mixed-use developments along the Johnson Drive corridor	High	High	3-5 years	3-5 years
Goal 3 - Promote a sustainable, diverse economy.					
3A	Foster a healthy relationship between the City and existing businesses	High	High	1-3 years	1-3 years
3B	Actively seek new business opportunities in key commercial corridors	High	High	1-3 years	1-3 years
3C	Remain adaptable to changes in economic conditions while continuing to protect community interests	Medium	Medium	-	Ongoing
3D	Enhance transit and pedestrian/bicycle infrastructure as a means of economic development	Medium	Medium	-	Ongoing
3E	Support the growth and enhancement of all of Mission's commercial areas	Medium	Medium	Ongoing	Ongoing
3F	Use incentives and assistance programs to expand and support community priorities and values	High	High	Ongoing	Ongoing
Goal 4 - Promote consistent public identity at city gateways. This is an opportunity to incorporate creative entry-ways as part of renewed efforts for redevelopment.					
4A	Encourage development and enhancement in and around major gateways in Mission	Medium	Medium	-	Ongoing
Goal 5 - Continue to build positive redevelopment without losing the unique character of residential and commercial districts.					
5A	Seek development that enhances the unique character of Mission by building upon successful past redevelopment	High	High	Ongoing	Ongoing
Goal 6 - Build on the success of the Johnson Drive Corridor improvement project. This area with wide sidewalks, friendly neighbor interactions, and local shops creates a unique mix of charm and practicality that resonates with citizens and visitors.					
6A	Continue to expand the Johnson Drive Corridor improvement treatment to other corridors throughout Mission	Low	Low	-	Ongoing

Economic Revitalization

	Strategy	Previous Priority	New Priority	Previous Time Frame	New Time Frame
Goal 7 - Respect the character and authenticity of Downtown when redeveloping and building new or infill projects to maintain a thriving district.					
7A	Seek new opportunities to enhance Downtown Mission through available programs and grants	Medium	Medium	-	Ongoing
Goal 8 - Use fresh and innovative signage.					
8A	Create and design signage for Mission's three Downtown districts that represent the unique character of each area	Low	Low	-	3-5 years
Goal 9 - Encourage property owners to refresh building facades and outdated store fronts through the use of city grants or special programs.					
9A	Support Mission businesses that want to improve building facades and enhance the property's sustainability	High	High	1-3 years	1-3 years
Goal 10 - Incorporate new technologies for city-wide broadband access and Smart City strategies to provide easy access to information for residents, commercial properties, visitors, and stakeholders.					
10A	Promote awareness of Mission's high-speed broadband connectivity capabilities	Low	Low	-	Ongoing
10B	Promote Smart City Initiatives in Mission and actively pursue regional partnerships	Low	Low	-	Ongoing

Housing and Neighborhoods

	Strategy	Previous Priority	New Priority	Previous Time Frame	New Time Frame
Goal 1 - Promote effective development and redevelopment of sustainable single-family and affordable multi-family housing options for all ages.					
1A	Allow for creative infill housing development	High	High	5+ years	5+ years
1B	Create multi-family developments in mixed-use zones	Medium	Medium	-	Ongoing
1C	Examine the existing Form Based Code District for new developments and redevelopments	High	High	1-3 years	1-3 years
Goal 2 - Encourage residential revitalization as the housing stock in Mission continues to age and market demands change.					
2A	Encourage Universal Design features in residences	Low	Low	-	3-5 years
2B	Promote grant and assistance programs available for housing revitalization at the local, county, and state levels	High	High	3-5 years	Ongoing
Goal 3 - Reduce impact on residential properties through effective transitional land uses policies and development standards.					
3A	Utilize buffers for gradual density shifts in neighborhoods	High	High	3-5 years	3-5 years
3B	Intentionally redevelop along transitional land uses	Low	High	-	Ongoing
3C	Adopt site development standards for residential land uses	Low	High	-	1-3 years
3D	Implement residential design standards related to the scale, setback, footprint, etc. for housing in Mission	High	High	1-3 years	1-3 years
Goal 4 - Add Missing Middle zones to address infill development and reduce barriers to transitional and higher-density uses.					
4A	Promote Missing Middle Housing that complements the context and scale of surrounding properties	High	High	3-5 years	3-5 years
Goal 5 - Continue building upon positive redevelopment trends and promote a positive city-wide image of Mission in residential and commercial areas.					
5A	Assist resident-based groups in Mission neighborhoods	Medium	Medium	Ongoing	Ongoing
Goal 6 - Continue emphasis on code enforcement to maintain the aesthetic, life, health, and safety of the community.					
6A	Expand the Inspection Program to annually include more multi-family units	Medium	Medium	Ongoing	Ongoing
6B	Educate property owners and neighborhoods on code and code violations	High	High	1-3 years	1-3 years
Goal 7 - Develop an on-going strategy to protect and expand affordable housing in Mission.					
7A	Set a term of affordability for new developments in Mission	Low	Medium	-	1-3 years
7B	Establish building design standards for new affordable residential developments	Medium	Medium	-	3-5 years
7C	Ensure there are move-up ready homes in Mission	Medium	Medium	-	3-5 years

Housing and Neighborhoods

Goal 8 - Create an environment that supports the application of Accessory Dwelling Units (ADUs).

8A	Develop criteria and standards for Accessory Dwelling Units where appropriate	Medium	Medium	-	3-5 years
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Infrastructure Maintenance and Enhancements

	Strategy		Priority		Time Frame
Goal 1 - Prioritize city budget allocations for continued and anticipated development needs in Mission over the next two decades and beyond.					
1A	Continue to allocate funding for stormwater management, including gray and green infrastructure solutions	High	High	3-5 years	3-5 years
1B	Continue to fund pedestrian and bicycle infrastructure enhancements along Mission roads	High	High	1-3 years	1-3 years
1C	Continue to improve and maintain Mission parks and trails while expanding the system as opportunities emerge	High	High	1-3 years	1-3 years
1D	Maintain Mission's roads and provide neighborhood specific streetscape enhancements to improve safety and placemaking	High	High	1-3 years	1-3 years
Goal 2 - Focus on primary infrastructure issues first - streets, sidewalks, curb and gutter, stormwater needs, etc.					
2A	Focus on primary infrastructure issues	Medium	Medium	Ongoing	Ongoing
Goal 3 - Continue to inventory existing conditions for maintenance and replacement.					
3A	Thoroughly analyze inventory of current infrastructure conditions	Medium	Medium	Ongoing	Ongoing
3B	Prioritize infrastructure resiliency in all utility improvements	Medium	Medium	Ongoing	Ongoing
Goal 4 - Coordinate improvements with other planned utility maintenance, repair, or replacement.					
4A	Coordinate infrastructure upgrades for the benefit of financial efficiency and quality of life improvements	Low	High	Ongoing	Ongoing
Goal 5 - Plan and include costs for added amenities that support the City's goals: people-oriented improvements, improved signage, streetscapes, outdoor sidewalk use, crosswalks, and neighborhood needs.					
5A	Prioritize people-oriented improvements	Low	Medium	-	Ongoing
5B	Enhance amenities to support neighborhoods and residents	Medium	Medium	-	Ongoing
5C	Leverage Complete Streets Policy for people-oriented improvements	Low	High	-	1-3 years
Goal 6 - Prioritize sustainability practices for all developments in Mission.					
6A	Strengthen sustainability requirements and best management practices	Medium	High	3-5 years	Ongoing